



# Village of Lincolnwood Plan Commission

*Meeting*  
**Wednesday September 11, 2013**  
**7:00 P.M.**

*in the*  
**Council Chambers Room**  
**Lincolnwood Village Hall - 6900 Lincoln Avenue**  
**Meeting Agenda**

1. **Call to Order/Roll Call**
2. **Pledge of Allegiance**
3. **Approval of Minutes**  
August 7, 2013 Meeting Minutes
4. **Public Hearing: 4007 West Touhy Avenue – Special Use and Zoning Variations**  
**Request:** Special Use Consideration for a Drive-Thru Facility and Consideration of Certain Zoning Variations for a Proposed Bank Facility
5. **Public Hearing: Commercial Masonry Requirements - Zoning Code Text Amendment**  
*(Remanded by Village Board and Committee on Ordinances, Rules, and Buildings)*  
**Request:** Text Amendment to Modify the Commercial Design Standards Relative to Masonry Requirements and to Consider Definition of “Masonry”
6. **Public Hearing: Health Club or Private Recreation and Firearms Shooting Ranges - Zoning Code Text Amendment**  
*(Continued from July 10, 2013, May 1, 2013, March 6, 2013 and February 6, 2013)*  
**Request:** Text Amendment to Definition Section to consider Establishing or Modifying Certain Definitions Including but Not Limited to Health Club or Private Recreation, Firearms Shooting Ranges, or Similar Terms; Consideration of Firearms Shooting Ranges as a Special Use in M-B District Subject to Certain Restrictions; and, Establishing or Modifying Off-Street Parking Requirements for Firearms Shooting Ranges, Commercial Recreation Facility, or Other Similar Uses

7. **Public Hearing: Accessory Structures - Zoning Code Text Amendment**  
*(Continued from July 10, 2013, June 5, 2013, May 1, 2013, March 6, 2013 and February 6, 2013)*  
**Request:** Text Amendment to Permitted Obstructions in Required Yards Section of Code to consider Modifying Regulations Pertaining to and Including but Not Limited to Open Balconies, Porches, and Open Patios or Terraces
  
8. **Public Hearing: Lincoln Avenue Plan – Amendment to Comprehensive Plan**  
**Request:** Moratorium and Amendment to Comprehensive Plan Concerning the Lincoln Avenue Subarea Including but Not Limited to Multi-Family Housing
  
9. **Other Business: Public Hearing Procedures**
  
10. **Next Meeting**
  
11. **Public Comment**
  
12. **Adjournment**

**MEETING MINUTES OF THE  
PLAN COMMISSION  
August 7, 2013  
7:00 P.M.**

**DRAFT**

**LINCOLNWOOD VILLAGE HALL  
COUNCIL CHAMBERS  
6900 NORTH LINCOLN**

**MEMBERS PRESENT:**

Chairman Paul Eisterhold  
Irving Fishman  
Don Sampen  
Mark Yohanna  
Patricia Goldfein  
Sue Auerbach

**MEMBERS ABSENT:**

Steve Jakubowski

**STAFF PRESENT:**

Community Development Manager Aaron N. Cook, AICP  
Community Development Coordinator Lauren Wolf

**VILLAGE COUNSEL:**

None.

**OTHERS PRESENT:**

Trustee Cope

**I. CALL TO ORDER**

Chairman Eisterhold noted a quorum of six members present and called the meeting to order at 7:03 p.m. Chairman Eisterhold notified the public that Commissioner Jakubowski would be absent.

**II. APPROVAL OF MINUTES**

Motion to approve the July 10, 2013 Plan Commission minutes with minor amendments was made by Commissioner Fishman and Seconded by Commissioner Yohanna. Motion approved 6-0.

**III. Public Hearing: Definitions – Zoning Code Text Amendment**

*(Continued from June 5, 2013, May 1, 2013, March 6, 2013 and February 6, 2013)*

**Request: Text Amendment to Definition Section to consider Modifying and Adding Certain Definitions Including but Not Limited to Banquet Facility, Banquet Hall, Façade, Fence, Semi-Private or Semi-Private Fence, Restaurant and Restaurants, Fast-Food or Carryout**

August 7, 2013

Chairman Eisterhold noted that this agenda item will be continued until Trustee Cope arrives to speak on the matter.

**IV. PUBLIC HEARING: Illumination/Glare Requirements for Signs – Zoning Code Text Amendment**

*(Continued from June 5, 2013, May 1, 2013, March 6, 2013 and February 6, 2013)*

**Request: Text Amendment to Organize or Modify References to Glare Reduction Regulations for Signs**

Development Manager Cook explained the matter for consideration is for the sign chapter of the zoning code. This Text Amendment is for possible re-organization to group all glare requirements in one location. Staff found the sign chapter is effective in providing these types of regulations. Staff doesn't believe that re-organizing is needed but, should modify the prior action taken by the Plan Commission and the Village Board with the concern of the sign code in 2011. Glare reduction was added with a maximum level of foot-candles. In section 11.07.5 it still makes reference to the language of a dark background and light lettering which had been previously eliminated and needs to be removed from the zoning code. Similarly section 11.08(13)IX had been drafted based upon the prior eliminated requirement and Staff notes this should also be removed.

Chairman Eisterhold asked if there was anyone in the audience who wanted to comment. There was none.

Motion to eliminate the requirements in the sign code in section 11.07(5) and 11.08(13)IX as presented by Staff was made by Commissioner Fishman. Seconded by Commissioner Sampen. Motion approved 6-0 Aye.

**V. PUBLIC HEARING: Voting Requirements – Zoning Code Text Amendment**

*(Continued from June 5, 2013, May 1, 2013, March 6, 2013, & February 6, 2013)*

**Request: Text Amendment to Consider Modifying or Eliminating Voting Deadlines for Plan Commission and Zoning Board of Appeals**

Development Manager Cook explained in administering the Zoning Ordinance staff has concerns with the voting/recommendation deadlines placed on the Plan Commission and Zoning Board of Appeals. The purpose of this consideration is to discuss the self-imposed deadlines relative to the Plan Commission and Zoning Board of Appeals voting. Basically, after 90 days of the completion of the application for the proposed variation the Commission shall make findings and recommend whether the variation, special use or otherwise should approved and shall transmit such a recommendation in writing to the Board of Trustees. Currently, if the Plan Commission or Zoning Board of Appeals fails to act within the identified deadline, the Zoning Ordinance states that such failure to act constitutes a recommendation to approve the application. The deadlines are also vague as when the clock starts, when will the 90 day period begin and end.

August 7, 2013

Commissioner Goldfein questioned whether the 90 day period should be extended to something like 120 days. Development Manager Cook responded that the amount of days is not the concern, however, when the 90 day period begins.

Commissioner Fishman indicated that the 90 days should begin at the start of the first scheduled hearing date.

Commissioner Yohanna stated that he doesn't have a problem leaving this section the way it is, there has not been any problems.

Chairman Eisterhold asked if there was anyone in the audience who wanted to comment. There was none.

Motion to make a recommendation of no change to this provision of the code made by Commissioner Yohanna. Seconded by Commissioner Auerbach. Vote 6-0 Aye.

#### **VI. PUBLIC HEARING: Public Hearing: Definitions – Zoning Code Text Amendment**

*(Continued from June 5, 2013, May 1, 2013, March 6, 2013 and February 6, 2013)*

#### **Request: Text Amendment to Definition Section to consider Modifying and Adding Certain Definitions Including but Not Limited to Banquet Facility, Banquet Hall, Façade, Fence, Semi-Private or Semi-Private Fence, Restaurant and Restaurants, Fast-Food or Carryout**

Trustee Cope joined the Plan Commission meeting.

Development Manager gave a short PowerPoint presentation on the matter. The definitions are vague and may raise some questions. How does the Village want to define these uses. Currently, the village code defines Restaurants and Restaurant Fast-Food or Carryout separately. The reference and consideration that these are different types of uses are repeated in Section VII Table 7.01.1 the Off-Street Parking Schedule. However, the Permitted and Special Uses Table 4.01.1 lists only Restaurant with no reference to Restaurant Fast-Food or Carryout. Staff has interpreted that the Permitted and Special Uses Table considers Restaurants and Restaurants Fast-Food or Carryout to be the same use only differentiated by floor area. The Land Use Table allows for all Restaurants not specific on the type of Restaurant, it is based off square footage if it is 5000 Square Feet or less then you are allowed in the B1, B2, B3 and a Special Use in MB. Recently, Staff has been questioned on this interpretation and in order to remedy any confusion Staff believes these definitions need to be modified.

Trustee Cope was welcomed to speak on the matter. Trustee Cope indicated that this issue has not been thought through by Staff. The proposed definition and Land Use Table is not good. Defining Restaurant and including Carryout is not a way to solve the problem. They need to all be defined separately. The approach is not proper, need to clarify and designate. Trustee Cope also indicated that Fast-Food and Carryout should

August 7, 2013

always be defined as a Special Use. Neighboring communities such as Evanston, Wilmette and Skokie all define Restaurants separately by use.

Chairman Eisterhold indicated that the Plan Commission should review other communities before any recommendation is made.

Chairman Eisterhold asked if there was anyone in the audience who wanted to comment. There was none.

Motion to continue to October 2, 2013 made by Commissioner Fishman and Seconded by Commissioner Auerbach. Motion approved 6-0.

Informal request by Chairman Eisterhold to place on the Agenda Fence Requirements.

### **VII. Driveway Requirements – Zoning Code Text Amendment**

*(Continued from June 5, 2013, May 1, 2013, March 6, 2013 and February 6, 2013)*

#### **Request: Text Amendment to Consider Residential Driveway Requirements**

Commissioner Sampen left the meeting.

Development Manager Cook indicated that driveway regulations are split between the Zoning Code and Municipal Code. Staff wants to bring the Municipal Code requirements into the Zoning Code. Therefore, all driveway requirements would be in one document. If relief is sought for Village requirements currently, petitioner would need to go to the Zoning Board of Appeals and Village Board. If this changes then petitioner would only need to seek relief from the Village Board and a public hearing is not needed. All relief requests for driveways would follow the same process.

Chairman Eisterhold asked if there was anyone in the audience who wanted to comment. There was none.

Motion to incorporate the driveway requirements found only in the Municipal Code into the Zoning Ordinance as to be determined by the Village Attorney per best practice made by Commissioner Auerbach. Seconded by Commissioner Goldfein. Motion approve 5-0.

### **VIII. PUBLIC HEARING: Final Approval Procedure of Planned Unit Development – Zoning Code Text Amendment**

*(Continued from June 5, 2013, May 1, 2013, March 6, 2013 and February 6, 2013)*

#### **Request: Text Amendment to Consider Requiring Public Hearing for Final Approval of Planned Unit Developments**

Development Manager Cook explained all public hearing approvals require a full application. Including, notices, letters, applications, payments and hearings. The full PUD process has no notifications, letters, or notice in the paper. Staff seeks consideration of a text amendment to modify the Final PUD approval process to require a fully noticed public hearing. Final PUD review is not necessarily routine in nature and as a result staff

August 7, 2013

seeks the Plan Commission's input on requiring a Final PUD approval to be a public hearing. If requested, staff will conduct research of nearby communities to determine best practice.

Commissioner Fishman indicated he would like to see best practice and information from other communities.

Chairman Eisterhold asked if there was anyone in the audience who wanted to comment. There was none.

Motion to continue to November 6, 2013 made by Commissioner Yohanna. Seconded by Commissioner Auerbach. Motion approved 5-0 Aye.

**IX. OTHER BUSINESS:**

Public Hearing Procedures will discuss at a future meeting.

**X. PUBLIC COMMENT:** None.

**XI. ADJOURNMENT:**

The next Plan Commission public hearing will be held on September 11, 2013. Hearing no further business, Motion to adjourn made by Commissioner Yohanna. Seconded by Commissioner Fishman. Motion approved unanimously 5-0. Meeting adjourned at 9:40 PM.

Respectfully submitted,

Lauren Wolf  
Community Development Department Coordinator

**Agenda Item #4**



**Staff Report  
Plan Commission  
September 11, 2013**

**Subject Property:**

4007 West Touhy Avenue

**Zoning District:**

B2 General Business District

**Petitioner:**

The Private Bank, Contract Purchaser and  
Next Touhy, LLC, Property Owner

**Nature of Request:** The petitioner is seeking special use approval for a drive-through in conjunction with a bank and certain variations to construct a new building and off-street parking improvements.



**Requested Action:** Special Use for Facility with drive-through as per Table 4.01.1 and Variations sought: **1)** from Article VI, Section 6.14(1) to permit less than the required minimum perimeter landscape width and minimum screening; **2)** from Article VII, Section 7.06(5) to permit parking in the front and corner side yard; **3)** from Article VI, Section 6.14(2) to permit the off-street parking area to not have the required minimum interior landscaping; and **4)** from Article IV, Section 4.07(8)b to permit less than the required stacking for the drive-through facility.

**Notification:** Notice in Lincolnwood Review dated August 22, 2013, Public Hearing Sign Installed at 4007 West Touhy Avenue, and Mailed Legal Notices Dated August 19, 2013 to Properties within 250 Feet.

**Summary of Request**

The Private Bank (petitioner) and Next Touhy, LLC (owner) seeks a Special Use in the B2 General Business District to permit a drive-through facility in conjunction with a bank. Banks are classified as a Permitted use in the B2 District however a drive-through, regardless of the primary use, requires Special Use approval.

The petitioner proposes to demolish the existing vacant structure that was once used as an automobile service and repair station, and construct a new 2,500 square foot bank with a drive-through. The Private Bank also proposes an improved off-street parking area with new

perimeter landscape areas and increased buffer to the residential property to the south. The petitioner proposes to utilize the existing curb cuts off of Touhy Avenue and Crawford Avenue for access. Based on the scope of property improvements, certain variations are required to approve the proposed site plan in addition to the Special Use for the drive-through.

The Village of Lincolnwood through Home Rule authority has created a process in which the Plan Commission considers zoning variations when requested along with Special Use requests. This is intended to streamline the hearing process for the benefit of the applicant.

Special Use to Approve a Facility with a Drive-Through

Below is a summary of drive-through facilities in the community. As you can see, the majority are associated with banks. Additionally, based on staff’s research, all but one bank in the community has a drive-through. Brickyard Bank appears to be the only bank in Lincolnwood without a drive-through facility.

<b>Banks w/ Drive-Through</b>	<b>Address</b>
Republic Bank	4433 West Touhy Avenue
MB Financial	4010 West Touhy Avenue
Bank Financial	3443 West Touhy Avenue
MB Financial	6401 North Lincoln Avenue
Liberty Bank	6666 North Lincoln Avenue
Charter One	6677 North Lincoln Avenue
<b>Other Uses w/ Drive-Through</b>	<b>Address</b>
Walgreens	6770 North Lincoln Avenue
CVS	3950 West Devon Avenue
CVS	7179 North Lincoln Avenue
Jaffa Bagels (Not Opened)	3300 West Devon Avenue

Variations

The petitioner also seeks several variations needed in order to redevelop the property. The bulk of the variations relate to the off-street parking area and the perimeter landscape areas. The petitioner has modified the building design to eliminate the requested variation relative to building materials. Below is a summary of the requested variations.

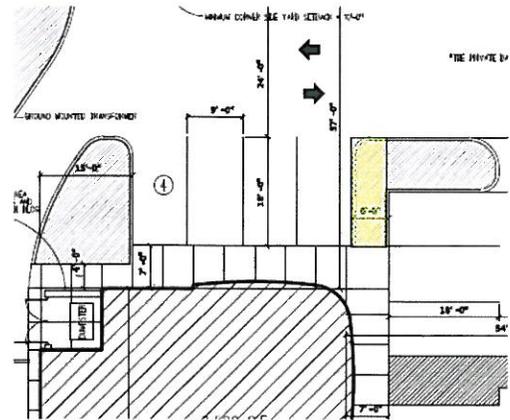
Off-Street Parking and Landscape Variations

The petitioner seeks to use the parking area in the current location which does comply with the off-street parking location requirements. The Zoning Code does not permit off-street parking to be located in the front or corner side yards. The subject property is a corner lot with the Crawford Avenue lot line being the front lot line and the Touhy Avenue lot line is the corner side yard. Therefore, the Zoning Code prohibits parking between the building and both the Crawford Avenue and Touhy Avenue lot lines.

The petitioner seeks relief from the perimeter and interior landscape requirements. These standards include minimum perimeter landscape and minimum interior landscape (landscape islands). The new off-street parking area incorporates landscape areas which the existing off-street parking area does not include. While additional landscape areas are to be introduced to the site as part of the proposed redevelopment, the scope of the redevelopment requires several variations. The redeveloped off-street parking area encroaches into the Crawford Avenue

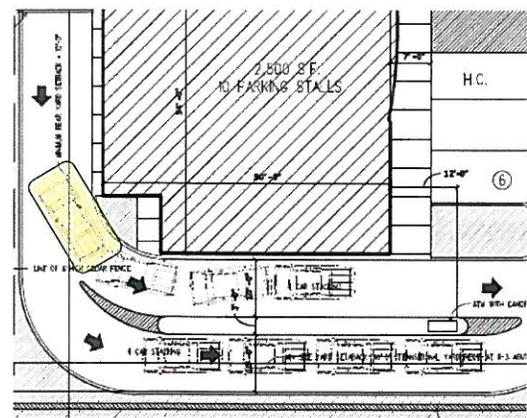
eight foot perimeter landscape area. The petitioner has provided a continuous row of perimeter screening within both perimeter landscape areas adjacent to Crawford Avenue and Touhy Avenue.

The renovated off-street parking includes interior landscape areas however not in a manner wholly compliant with the Zoning Code standards for interior landscape areas. More specifically, the row of parking along the north side of the building is required to have landscape islands at the end of each row equal in area to a parking space. The landscape island at the east end of this row of parking is substandard in area. This landscape island measures 6 feet by 18 feet rather than the code required 9 feet by 18 feet. The landscape island is highlighted for clarity.



Drive-Through Stacking Variation

The petitioner proposes two drive-through stations at the south end of the proposed bank. Each drive-through per the Zoning Code is required to have sufficient stacking space of four vehicles at each bay window or machine. The site plan as proposed shows a fourth vehicle stacked at one of the two drive-through stations. This fourth vehicle stacked obstructs the second drive-through station. The condition is highlighted to the right for clarity.



Recent Similar Requests

The current property owner, Next Touhy LLC requested consideration of variations for the purpose of redeveloping and reusing the existing building and off-street parking area. In March 2013 the Zoning Board of Appeals considered Next Touhy LLC’s request on six variations including:

1. Permit less than minimum perimeter landscape width and minimum screening of the off-street parking area;
2. Permit off-street parking to the front and corner side yard;
3. Permit off-street parking to have less than the required minimum interior landscaping;
4. Permit less than required minimum foundation planting (withdrawn at ZBA hearing);
5. Permit less than the minimum drive aisle width for an aisle accessing off-street parking; and
6. Permit the existing building to be within the required side yard setback.

The ZBA deliberated the proposed site plan and recommended several modifications to the proposed site plan. The ZBA unanimously recommended approval of the variations subject to 1) additional landscape screening at the northeast corner of the property, 2) additional tree added at northeast corner, 3) a parking space determined to be awkward by the ZBA and the space should be limited to employee only parking, and 4) the final landscape plan should be

reviewed by the Village's Landscape Architect. The ZBA found that the variations were and necessary as a result of improving the property which was non-conforming. The ZBA noted that by renovating and reusing the property there were limited opportunities for full compliance with the Zoning Code. The ZBA also identified that the renovation and reuse plan was in substantially greater compliance than the current condition of the property. The Village Board approved the requested variations and adopted the conditions of approval as recommended by the ZBA.

In 2012 the ZBA considered a request by Meatheads for approval of similar variations at 6734 North Lincoln Avenue for the construction of a new freestanding restaurant and associated off-street parking area. The Village Board granted approval of the requested variations in each case.

### **Conclusion**

The petitioner is seeking a Special Use to permit a drive-through facility in conjunction with a new bank at 4007 West Touhy Avenue. The variation requests can be summarized as off-street parking landscape and design, off-street parking location, and drive-through stacking variations. As originally submitted, the petitioner required a variation to permit the proposed structure to be constructed from a material not included on the list of "high quality materials". Since the application the petitioner has revised the building design which is compliant with the current standards. Therefore, that requested variation is no longer necessary. No inquiries from the public were received relative to this request.

If the Plan Commission recommends approval of the request by Private Bank, staff suggests that a condition of approval should be to revoke Ordinance No. 2013-3048 which granted variations for the renovation and reuse of the existing building at 4007 W. Touhy Avenue.

### **Documents Attached**

1. Revised Building Elevations
2. Public Hearing Application Packet – February 15, 2013
  - a. Public Hearing Special Use and PUD Application
  - b. Special Use Standards
  - c. Public Hearing Variation Application
  - d. Variation Standards
  - e. Stormwater Calculations
  - f. Proof of Ownership
  - g. Photographs
  - h. Traffic Impact Study
  - i. Building Elevations
  - j. Plat of Survey
  - k. Landscape Plan
  - l. Tree Inventory
  - m. Proposed Site Plan
  - n. Proof of Ownership
  - o. Proposed Signs
  - p. Photometric Plan



### SUBJECT PROPERTY

Property Address: 4007 West Touhy Avenue

Permanent Real Estate Index Number(s): 10-34-205-020-0000

Zoning District B2 Lot Area: 16,800 square feet

List all existing structures on the property. Include fencing, sheds, garages, pools, etc.

1700 square foot structure formerly used as an automobile service and repair station

Are there existing development restrictions affecting the property?  Yes  No  
(Examples: previous variations, conditions, easements, covenants)

If yes, describe: Previous variation granted to Next Realty, LLC for parking

### REQUESTED ACTION

Special Use - Non-Residential

Planned Unit Development (PUD)

Special Use - Residential

Other

### PROJECT DESCRIPTION

Describe the Request and Project: Approval of a special use for a drive-thru facility in  
conjunction with the development of a branch of The Private Bank

### PROPERTY OWNER/PETITIONER INFORMATION

#### Property Owner(s):

Name: (List all beneficiaries if Trust): Next Touhy, LLC

Address: c/o Next Realty, LLC, Attn: Marc Blum 400 Skokie Blvd., Northbrook, IL 60062

Telephone: ( 847 ) 881-2002 Fax: ( 847 ) 881-2062 E-mail Address: \_\_\_\_\_

#### Petitioner (if different from owner):

Name: The Private Bank Relationship to Property: Contract Purchaser

Address: Attn: Kelly Stradinger 120 S. LaSalle Street, Chicago, IL 60603

Telephone: ( 312 ) 564-6029 Fax: ( \_\_\_\_\_ ) \_\_\_\_\_ E-mail Address: kstradinger@theprivatebank.com





# VILLAGE OF LINCOLNWOOD

## COMMUNITY DEVELOPMENT DEPARTMENT

### SPECIAL USE STANDARDS

To be approved, each special use request must meet certain specific standards. These three standards are listed below. After each listed standard, explain how the special use request satisfies the listed standard. Use additional paper if necessary.

1. Please explain how the use is necessary for the public convenience at this location, and the subject property is deemed suitable for the use. (Please explain in detail)  
See attached document.

---

---

---

---

---

---

---

---

---

---

2. Please explain how the use is so designed, located and proposed to be operated that the public health, safety and welfare will be protected.  
See attached document.

---

---

---

---

---

---

---

---

---

---

3. Please explain how this use would not cause substantial injury to the value of other property in the neighborhood in which it is located.  
See attached document.

---

---

---

---

---

---

---

---

---

---

4. The special use is consistent with the goals and policies of the Comprehensive Plan.  
See attached document.

---

---

---

---

---

---

---

---

---

---

5. The special use would not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the underlying zoning district.  
See attached document.

---

---

---

---

---

---

---

---

---

---

6. Please explain how the special use is so designed to provide adequate utilities, access roads, drainage, or necessary facilities.  
See attached document.

---

---

---

---

---

---

---

---

---

---

7. Please explain how the special use is so designed to provide ingress and egress to minimize traffic congestion on public streets.  
See attached document.

---

---

---

---

---

---

---

---

---

---

**4007 W. TOUHY AVENUE  
PUBLIC HEARING APPLICATION  
SPECIAL USE STANDARDS**

- 1. Please explain how the use is necessary for the public convenience at this location, and the subject property is deemed suitable for the use. (Please explain in detail)**

The use of a branch bank with drive-thru facilities is appropriate for the southwest corner of Touhy Avenue and Crawford Street because the high traffic nature of these streets lends itself to a convenience-oriented bank facility where the bank's customers (and the general public for the ATM) shall have easy access to necessary and useful financial transactions.

- 2. Please explain how the use is so designed, located and proposed to be operated that the public health, safety and welfare will be protected.**

The public health, safety and welfare are protected by many features of the proposed site design. First, the proposed bank use will generate less traffic than the previous service station use as well as less traffic than the other service stations at this intersection. The Touhy access point will be a full service access point, which according to Petitioner's Traffic Engineers, KLOA, Inc., will have a volume of no more than eight (8) cars per hour turning left on to westbound Touhy. The access point on to Crawford is by definition a "right in, right out" entrance due to the existing landscaped center median at this location. In addition, the drive-thru lanes can stack seven cars total: four for the ATM and three at the manned transaction window, and ten parking spaces are provided for the bank's employees and customers, which shall be more than adequate.

- 3. Please explain how this use would not cause substantial injury to the value of other property in the neighborhood in which it is located.**

The environs surrounding the property in questions is both commercial and residential in character. To the west of the proposed bank is a Jiffy Lube auto care facility. To the south of the proposed bank, there is a multifamily residential. The residential use will be protected from automobile noise from cars entering the drive-thru by an eight-foot masonry wall. Because a bank is a more upscale use than the previous automobile service and repair station and the adjoining automobile-related use, petitioner believes that the location of the bank at the Touhy-Crawford Intersection will enhance property values in the immediate area.

- 4. The special use is consistent with the goals and policies of the Comprehensive Plan.**

A bank use is a permitted use in the Village's B-2 Zoning District and consistent with the Comprehensive Plan.

5. **The special use would not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the underlying zoning district.**

The drive-thru, if approved, would not impede development of surrounding properties in any way. It would not disrupt utilities, traffic flow or create any form of nuisance.

6. **Please explain how the special use is so designed to provide adequate utilities, access roads, drainage, or necessary facilities.**

The proposed circulation design permits easy access from both Touhy and Crawford to the drive-thru facility with a circular traffic pattern that has two-way traffic in front of the bank and to the access points to Touhy and Crawford, but creates one-way traffic around the rear of the bank through the proposed drive-thru facility. Given the expected traffic flow through the drive-thru and the time of average transactions, Petitioner believes that the stacking of three cars to the manned transaction window and four to the ATM will be sufficient to handle the number of cars waiting to use the drive-thru facility.

7. **Please explain how the special use is so designed to provide ingress and egress to minimize traffic congestion on public streets.**

The stacking of cars in the drive-thru lanes will prevent any blockage of the public way.



### SUBJECT PROPERTY

Property Address: 4007 West Touhy Avenue

Permanent Real Estate Index Number(s): 10-34-205-020-0000

Zoning District B2 Lot Area: 16,800 square feet

List all existing structures on the property. Include fencing, sheds, garages, pools, etc.

1700 square foot structure formerly used as an automobile service and repair station

Are there existing development restrictions affecting the property?  Yes  No  
(Examples: previous variations, conditions, easements, covenants)

If yes, describe: Previous variation granted to Next Realty LLC for parking

### REQUESTED ACTION

- |   |  |
|---|--|
| <input type="checkbox"/> Variation - Residential                | <input type="checkbox"/> Variation - Signs/Special Signs |
| <input checked="" type="checkbox"/> Variation - Non-Residential | <input type="checkbox"/> Minor Variation                 |
| <input type="checkbox"/> Variation - Off-Street Parking         | <input type="checkbox"/> Other                           |
| <input type="checkbox"/> Variation - Design Standards           |  |

### PROJECT DESCRIPTION

Describe the Request and Project: See attached document.

### PROPERTY OWNER/PETITIONER INFORMATION

#### Property Owner(s):

Name: (List all beneficiaries if Trust): Next Touhy, LLC

Address: c/o Next Realty, LLC, Attn: Marc Blum 400 Skokie Blvd., Northbrook, IL 60062

Telephone: ( 847 ) 881-2002 Fax: ( 847 ) 881-2062 E-mail Address: \_\_\_\_\_

#### Petitioner (if different from owner):

Name: The Private Bank Relationship to Property: Contract Purchaser

Address: Attn: Kelly Stradinger 120 S. LaSalle Street, Chicago, IL 60603

Telephone: ( 312 ) 564-6029 Fax: ( \_\_\_\_\_ ) \_\_\_\_\_ E-mail Address: kstradinger@theprivatebank.com

**REQUIRED ATTACHMENTS \***

Check all documents that are attached:

Plat of Survey	<u>    X    </u>
Site Plan	<u>    X    </u>
Proof of Ownership	<u>    X    </u>
Floor Plans	<u>          </u>
Elevations	<u>    X    </u>
Applicable Zoning Worksheet	<u>  N/A  </u>
Photos of the property	<u>    X    </u>
PDF files of all drawings	<u>    X    </u>

**For Office Use Only**

Fee: \_\_\_\_\_ Deposit: \_\_\_\_\_

Date Received: \_\_\_\_\_

Checked By: \_\_\_\_\_

The article(s), section(s) and paragraph(s) of the Village of Lincolnwood Zoning Ordinance from which the Action is being sought:

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

*\*The above documents are required for all applications. The Zoning Officer may release an applicant from specific required documents or may require additional documents as deemed necessary.*

**COST REIMBURSEMENT REQUIREMENT**

The Village requires reimbursement of certain out-of-pocket costs incurred by the Village in connection with applications for zoning approvals and relief. These costs include, but are not limited to, mailing costs, attorney and engineer costs, and other out-of-pocket costs incurred by the Village in connection with this application. In accordance with Section 5.02 of the Village of Lincolnwood Zoning Ordinance, both the petitioner and the property owner shall be jointly and severally liable for the payment of such out-of-pocket costs. Out-of-pocket costs incurred shall be first applied against any hearing deposit held by the Village, with any additional sums incurred, to be billed at the conclusion of the hearing process.

Invoices in connection with this application shall be directed to:

Name:     The Private Bank      
Address:     120 S. LaSalle Street      
City, State:     Chicago, IL 60603    

**ATTESTMENT AND SIGNATURE**

I hereby state that I have read and understand the Village cost reimbursement requirement, as well as the requirements and procedures outlined in Article V of the Village Zoning Ordinance, and I agree to reimburse the Village within 30 days after receipt of an invoice therefor. I further attest that all statements and information provided in this application are true and correct to the best of my knowledge and that I have vested in me the authority to execute this application.

PROPERTY OWNER

    Marc Blum         8/9/13      
Signature Date

    Marc Blum      
PRINT NAME

PETITIONER (If different than property owner)

    Kelly Stradinger         August 8, 2013      
Signature Date

    KELLY STRADINGER      
PRINT NAME



**VILLAGE OF LINCOLNWOOD**  
COMMUNITY DEVELOPMENT DEPARTMENT

**VARIATION STANDARDS**

To be approved, each variation request must meet certain specific standards. These eight standards are listed below. After each listed standard, explain how your variation request satisfies the listed standard. Use additional paper if necessary.

1. The requested variation is consistent with the stated intent and purposes of the Zoning Ordinance and the Comprehensive Plan.

See attached document.

---

---

---

---

---

---

---

2. The particular physical surroundings, shape or topographical conditions of the subject property would bring a particular hardship upon the owner, as distinguished from a mere inconvenience, if the strict letter of this Zoning Ordinance is enforced.

See attached document.

---

---

---

---

---

---

---

3. The conditions upon which the petition for the variation is based would not be applicable generally to other property within the same Zoning District.

See attached document.

---

---

---

---

---

---

---

4. The variation is not solely and exclusively for the purpose of enhancing the value of or increasing the revenue from the property.

See attached document.

---

---

---

---

---

---

---

**VARIATION STANDARDS (continued)**

5. The alleged difficulty or hardship has not been created by any person presently having an interest in the property.

See attached document.

---

---

---

---

---

---

6. The granting of the variation will not be detrimental to the public welfare or injurious to other property or improvements in the neighborhood in which the property is located.

See attached document.

---

---

---

---

---

---

7. The variation granted is the minimum change to the Zoning Ordinance standards necessary to alleviate the practical hardship on the subject property.

See attached document.

---

---

---

---

---

---

8. The proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the danger of fire, or otherwise endanger the public safety, or substantially diminish or impair property values within the neighborhood.

See attached document.

---

---

---

---

---

---

**SIGN VARIATION STANDARDS**

*For all Sign Variation and/or Special Sign requests the Applicant shall also complete Questions 9-12.*

9. The proposed variation is consistent with the statement of purpose set forth in Section 11.01 of the Zoning Ordinance.

N/A

---

---

---

---

---

---

10. The proposed sign complies with any additional standards or conditions set forth in Article XI of the Zoning Ordinance.

N/A

---

---

---

---

---

---

11. The proposed sign will substantially enhance the architectural integrity of the building or other structure to which it will be attached, if any.

N/A

---

---

---

---

---

---

12. The proposed sign conforms with the design and appearance of nearby structures and signs.

N/A

---

---

---

---

---

---



**VILLAGE OF LINCOLNWOOD**  
**COMMUNITY DEVELOPMENT DEPARTMENT**

**PUBLIC HEARING FEES & DEPOSITS SCHEDULE**

**Plan Commission**

Hearing Type	Hearing Fee*	Hearing Deposit**
Special Use - Non Residential Property	\$500	\$2,000
Special Use - Residential Property	\$250	NA
Text Amendment	\$500	\$2,000
Map Amendment	\$500	\$2,000
Planned Unit Development (PUD) 2 to 5 acres	\$1,250	\$10,000
Planned Unit Development (PUD) 5 to 10 acres	\$2,500	\$10,000
Planned Unit Development (PUD) Over 20 acres	\$3,000	\$10,000
Minor Subdivision	\$250	NA
Major Subdivision	\$500	\$2,000

**Zoning Board of Appeals**

Hearing Type	Hearing Fee*	Hearing Deposit**
Major Variation - Non Residential Property	\$500	NA
Major Variation - Residential Property	\$250	NA
Variation - Off-Street Parking	\$500	NA
Variation - Design Standards	\$250	NA
Minor Variation	\$125	NA
Sign Variation/Special Signs	\$500	NA

\* Hearing fees are non-refundable.

\*\* Hearing Deposits shall be applied to out-of-pocket expenses incurred by the Village as the result of the public hearing process. If additional costs are incurred, or if no deposit is provided, such out-of-pocket expenses will be billed directly to the applicant.

**4007 W. TOUHY AVENUE  
PUBLIC HEARING APPLICATION  
VARIATIONS REQUESTED**

In connection with Petitioner's proposed development of a branch bank at 4007 West Touhy Avenue, Petitioner requests the following variations:

1. Variation from Section 4.07(8)(b) to permit vehicle stacking for one drive-thru lane of three cars instead of the required four;
2. Variation from Section 706(5) to permit parking in required front and corner side yards;
3. Variation from Section 6.14(1) to permit a 2 foot width of perimeter landscape;
4. Variation from Section 6.14(2) to permit a reduced landscape island area;
5. Variation from Section 6.15 to permit zero foundation plantings; and
6. Variation from Section 6.04(3) to permit "rain-screen" or "ventilated façade" system of a terra cotta or cementitious exterior cladding material in lieu of masonry.

**4007 W. TOUHY AVENUE  
PUBLIC HEARING APPLICATION  
VARIATION STANDARDS:  
PARKING AND LANDSCAPING VARIATIONS APPLICATION**

- 1. The requested variation is consistent with the stated intent and purposes of the Zoning Ordinance and the Comprehensive Plan.**

The requested variations are consistent with the intent and purposes of the Zoning Ordinance and the Comprehensive Plan because the variations would permit the development of a small site with a desirable land use.

- 2. The particular physical surroundings, shape or topographical conditions of the subject property would bring a particular hardship upon the owner, as distinguished from a mere inconvenience, if the strict letter of this Zoning Ordinance is enforced.**

The property is a corner lot consisting of only 16,800 square feet. To locate a branch bank with a double drive-thru facility and ten parking spaces has required that one stacking lane hold one less car than required, that portions of the parking spaces reside in required yards, and that not all of the otherwise required landscape areas could be included. Applying the strict letter of the Zoning Ordinance to a site this small would be a significant hardship on the owner and prevent the development of the property for the bank use.

- 3. The conditions upon which the petition for the variation is based would not be applicable generally to the other property within the same Zoning District.**

Due to the small size of the property and it being a corner lot, the conditions would not apply generally to other properties in the B-2 Zoning District.

- 4. The variation is not solely and exclusively for the purpose of enhancing the value of or increasing the revenue from the property.**

The variations are sought not solely for enhancing the value of or revenue from the property. The variations are sought to provide for the development of a desirable use of the property and to remove an unattractive automobile service and repair station that has stood vacant for some time.

- 5. The alleged difficulty or hardship has not been created by any person presently having an interest in the property.**

The condition of the property has not changed since the property owner purchased it in 2012.

- 6. The granting of the variation will not be detrimental to the public welfare or injurious to other property or improvements in the neighborhood in which the property is located.**

The proposed site plan accommodates the automobile stacking, landscaping and setback requirements of the Zoning Ordinance to the extent possible and still maintains 10 parking spaces and the double drive-thru facility. The front and corner side yards are the only location for the parking spaces, the location of which facilitates safe movement for automobiles and pedestrians. The reduction in landscape areas are minimal and will not have any material, visual impact on surrounding properties. In particular, one landscape island area is slightly less than the Zoning Ordinance requirement of 9 by 18 feet because the island's curved ends are necessary to facilitate the safe movement of automobiles.

- 7. The variation granted is the minimum change to the Zoning Ordinance standards necessary to alleviate the practical hardship on the subject property.**

Within the constraints of the site, the variations requested by Petitioner are the minimum necessary for Petitioner to develop the site with a branch bank and drive-thru facility. The variations are necessary for Petitioner as it seeks to revitalize the property.

- 8. The proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the danger of fire, or otherwise endanger the public safety, or substantially diminish or impair property values within the neighborhood.**

The site plan proposes a layout that will maintain the safety of the public. The proposed variations will have no impact on adjacent properties, will not cause an increase in the risk of fire, and will not impair property values within the neighborhood.

**4007 W. TOUHY AVENUE  
PUBLIC HEARING APPLICATION  
VARIATION STANDARDS:  
BUILDING EXTERIOR MATERIAL VARIATION APPLICATION**

- 1. The requested variation is consistent with the stated intent and purposes of the Zoning Ordinance and the Comprehensive Plan.**

The purpose of Section 6.04(3) of the Zoning Ordinance is to have commercial building facades be made of quality masonry. Petitioner seeks to use a “rain-screen” or “ventilated façade” system of a terra cotta or cementitious material, which is shown on the elevations submitted with this petition. Petitioner desires to use one of these materials for the building exterior because, as installed, they allow an air gap between the wall and the cladding which does not transmit heat or cold from the cladding, resulting in higher energy efficiency for the building. Also, when the material is used in darker brown or red colors, it gives an impression of an 8 inch tall masonry block but with strong, modern horizontal lines.

- 2. The particular physical surroundings, shape or topographical conditions of the subject property would bring a particular hardship upon the owner, as distinguished from a mere inconvenience, if the strict letter of this Zoning Ordinance is enforced.**

In a climate that experiences both extremes of heat and cold, the energy efficiency of the proposed exterior is a desirable upgrade in energy efficiency over standard masonry without any loss of visual character.

- 3. The conditions upon which the petition for the variation is based would not be applicable generally to the other property within the same Zoning District.**

The variation would not be applicable to all other improved properties within the B-2 Zoning District; it is possible that future owners building structures could seek a similar variation.

- 4. The variation is not solely and exclusively for the purpose of enhancing the value of or increasing the revenue from the property.**

Petitioner desires to use the “rain-screen” or “ventilated façade” system of a terra cotta or cementitious material because they are green building products that promote energy efficiency and provide a crisp, modern look for Petitioner’s branch bank.

- 5. The alleged difficulty or hardship has not been created by any person presently having an interest in the property.**

The desire to create energy-efficient buildings is not unique to Petitioner.

6. The granting of the variation will not be detrimental to the public welfare or injurious to other property or improvements in the neighborhood in which the property is located.

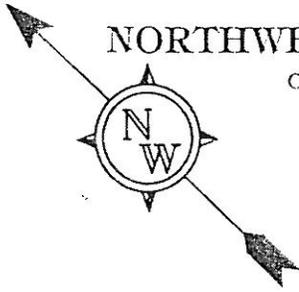
The granting of the variation will enhance the public welfare by saving energy and creating a beautiful building where a metal automobile service and repair station once stood.

7. The variation granted is the minimum change to the Zoning Ordinance standards necessary to alleviate the practical hardship on the subject property.

Petitioner seeks the variance only for those parts of the building without a glass curtain wall. Accordingly, the variation as proposed is the minimum change required.

8. The proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the danger of fire, or otherwise endanger the public safety, or substantially diminish or impair property values within the neighborhood.

The variation will have no impact on light and air, will cause no increase in the danger of fire, will have no impact on public safety and will not impair property values in the neighborhood. The razing of a metal building that used to contain an automobile service and repair station in favor of a new, attractive branch bank will be a positive development for the neighborhood.



# NORTHWESTERN ENGINEERING CONSULTANTS, P.C.

CIVIL ENGINEERS • SITE PLANNERS • LAND SURVEYORS

1110 LAKE COOK ROAD  
BUFFALO GROVE, ILLINOIS 60089

August 9, 2013

Mr. Jim Johnson  
Village Engineer  
Village of Lincolnwood  
6900 N. Lincoln Avenue  
Lincolnwood, IL 60712

Ref: 13-35  
The Private Bank  
Touhy Avenue & Crawford Avenue  
Lincolnwood, IL

Dear Mr. Johnson:

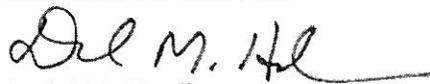
We have prepared and are attaching storm water detention calculations for the proposed The Private Bank project at the southwest corner of Touhy Avenue and Crawford Avenue in Lincolnwood. The calculations show that for the 100-year storm, a total of 4922 cubic feet of detention volume is required for this project per the Lincolnwood ordinance.

Due to ground contamination at the site, and to minimize excavation, we are requesting that a fee be paid in lieu of providing the actual detention volume on the site. A letter discussing the site contamination from the environmental engineer, Apex Companies, LLC, is also attached.

Please do not hesitate to contact us with any questions.

Very truly yours,

NORTHWESTERN ENGINEERING  
CONSULTANTS, P.C.

  
Daniel M. Havlir

DMH/bsb

1335ltr.doc

Northwestern Engineering Consultants, P.C.  
 Civil Engineers • Site Planners • Land Surveyors  
 1110 Lake Cook Road  
 BUFFALO GROVE, ILLINOIS 60089  
 (847) 520-8410  
 FAX (847) 520-8481

JOB 13-35 PRIVATE BANK LINCOLNWOOD, IL  
 SHEET NO. 1 OF 3  
 CALCULATED BY MKS DATE 8-5-13  
 CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_

ILLINOIS PROFESSIONAL DESIGN FIRM LIC# 134-002625 STATE EXPIRES 4-30-15

STORMWATER DISTRIBUTION CALCULATIONS FOR 100 YEAR STORM  
 PRIVATE BANK TOWNSHIP & CRAWFORD LINCOLNWOOD, IL

REF: VILLAGE OF LINCOLNWOOD ORDINANCE CHAPTER  
 12 ARTICLE 6

AREA OF SITE =  $120 \times 140 = 16,800 \text{ SF} = 0.386 \text{ ACRES}$

PER AITA CURB CUT DITCH 1/25/12 SITE IS CURRENTLY 100% IMPERVIOUS

PROVIDED PERMITS UNDER PER CAMBRIDGE & TURTLE CREEK A/C  
 SITE PLAN DATED 7-15-13

WEST SIDE LOT R' (18)(12.5) + (18.5)(13.5) + (4.5)(3) +  
 $\left[ \frac{45^2 - 20^2}{4} \right] + (1.5)(5) + \frac{1}{2}(8)(3) + \left( \frac{21+13}{2} \right)(20) + (21)(14)$   
 = 1712 SF

NORTH AND EAST R' (4.5)(53) + (7.5)(67) +  $\left[ \frac{52^2 - 25^2}{4} \right] = 1104 \text{ SF}$

NE COR BLOCK  $5 \times 17 + 8 \times 17 = 211 \text{ SF}$

SE COR BLOCK  $7 \times 17 = 119 \text{ SF}$

SW COR BLOCK  $7 \times 7 = 49 \text{ SF}$

NW COR BLOCK  $\left( 7 + \frac{14}{2} \right)(17) = 178 \text{ SF}$

MINUS - TREE 6'x6' SIGN 6'x1' MISCARY WALK 10'x1' = 150 SF

TOTAL PERVIOUS AREA UNDER PROPOSED SITE PLAN =  
 $1712 + 1104 + 211 + 119 + 49 + 178 - 150 = 3263 \text{ SF}$

PER ORDINANCE COEFFICIENTS FOR RUNOFF PAIGMENT  
 C = 0.45 FOR GRASS (ADJACENT)

$C = \frac{(0.95)(16,800 - 3263) + (0.45)(3263)}{16,800} = 0.85$

$Q_p (1.60 \text{ in}^2) = 0.15 \times 0.85 \times 16,800 = 0.058$

100 YEAR DISTRIBUTION VOLUME REQUIRED =  $0.113 \text{ AC FT}$  (SEE SHEET 2)  
 = 4922 CUBIC FEET

JOB NO. 13--35  
 NAME PRIVATE BANK LINCOLN  
 DATE 05-Aug-13  
 BY MKS

REQUIRED 100 YEAR DETENTION VILLAGE OF LINCOLNWOOD MWRD METHOD BULLETIN 70

C Avg= 0.850  
 Area= 0.386 ACRES  
 Qr= 0.058 CFS

Time	I 100	Q 100	Qr	Q100-Qr	Storage
0.083	9.950	3.265	0.058	3.207	0.266
0.170	9.130	2.996	0.058	2.938	0.499
0.250	7.480	2.454	0.058	2.396	0.599
0.330	6.480	2.126	0.058	2.068	0.683
0.500	5.140	1.686	0.058	1.629	0.814
0.667	4.290	1.408	0.058	1.350	0.900
0.833	3.750	1.230	0.058	1.172	0.977
1.000	3.270	1.073	0.058	1.015	1.015
1.500	2.520	0.827	0.058	0.769	1.153
2.000	2.050	0.673	0.058	0.615	1.229
3.000	1.480	0.486	0.058	0.428	1.283
4.000	1.190	0.390	0.058	0.333	1.330
5.000	1.000	0.328	0.058	0.270	1.351
6.000	0.870	0.285	0.058	0.228	1.365
7.000	0.770	0.253	0.058	0.195	1.363
8.000	0.690	0.226	0.058	0.168	1.348
9.000	0.630	0.207	0.058	0.149	1.339
10.000	0.580	0.190	0.058	0.132	1.324
11.000	0.540	0.177	0.058	0.119	1.312
12.000	0.500	0.164	0.058	0.106	1.274
13.000	0.490	0.161	0.058	0.103	1.337
14.000	0.470	0.154	0.058	0.096	1.348
15.000	0.440	0.144	0.058	0.086	1.297
16.000	0.400	0.131	0.058	0.073	1.173
17.000	0.380	0.125	0.058	0.067	1.135
18.000	0.360	0.118	0.058	0.060	1.084
19.000	0.350	0.115	0.058	0.057	1.082
20.000	0.330	0.108	0.058	0.050	1.007
21.000	0.320	0.105	0.058	0.047	0.989
22.000	0.310	0.102	0.058	0.044	0.964
23.000	0.300	0.098	0.058	0.041	0.932
24.000	0.290	0.095	0.058	0.037	0.894

Maximum Stored = 1.365 CFS-HR

REQUIRED STORAGE = 0.113 ACRE FEET

Northwestern Engineering Consultants, P.C.  
Civil Engineers • Site Planners • Land Surveyors  
1110 Lake Cook Road  
BUFFALO GROVE, ILLINOIS 60089  
(847) 520-8410  
FAX (847) 520-8481

JOB 13-35 PRIVATE BANK LINCOLNWOOD, IL

SHEET NO. 3 OF 3

CALCULATED BY M/S DATE 3-5-13

CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_

ILLINOIS PROFESSIONAL DESIGN FIRM LIC#184-002695 STATE EXPIRES 4-30-15

CALCULATE RESTRICTOR SIZE FOR 100 YEAR STORM

ASSUME STORMWATER DETENTION WEIR TO BE PROVIDED ON PARKING LOT 6" OVER CATCHBASIN RIMS. ASSUME 4' FROM HIGH WATER LEVEL TO  $\phi$  OF ORIFICE. ACTUAL HEAD SUBJECT TO FINAL ENGINEERING DESIGN.

USE MURDO ORIFICE FORMULA  $C_d = 0.73$  FOR PROJECTING ORIFICE

$$0.058 = C_d A \sqrt{2gh} = 0.73 A \sqrt{2 \times 32.2 \times 4.0}$$

$$A = 0.00495 \text{ FT}^2 = 0.712 \text{ IN}^2 = \pi r^2 \quad r = 0.48 \text{ IN}$$

RESTRICTOR WOULD BE 0.96"  $\phi$  DIAMETER. THIS IS LESS THAN THE VILLAGE'S MAXIMUM 3" RESTRICTOR DIAMETER. PAY FEE IN LIEU OF PROVIDING ON-SITE DETENTION.

**TOTAL PAYMENT DUE**

**\$11,274.63**

By 08/01/13 (on time)

**2012 Second Installment Property Tax Bill**

Property Index Number (PIN) 10-34-205-020-0000 Volume 129 Code 24015 Tax Year (Payable In) 2012 (2013) Township NILES Classification 5-22

IF PAYING LATE PLEASE PAY 08/02/13-09/01/13 \$11,443.75 09/02/13-10/01/13 \$11,612.87 10/02/13-11/01/13 \$11,781.99 LATE PENALTY IS 1.5% PER MONTH, BY STATE LAW

**TAXING DISTRICT BREAKDOWN**

Taxing District	2012 Tax	2012 Rate	2012 %	Pension	2011 Tax
<b>MISCELLANEOUS TAXES</b>					
North Shore Mosquito Abatement	26.03	0.010	0.11%		27.56
Metro Water Reclamation District	963.15	0.370	4.08%	54.66	881.99
Lincolnwood Public Library	957.95	0.368	4.06%	33.84	909.55
<b>Miscellaneous Taxes Total</b>	<b>1,947.13</b>	<b>0.748</b>	<b>8.25%</b>		<b>1,819.10</b>
<b>SCHOOL TAXES</b>					
Oakton Community College Dist 535	570.08	0.219	2.42%		540.22
Community High School District 219	8,475.76	3.256	35.91%	109.33	8,004.03
School District 74	8,762.10	3.366	37.12%	127.55	8,362.34
<b>School Taxes Total</b>	<b>17,807.94</b>	<b>6.841</b>	<b>75.45%</b>		<b>16,906.59</b>
<b>MUNICIPALITY/TOWNSHIP TAXES</b>					
Village of Lincolnwood	2,178.81	0.837	9.23%	585.70	2,075.43
Road & Bridge Niles	0.00	0.000	0.00%		0.00
General Assistance Niles	15.62	0.006	0.07%		13.78
Town Niles	109.33	0.042	0.46%		101.98
<b>Municipality/Township Taxes Total</b>	<b>2,303.76</b>	<b>0.885</b>	<b>9.76%</b>		<b>2,191.19</b>
<b>COOK COUNTY TAXES</b>					
Cook County Forest Preserve District	164.00	0.063	0.69%	5.20	159.86
Consolidated Elections	0.00	0.000	0.00%		68.91
County of Cook	747.10	0.287	3.16%	281.13	614.63
Cook County Public Safety	471.16	0.181	2.00%		443.75
Cook County Health Facilities	164.00	0.063	0.69%		214.98
<b>Cook County Taxes Total</b>	<b>1,546.26</b>	<b>0.594</b>	<b>6.54%</b>		<b>1,502.13</b>
<b>(Do not pay these totals)</b>	<b>23,605.09</b>	<b>9.068</b>	<b>100.00%</b>		<b>22,419.01</b>

NEITHER PARTY SHALL BE RESPONSIBLE FOR PAYING THE TAXES DUE ON THIS PROPERTY.

VENDOR COOCCO ENTITY NTOUHY

GL	DISCRPTION	AMOUNT
2050-0000	12 Pay 13 RET	11,274.63

**TAX CALCULATOR**

2011 Assessed Value	92,783	2012 Total Tax Before Exemptions	23,605.09
2012 Assessed Value	92,783	Homeowner's Exemption	00
2012 State Equalization Factor X	2.8056	Senior Citizen Exemption	00
2012 Equalized Assessed Value (EAV)	260,312	Senior Assessment Freeze Exemption	00
2012 Local Tax Rate X	9.068%	<b>2012 Total Tax After Exemptions</b>	<b>23,605.09</b>
<b>2012 Total Tax Before Exemptions</b>	<b>23,605.09</b>	First Installment	12,330.46
		Second Installment +	11,274.63
		Total 2012 Tax (Payable In 2013)	23,605.09

**IMPORTANT MESSAGES**

- Thank you for your **TOTAL** installment payment of \$12,330.46 on 02-26-13

DATE APPROVAL DUE DATE

PROPERTY REMBURSEMENT

**PROPERTY LOCATION**

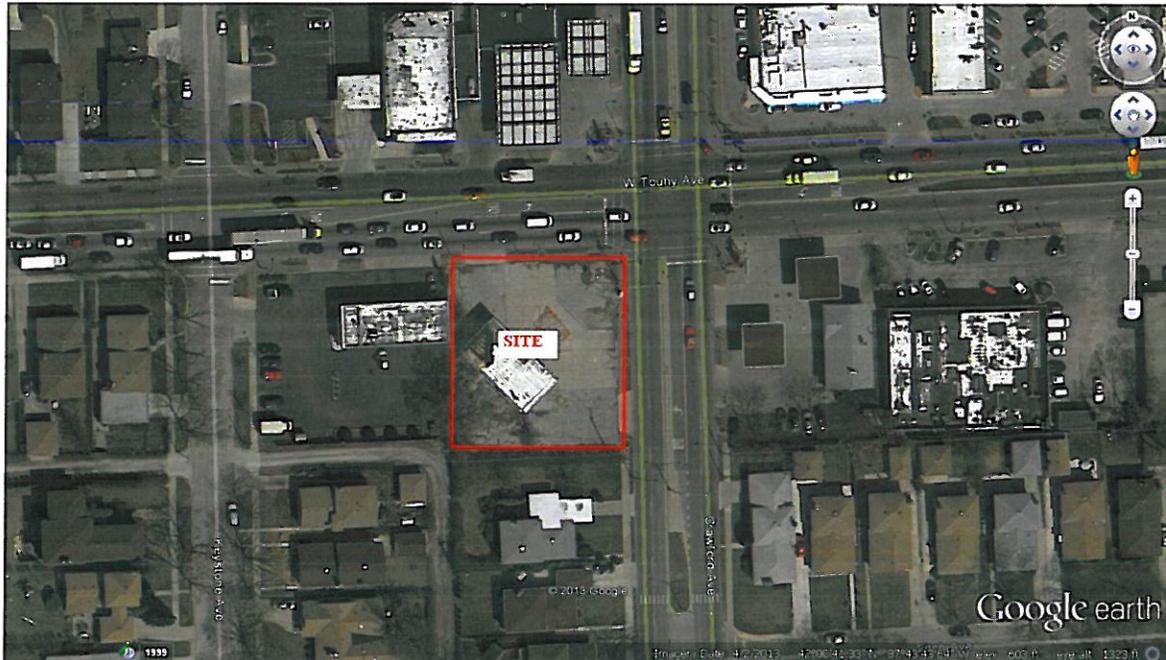
4007 TOUHY AVE  
LINCOLNWOOD IL 60712 2028

**MAILING ADDRESS**

NEXT TOUHY LLC  
400 SKOKIE BLVD #800  
NORTHBROOK IL 60062-7908



# Traffic Impact Study The PrivateBank Lincolnwood, Illinois



Prepared by:



**Kenig, Lindgren, O'Hara, Aboona, Inc.**

**August 8, 2013**

## **Introduction**

This report summarizes the methodologies, results and findings of a site traffic analysis conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for The PrivateBank, a drive-through bank proposed to occupy the southwest quadrant of the intersection of Touhy Avenue and Crawford Avenue in Lincolnwood, Illinois.

The approximate 0.4 acre site is proposed to contain an approximate 2,500 square-foot bank with a two-lane attached drive-through facility. As proposed, the site will provide 10 parking spaces.

**Figure 1** shows the location of the site in relation to the area roadway system. **Figure 2** shows an aerial view of the site area.

The purpose of this study was to examine existing traffic conditions, assess the impact that the proposed development would have on traffic conditions in the area, and determine if any roadway or access improvements are necessary to accommodate development-generated traffic.

## **Existing Conditions**

Existing roadway and traffic conditions near the site were documented based on field visits and traffic counts. The following provides a detailed description of the physical characteristics of the roadways including geometry and traffic control, adjacent land uses and peak hour traffic flows along area roadways.

### **Site Location**

As previously mentioned, the proposed bank will occupy the southwest quadrant of the intersection of Touhy Avenue and Crawford Avenue. Land uses in the area include a gas stations in the northwest and southeast quadrants, a commercial building in the northeast quadrant, Jiffy Lube to the west and residential homes to the south.





**Aerial View of Site**

**Figure 2**

*The PrivateBank  
Lincolnwood, Illinois*

## **Existing Roadway System Characteristics**

The characteristics of the existing roadways near the site are described below.

*Touhy Avenue* is a four-lane east-west roadway with a raised landscape median east of Crawford Avenue and west of Keystone Avenue. Touhy Avenue has a posted speed limit of 35 miles per hour (mph) within the vicinity of the site and prohibits on street parking on both sides of the road. At its signalized intersection with Crawford Avenue, Touhy Avenue provides an exclusive left-turn lane, an exclusive through lane and a shared through/right-turn lane on both approaches. Touhy Avenue is under the jurisdiction of Illinois Department of Transportation (IDOT) and carries a daily traffic volume (ADT) of 26,000 vehicles west of Crawford Avenue and 30,600 vehicles east of Crawford Avenue.

*Crawford Avenue* is a north-south four-lane roadway divided by a landscaped median. At its signalized intersection with Touhy Avenue, exclusive left-turn lanes are provided. Crawford Avenue is under the jurisdiction of Cook County Highway Department and carries an ADT of 13,000 vehicles.

## **Existing Traffic Volumes**

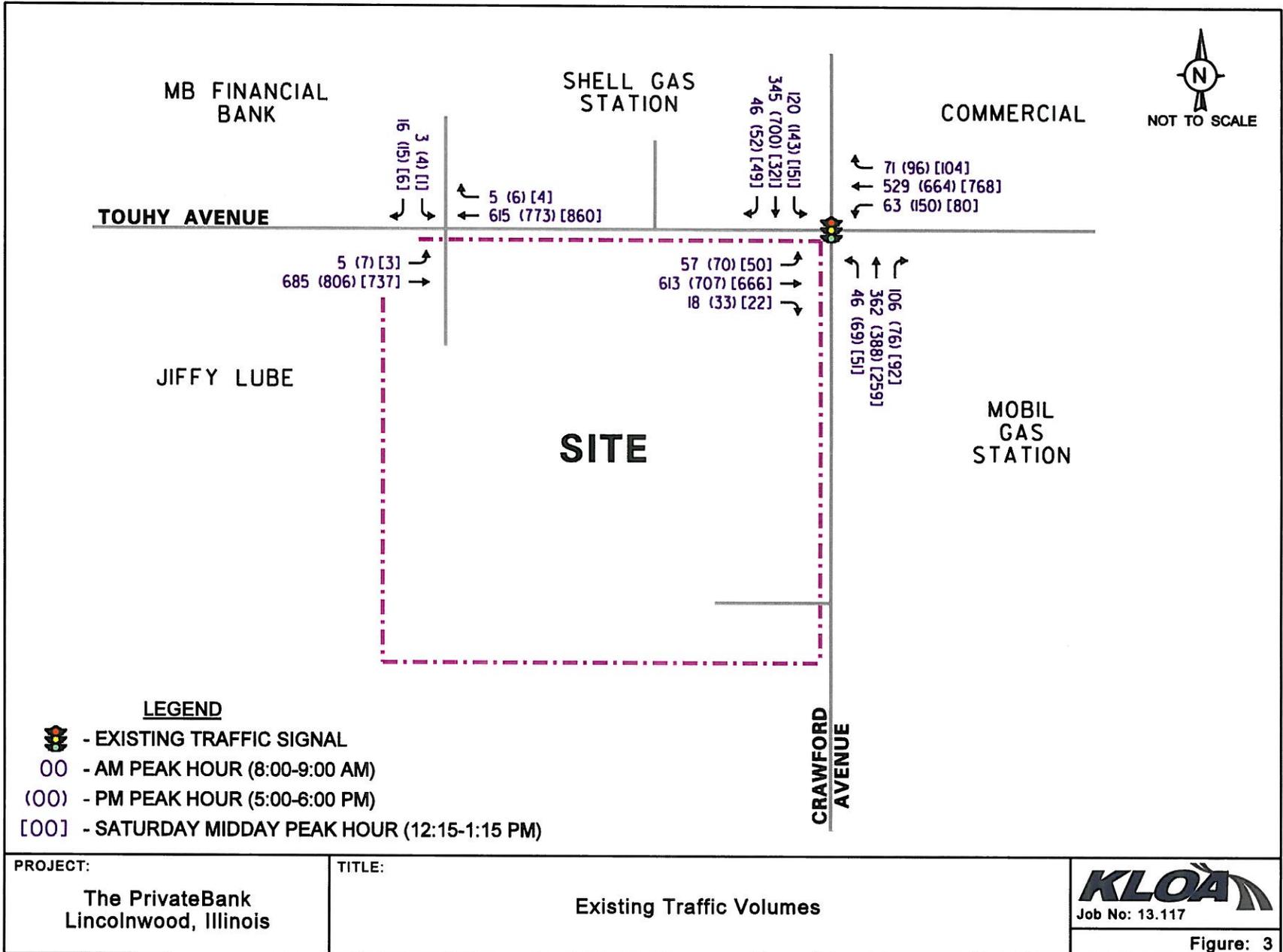
Manual traffic counts were conducted on Thursday, July 18, 2013 during the weekday morning (7:00 to 9:00 A.M.) and weekday evening (4:00 to 6:00 P.M.) and on Saturday, July 20, 2013 between 12:00 Noon and 2:00 P.M. at the following two intersections.

1. Touhy Avenue and Crawford Avenue
2. Touhy Avenue and the Shell Gas Station westerly driveway

The time periods on the weekday represent the peak traffic periods of the adjacent roadway network. The Saturday time period was chosen based on the proposed hours of operation for the bank on a Saturday. The traffic count data indicates that the weekday morning peak hour occurs between 8:00 and 9:00 A.M., the weekday evening peak hour occurs between 5:00 and 6:00 P.M. and the Saturday midday peak hour occurs between 12:15 A.M. and 1:15 P.M. The existing peak hour vehicle traffic volumes for the three identified peak hours are shown in **Figure 3**.

## **Traffic Characteristics of The PrivateBank**

To evaluate the impact of the subject development on the area roadway system, it was necessary to quantify the number of vehicle trips the site will generate during the weekday morning, weekday evening, and Saturday midday peak hours and then determine the directions from which this traffic will approach and depart the site.



## **Proposed Site and Development Plan**

As mentioned, the site currently contains a vacant one-story structure. Access to the site is currently provided off Touhy Avenue and Crawford Avenue. The site is approximately 0.4 acre in size and will contain a 2,500 square-foot drive-through bank and will provide a total of 10 parking spaces. The bank will provide two drive-through lanes located on the south side of the building. The outside lane will be a dedicated ATM only lane.

### **Site Access**

The bank site will utilize the existing access drives serving the property.

#### Access Drive on Touhy Avenue

The full access drive intersects Touhy Avenue near the west property line approximately 90 feet west of the stop bar at the Crawford Avenue intersection. This access drive will continue to allow full movements with one inbound one and one outbound lane. Outbound movements will be under stop sign control.

#### Access Drive on Crawford Avenue

This existing access drive located near the south property line is restricted to right turns only via the landscaped median on Crawford Avenue. The access drive provides one inbound lane and one outbound lane with outbound movements under stop sign control.

### **Directional Distribution of Site Traffic**

The directional distribution was estimated based on the traffic patterns reflected in the existing traffic volumes. The anticipated directional distribution of site traffic is illustrated in **Figure 4**.

### **Site Traffic Generation**

The estimates of traffic to be generated by the proposed development are based upon the proposed land use type and size. The volume of traffic generated was estimated using data published in the Institute of Transportation Engineers (ITE) *Trip Generation Manual*, 9<sup>th</sup> Edition. The total trips anticipated for the proposed bank are shown in **Table 1**. It is important to note that the trip generation shown in Table 1 is for both drive-through operations including the ATM lane and vehicles parking to access the building.

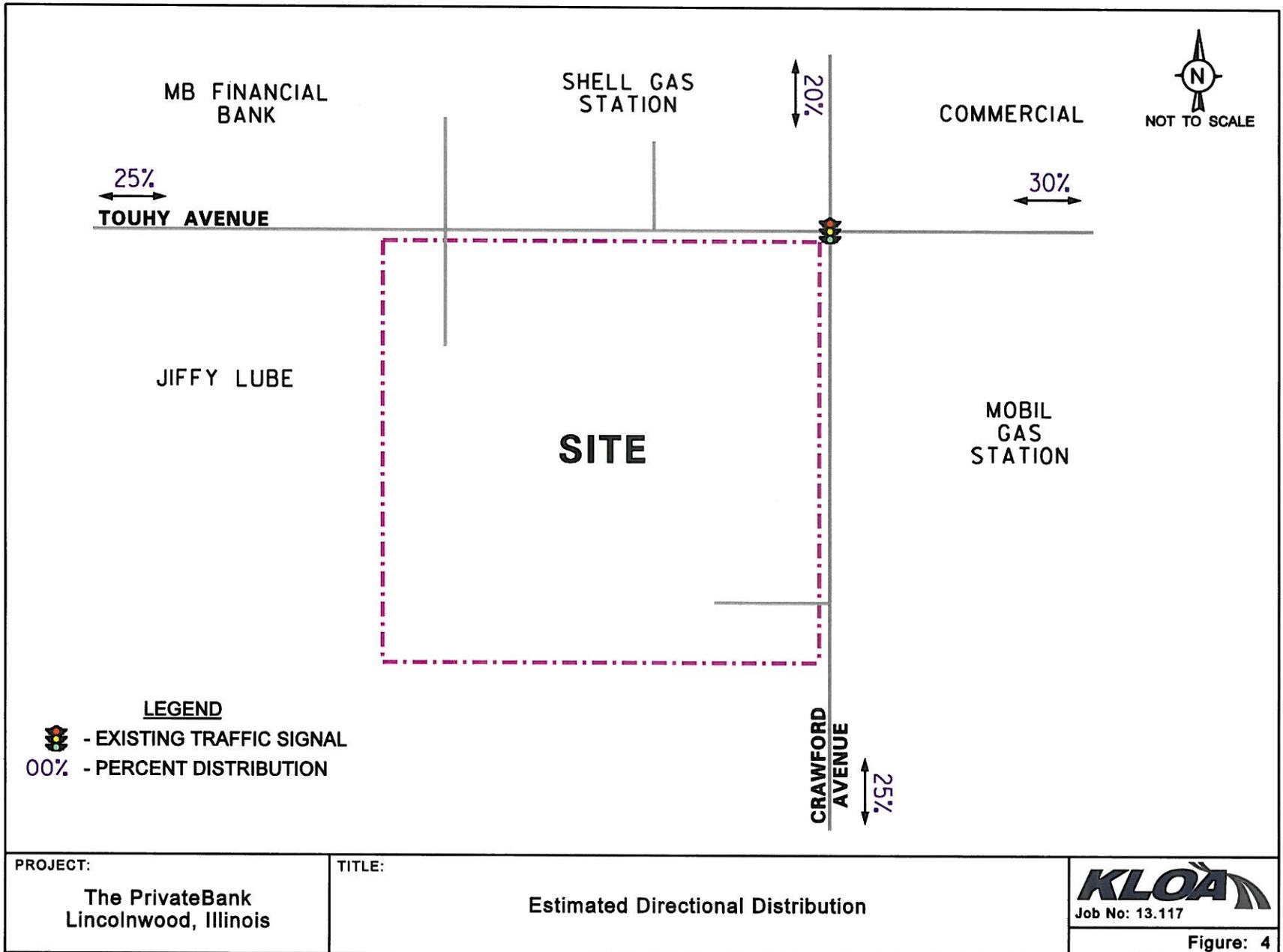


Table 1  
ESTIMATED SITE TRAFFIC VOLUMES

ITE Land-Use Code	Type	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour			Saturday Midday Peak Hour		
		In	Out	Total	In	Out	Total	In	Out	Total
912	Drive- Through Bank	17	30	30	30	30	60	33	33	66

It should be noted that not all trips generated by drive-through banks are new with some diverted from the existing traffic on the roadway system. As such, pass-by reduction is typically applied to banks with drive-through facilities. However, in order to provide for a conservative study, pass-by reduction was not applied to the proposed site-generated traffic volumes.

### Site Traffic Assignment

The peak hour traffic volumes projected for the proposed bank were assigned to the area roadways based on the directional distribution analysis shown in Table 1 and the proposed access driveways. **Figure 5** shows the assignment of the site-generated peak hour traffic volumes.

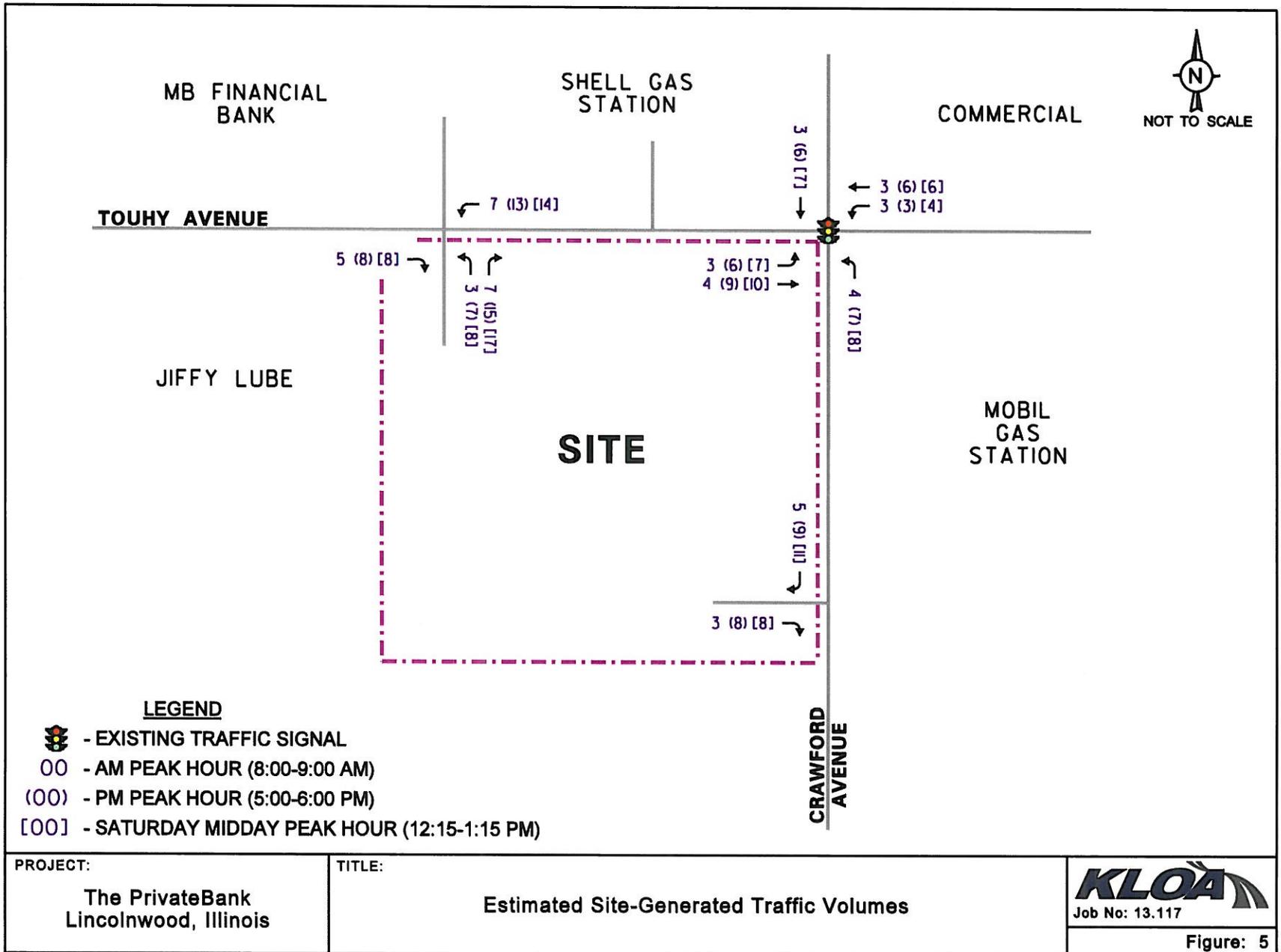
### Total Projected Traffic Conditions

The peak hour traffic volumes generated by the proposed bank (Figure 5) were combined with the background traffic volumes to determine the projected total peak hour traffic volumes. **Figure 6** shows the total traffic volumes for the future conditions. Included in the projected traffic volumes is the growth in background traffic assumed at 0.5 percent and applied over a five-year period to reflect construction plus five design conditions as required by IDOT.

### Traffic Analysis

Traffic analyses were performed for the critical intersections to determine the operation of the existing roadway system, evaluate the impact of the bank and determine the ability of the existing roadway system to accommodate projected traffic demands. Analyses were performed for the weekday morning, weekday evening, and Saturday midday peak hours for both the existing traffic volumes and future projected traffic volumes.

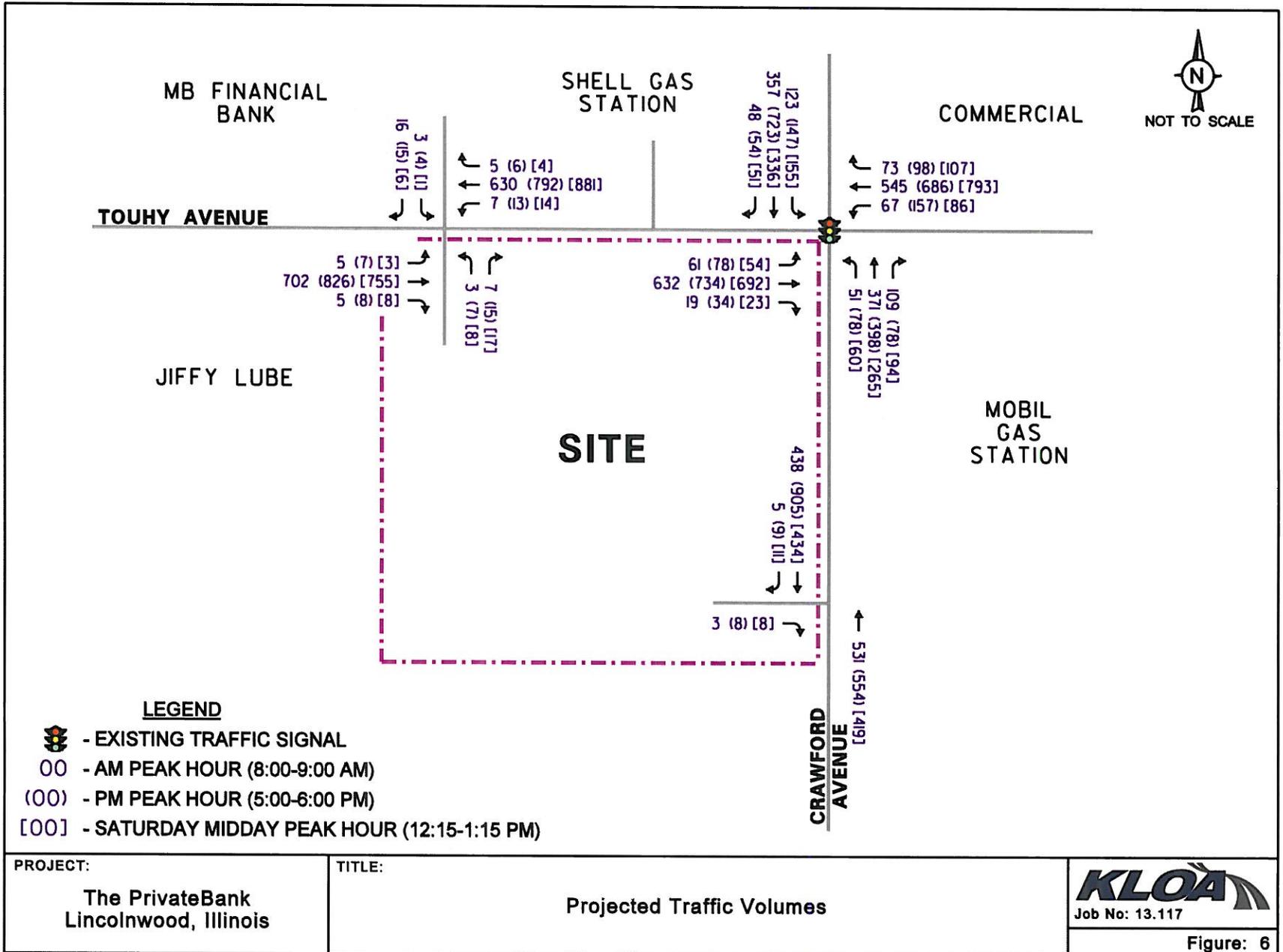
The traffic analyses were performed using the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM)*, 2010.



PROJECT:  
The PrivateBank  
Lincolnwood, Illinois

TITLE:  
Estimated Site-Generated Traffic Volumes

**KLOA**  
Job No: 13.117  
Figure: 5



The analyses for the traffic-signal controlled intersection of Touhy Avenue with Crawford Avenue was accomplished using field observed cycle lengths and existing roadway characteristics to determine the average overall vehicle delay, volume-to-capacity ratios, and levels of service.

The analyses for the unsignalized intersections determine the average control delay to vehicles at an intersection. Control delay is the elapsed time from a vehicle joining the queue at a stop sign (includes the time required to decelerate to a stop) until its departure from the stop sign and resumption of free-flow speed. The methodology analyzes each intersection approach controlled by a stop sign and considers traffic volumes on all approaches, lane characteristics, and the percentage of heavy vehicles.

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter from A to F based on the average control delay experienced by vehicles passing through the intersection. Control delay is that portion of the total delay attributed to the traffic signal or stop sign control operation, and includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. Level of Service A is the highest grade (best traffic flow and least delay), Level of Service E represents saturated or at capacity conditions and Level of Service F is the lowest grade (oversaturated conditions, extensive delays).

The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for both signalized and unsignalized intersections are shown in **Table 2**.

A summary of the capacity analysis results for the existing conditions and the future conditions are displayed in **Table 3** and **Table 4**, respectively.

Table 2  
**LEVEL OF SERVICE CRITERIA**

<b>Signalized Intersections</b>		
Level of Service	Interpretation	Average Control Delay (seconds per vehicle)
A	Favorable progression. Most vehicles arrive during the green indication and travel through the intersection without stopping.	≤10
B	Good progression, with more vehicles stopping than for Level of Service A.	>10 - 20
C	Individual cycle failures (i.e., one or more queued vehicles are not able to depart as a result of insufficient capacity during the cycle) may begin to appear. Number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping.	>20 - 35
D	The volume-to-capacity ratio is high and either progression is ineffective or the cycle length is too long. Many vehicles stop and individual cycle failures are noticeable.	>35 - 55
E	Progression is unfavorable. The volume-to-capacity ratio is high and the cycle length is long. Individual cycle failures are frequent.	>55 - 80
F	The volume-to-capacity ratio is very high, progression is very poor and the cycle length is long. Most cycles fail to clear the queue.	>80.0

<b>Unsignalized Intersections</b>		
Level of Service	Average Total Delay (SEC/VEH)	
A	0 - 10	
B	> 10 - 15	
C	> 15 - 25	
D	> 25 - 35	
E	> 35 - 50	
F	> 50	

Source: *Highway Capacity Manual*, 2010.

Table 3  
CAPACITY ANALYSIS RESULTS—EXISTING CONDITIONS

Intersection	Weekday A.M. Peak Hour	Weekday P.M. Peak Hour	Saturday Midday Peak Hour
Touhy Avenue and Crawford Avenue (signal)	C - 29.5	D - 37.2	C - 32.4
Touhy Avenue and Gas Station Westerly Access (stop sign)	B - 11.9	B - 14.4	B - 13.5

Delay is measured in seconds.  
LOS/delay for stop sign intersection represents the minor approach.  
LOS/delay for signalized intersection represents the intersection as a whole.

Table 4  
CAPACITY ANALYSIS RESULTS—FUTURE CONDITIONS

Intersection	Weekday A.M. Peak Hour	Weekday P.M. Peak Hour	Saturday Midday Peak Hour
Touhy Avenue and Crawford Avenue (signal)	C - 29.7	D - 37.7	C - 32.8
Touhy Avenue and Proposed Access/Gas Station Access (stop sign)			
• Northbound Approach	C - 15.6	C - 20.7	C - 15.3
• Southbound Approach	B - 12.8	C - 16.8	C - 19.8
Crawford Avenue and Proposed Access (stop sign)	A - 9.5	B - 11.2	A - 9.5

Delay is measured in seconds.  
LOS/delay for stop sign intersection represents the minor approach.  
LOS/delay for signalized intersection represents the intersection as a whole.

## **Traffic Evaluation**

The following is a summation of each of the analyzed critical intersections.

### **Touhy Avenue at Crawford Avenue**

This intersection will continue to operate at the existing levels of service for the future conditions with average delays increasing by less than one half of a second. Therefore, no geometric or traffic control improvements are needed at this intersection as a direct result of the traffic that will be generated by the bank. It should be noted that the queue analysis and field observation show that the eastbound queues on Touhy Avenue extend beyond the proposed access drive location during both the weekday evening and Saturday midday peak hours. However, these queues clear by the next green phase and will therefore not impede traffic operations at the access.

### **Access Drive at Touhy Avenue**

The results of the capacity analyses indicate that the inbound left-turn movements from Touhy Avenue will operate at the good Level of Service A during the peak hours while the left-turn outbound movement from the access drive onto Touhy Avenue will also operate at the acceptable Level of Service C. It is recognized, however, that due to the proximity of the access drive to the signalized intersection of Touhy Avenue and Crawford Avenue and the frequent queues that block the access drive, gaps for left turns in and out of the bank site will not always be readily available and that cars might have to wait longer than indicated by the results of the capacity analyses. However, with the queues clearing every cycle, vehicles will be able to enter and exit the site efficiently without significant delays.

### **Access Drive at Crawford Avenue**

This access drive will provide one inbound and one outbound lane and will be restricted to right turns only. The outbound movements should be under stop sign control. No geometric improvements on Crawford Avenue are needed at this access drive. This intersection will operate at a good level of service. It should be noted that the access drive will serve traffic approaching from the north on Crawford Avenue as well as traffic approaching from the east on Touhy Avenue that does not desire to turn left at the Touhy Avenue access drive. As such, this access drive adds flexibility to the access system and reduces the traffic load at the Touhy Avenue access drive.

## **Vehicle Stacking for the Bank Drive-Through System**

As proposed, the site plan has been designed to provide maximum ingress/egress flexibility for the site with minimal internal pedestrian/vehicle conflicts. Drive-through traffic can enter from Touhy Avenue and Crawford Avenue and circulate counterclockwise around the bank with vehicles being able to exit the site from either access drive. A review of the current site plan shows that three vehicles can be stored in the drive-up lane and four vehicles can be stored in the ATM lane therefore allowing a total of seven vehicles to be stored within the drive-through system with additional stacking areas available before internal circulation is impacted. The proposed stacking area will be more than adequate to accommodate the peak demand. This is primarily due to significant changes to the banking industry with on-line banking, direct deposit and more branch bank locations, a reduction in vehicular trips to this type of land use is occurring. The need for a large stacking area for vehicle queues and large parking lots is significantly reduced. This is especially true at smaller branch banks such as this proposed The PrivateBank. KLOA, Inc. has conducted a number of surveys of neighborhood banks. The data indicated that queues at the drive-through lanes average one to two vehicles per lane.

## **Parking**

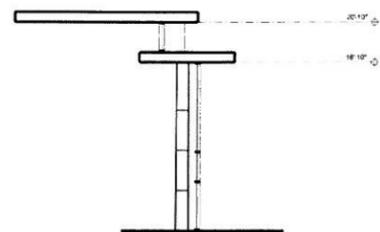
The proposed bank will provide a total of ten off-street parking spaces located on the east and north sides of the building. Based on a review of the *ITE Parking Generation Manual*, 4<sup>th</sup> Edition, a 2,500 square-foot drive-in bank will have a peak parking demand of ten spaces on a weekday and nine spaces on a Saturday. As such, the proposed number of parking spaces will be adequate to accommodate the projected peak parking demand and provide a surplus of eight stalls.

## Conclusion

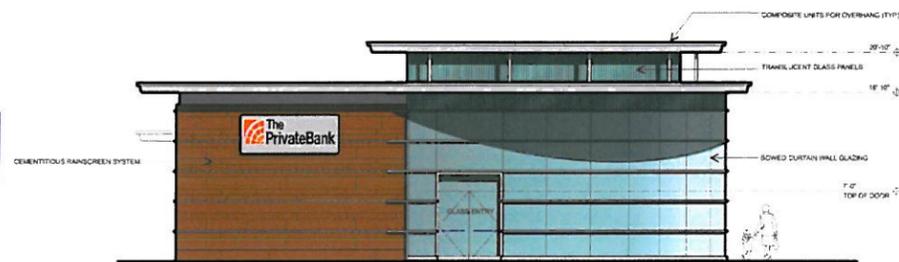
Based on the preceding analyses and recommendations, the following conclusions have been made.

- The bank-generated traffic can be accommodated efficiently without significant impact to the external roadway system.
- The two access drives will adequately serve the development traffic.
- The on-site circulation coupled with the proposed access drives will allow traffic maximum ingress/egress efficiency while at the same time reduce the impact on the external roadways.
- The drive-through facility provides adequate stacking to accommodate queued traffic internally.
- The proposed number of parking spaces will be adequate in accommodating the projected peak parking demand.

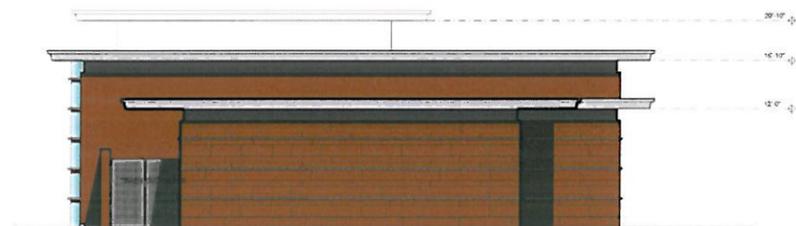




SECTION



EAST ELEVATION



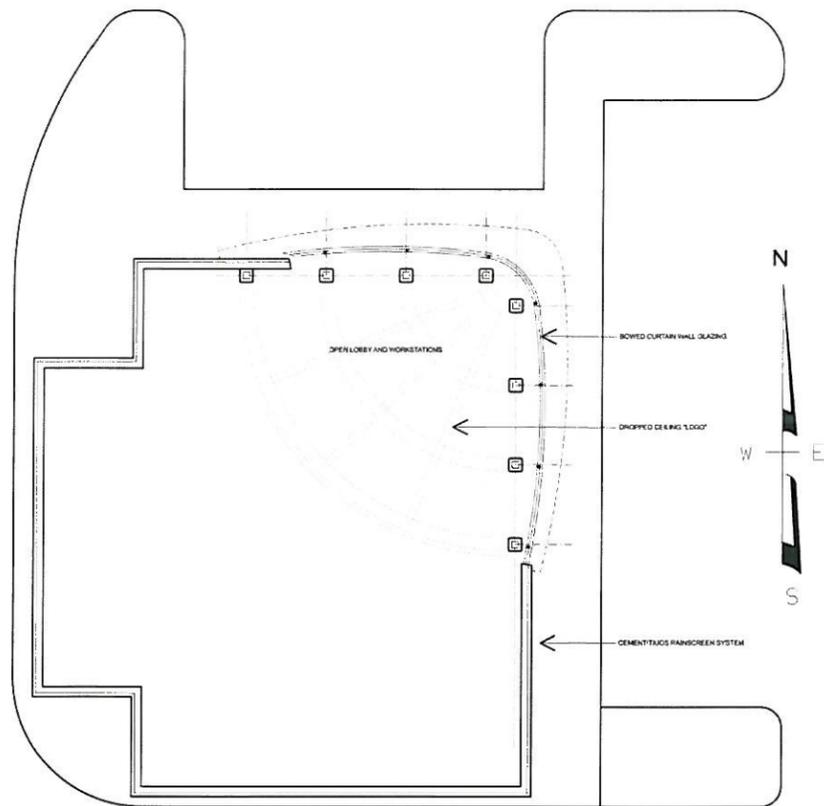
WEST ELEVATION



NORTH ELEVATION



SOUTH ELEVATION



PRELIMINARY PLAN

DEVELOPER INFO:

Urban Associates, LLC  
NEW CHANNELS REALTY  
2421 SIMPSON STREET  
EVANSTON, IL

The PrivateBank  
129 S. LaSalle Street  
Chicago, Illinois



Camburas & Theodore, Ltd.  
Architecture | Planning | Interiors | Engineering  
2654 E. Congress St., Ste. 200, Chicago, IL  
Tel: 847.298.3333 Fax: 847.298.0238  
www.camburasandtheodore.com



ISSUE TYPE:

TYPE	DATE
REVIEW	8-4-13

REVISIONS:

NUMBER	DATE

PROFESSIONAL STAMP

DATE: 7-11-13

DRAWN BY: JB

C+T JOB NUMBER: —

TITLE SHEET: PLANS + ELEVATIONS

4007 Touhy Ave. Lincolnwood Illinois

SHEET NUMBER:

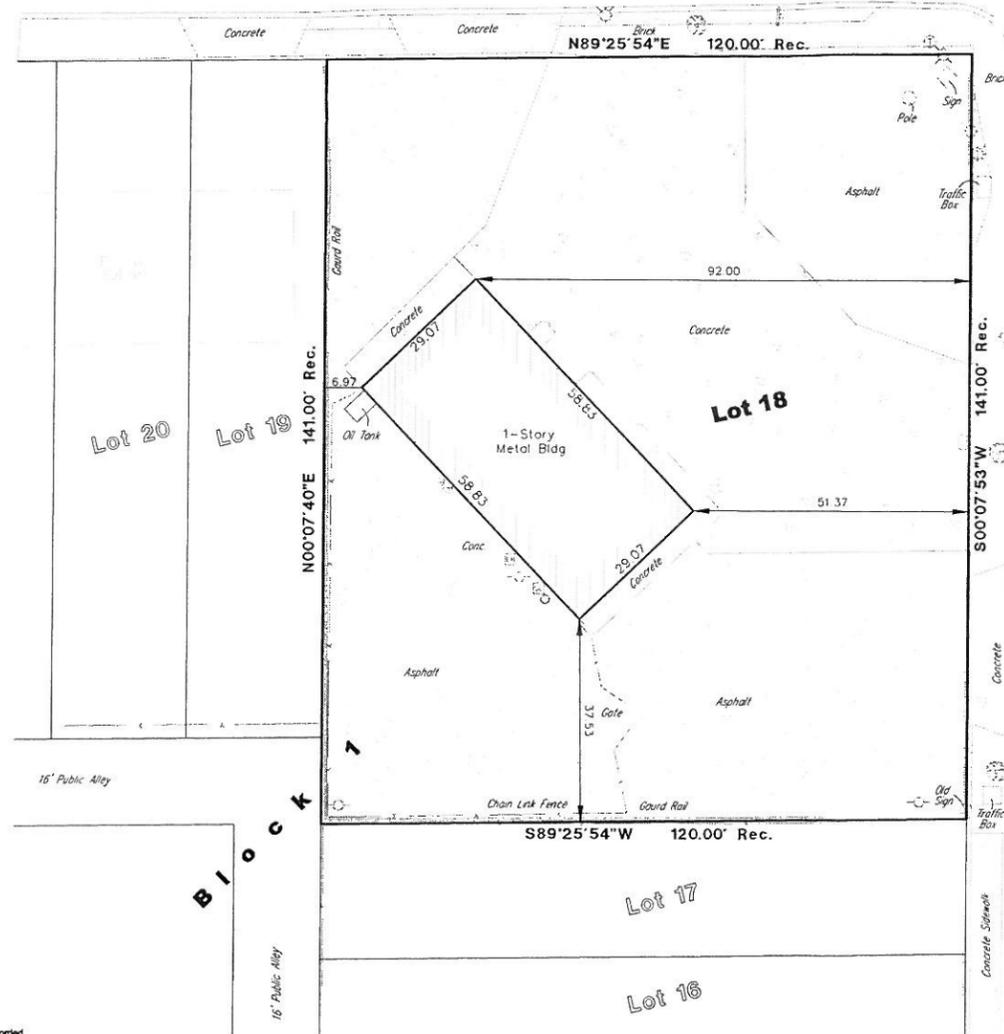
A-10



# ALTA/ACSM Land Title Survey

LOT 18 IN BLOCK 1 IN WITTBOLD'S SECOND ADDITION TO KENILWORTH HIGHLANDS, A SUBDIVISION OF THE NORTHEAST 1/4 OF THE NORTHEAST 1/4 OF THE NORTHEAST 1/4 OF SECTION 34, TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, ALL IN COOK COUNTY, ILLINOIS.

**Touhy Avenue**

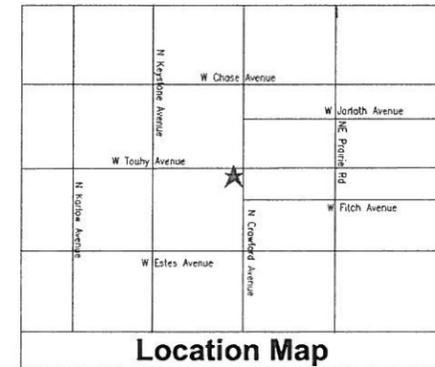


**Crawford Avenue**

- ☉ TRAFFIC LIGHT
- 🌳 TREE
- 📍 LIGHT POLE
- ⦿ BOLLARD
- ⊙ MISC LIGHT
- ☒ TRAFFIC BOX
- 📏 SIGN
- ⚡ ELEC. METER
- ⚙️ GAS LINE
- ⚙️ UTILITY POLE

**Surveyor Notes:**

1. Field Work Completed on 01-23-12
2. Prepared for Survey Services, Inc
3. Site Address: 4007 W Touhy Ave., Lincolnwood, IL 60712
4. Pin No. 10-34-205-020
5. The easements shown hereon are provided from the use of the recorded subdivision plat.
6. This plat was prepared with the use of Title Commitment, AFF-1208565 Dated February 14, 2012.
7. Compare deed description and site condition with the data given on the plat and report any discrepancies to the surveyor at once.
8. The utilities as shown on this drawing were developed from the information available. This does not imply nor intended to be the complete inventory of utilities in the area. It is the client's responsibility to verify the location of all utilities (whether shown or not) and protect said utilities from any damage.
9. The location of underground utilities was determined by field observations and visible markings only. Call 811.U.I.E. before digging.



**Location Map**

STATE OF ILLINOIS )  
COUNTY OF COOK ) SS

TO: Next Touhy, LLC  
FROM: Afinity Title Services, LLC, and/or their assigns or successors in interest

THIS IS TO CERTIFY THAT THIS MAP OR PLAN AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS IN 2011, AND INCLUDES NO ITEMS FROM TABLE A THEREOF PURSUANT TO THE ACCURACY STANDARDS AS ADOPTED BY ALTA AND NSPS AND IN EFFECT ON THE DATE OF THIS CERTIFICATION. UNDERSIGNED FURTHER CERTIFIES THAT IN MY PROFESSIONAL CAPACITY, AS A LAND SURVEYOR REGISTERED IN THE STATE OF ILLINOIS, THE RELATIVE POSITIONS SHOWN ON THIS SURVEY DOES NOT EXCEED THAT WHICH IS SPECIFIED THEREIN.

GIVEN UNDER MY HAND AND SEAL THIS 24th DAY OF February, A.D. 2012  
AT PALATINE, ILLINOIS.

ILLINOIS PROFESSIONAL LAND SURVEYOR NUMBER 3323



ADOPTED BY THE AMERICAN LAND TITLE ASSOCIATION ON February 23, 2011 AND BY THE BOARD OF DIRECTORS, AMERICAN CONGRESS ON SURVEYING AND MAPPING ON February 23, 2011. APPROVED BY THE BOARD OF DIRECTORS, NATIONAL SOCIETY OF PROFESSIONAL SURVEYORS ON February 23, 2011. THIS SURVEY WAS PREPARED FOR TITLE PURPOSES ONLY.

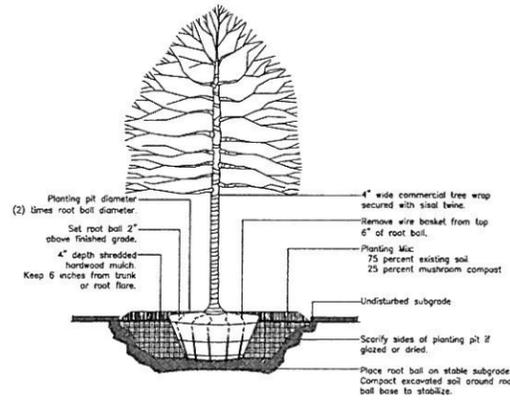
Drawn:	JJP	Date:	1-26-12	
Revision		Date	Drawn	Checked
Reviewed title & added certifications		2-24-12	JK	

**LSA Land Surveying Services, Inc.**  
574 W. Coffer Street Palatine, Illinois 60067  
Ph. (847)991-7700 Fax. (847)991-7707  
Professional Design Firm License No. 184-003632

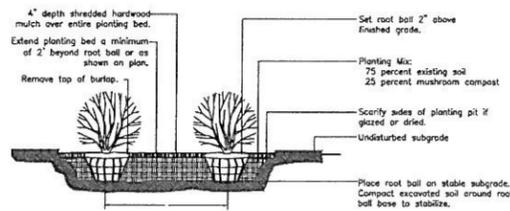
Field Work Completed: 01/23/12    Scale: 1" = 20'    Date: 01/25/12  
Site Address:  
**4007 W Touhy Ave  
Lincolnwood, Illinois**

Job Number  
**LS120019**  
Sheet Number  
**ALTA/ACSM**

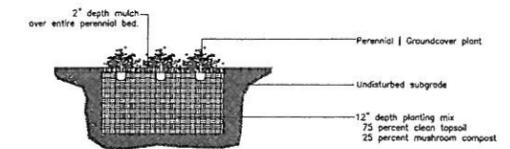
Sheet Name  
**ALTA/ACSM  
Land Title Survey**



**Detail**  
**Deciduous Tree Planting**



**Detail**  
**Shrub Planting**



**Detail**  
**Perennial and Groundcover Planting**

**Plant List**

Shade Trees					
Key	Qty	Size	Botanical Name	Common Name	Remarks
GPS	3	2.5"	Ginkgo biloba 'Princeton Sentry'	Princeton Sentry Ginkgo	BB
ULM	3	2.5"	Ulmus x 'Morton'	Accolade Elm	BB

Ornamental Trees					
Key	Qty	Size	Botanical Name	Common Name	Remarks
AMC	1	6'	Amelanchier canadensis	Shadblow Serviceberry	BB/Clump
MLS	1	4'	Malus sargentii	Sargent Crabapple	BB/Clump

Shrubs					
Key	Qty	Size	Botanical Name	Common Name	Remarks
BUX	10	24"	Buxus x microphylla 'Glencoe'	Chicagoland Green Boxwood	BB
HYA	1	24"	Hydrangea 'Annabelle'	Annabelle Hydrangea	BB
JCS	14	24"	Juniperus chinensis 'Sargentii'	Sargent Juniper	BB
RHG	14	24"	Rhus aromatica 'Gra-Low'	Gra-Low Fragrant Sumac	BB
RKO	10	#3	Rosa var. 'Nautraum'	Pink Flower Carpet Rose	BB
SMK	5	24"	Syringa patula 'Miss Kim'	Miss Kim Dwarf Lilac	BB
VIB	26	36"	Viburnum dentatum 'Seventeen'	Little Joe Viburnum	BB

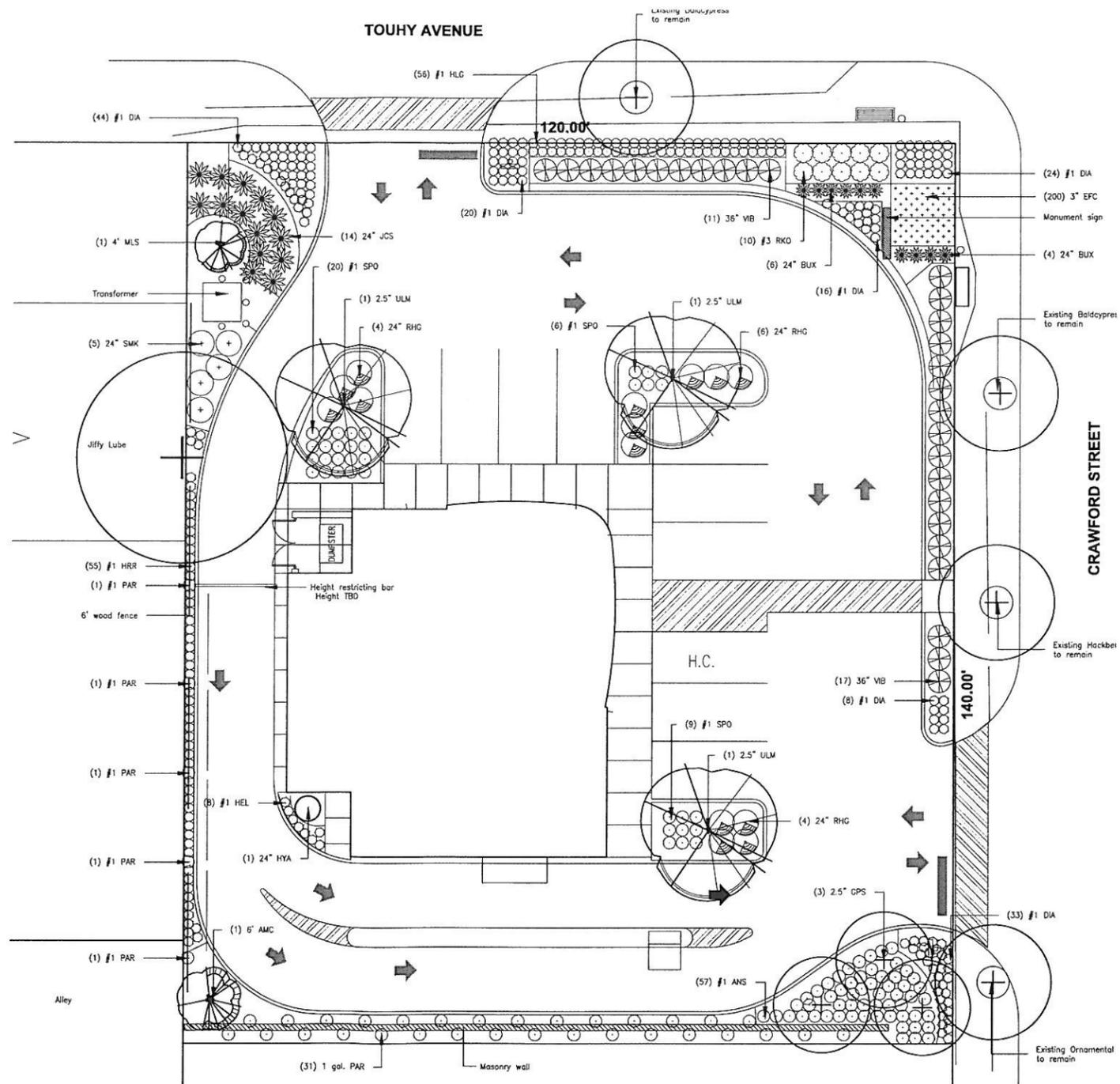
Perennials, Ornamental Grasses and Groundcovers					
Key	Qty	Size	Botanical Name	Common Name	Remarks
ANS	57	#1	Andropogon scoparius	Little Bluestem	Container
DIA	145	#1	Dianthus gratianopolitanus 'Firewitch'	Firewitch Cheddar Pink	Container
EFC	200	3"	Euonymus fortunei 'coloratus'	Purpleleaf Wintercreeper	Pole
HEL	8	#1	Helleborus 'Pink Lady'	Pink Lady Lenten Rose	Container
HRR	55	#1	Hemerocallis 'Rosy Returns'	Rosy Returns Daylily	Container
HLG	56	#1	Hemerocallis 'Little Grapette'	Little Grapette Daylily	Container
SPO	35	#1	Sporobolus heterolepis	Prairie Dropseed	Container
PAR	36	#1	Parthenocissus tricuspidata	Boston Ivy	Container

**Landscape Irrigation**

All planting beds shall be irrigated with a fully automatic landscape irrigation system.

**Notes**

Refer to existing tree survey for disposition of existing trees.  
Parkway trees shall be subject to review and acceptance by the Village Arborist.



Scale 1" = 10'  
North

**DEVELOPER INFO:**  
Urban Associates, LLC  
NEW CHANNELS REALTY  
2421 SIMPSON STREET  
EVANSTON, IL

The PrivateBank  
129 S. LaSalle Street  
Chicago, Illinois



Camburas & Theodore, Ltd.  
Architects | Planners | Engineers | Interiors | Engineers  
2454 E. Granger St., Oak Ridge, IL  
Tel: (847) 296-1525 Fax: (847) 298-0228  
www.camburasandtheodore.com

DAVID R. McCALLUM ASSOCIATES, INC.  
LANDSCAPE ARCHITECTS  
300 N. Meacham Avenue | Lombard, Illinois 60148  
T: 630.352.0200 | F: 630.352.0204



**ISSUE TYPE:**

TYPE:	DATE:
SUBMISSION	8/12/13

**REVISIONS:**

NUMBER:	DATE:

**PROFESSIONAL STAMP:**

DATE: 8-12-13  
DRAWN BY: DM  
C+T JOB NUMBER: ---

TITLE SHEET:  
**LANDSCAPE PLAN**

SHEET NUMBER:  
**L-1**

DEVELOPER INFO:  
 Urban Associates, LLC  
 NEW CHANNELS REALTY  
 2421 SIMPSON STREET  
 EVANSTON IL

The PrivateBank  
 129 S. LaSalle Street  
 Chicago, Illinois

**CT**  
 Camburas & Theodore, Ltd.  
 Architecture | Planning | Interiors | Engineering  
 2954 E. Desmarais St., Des Plaines, IL  
 Tel (847) 298-1222 Fax (847) 298-5228  
 www.camburasandtheodore.com

DAVID R. McCALLUM ASSOCIATES, INC.  
 LANDSCAPE ARCHITECTS  
 382 N. Milwaukee Avenue | Libertyville, IL 60064  
 T 847.352.0300 | F 847.352.0204



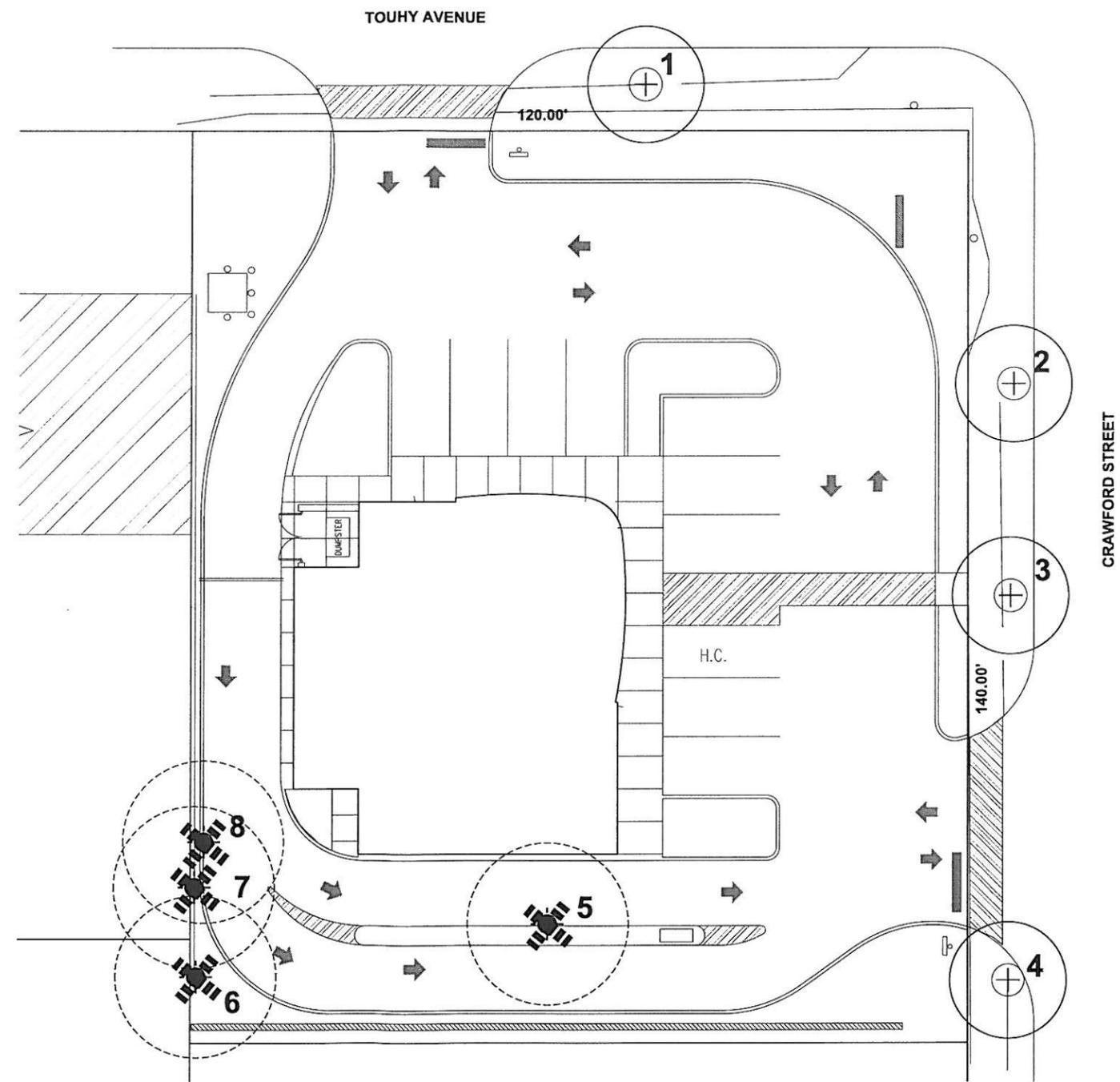
ISSUE TYPE:  
 TYPE: DATE:  
 SUBMISSION 8/12/13

REVISIONS:  
 NUMBER: DATE:

PROFESSIONAL STAMP:

DATE: 8-12-13  
 DRAWN BY: DM  
 C-Y JOB NUMBER: --

TITLE SHEET:  
 TREE INVENTORY  
 SHEET NUMBER:  
**L-2**



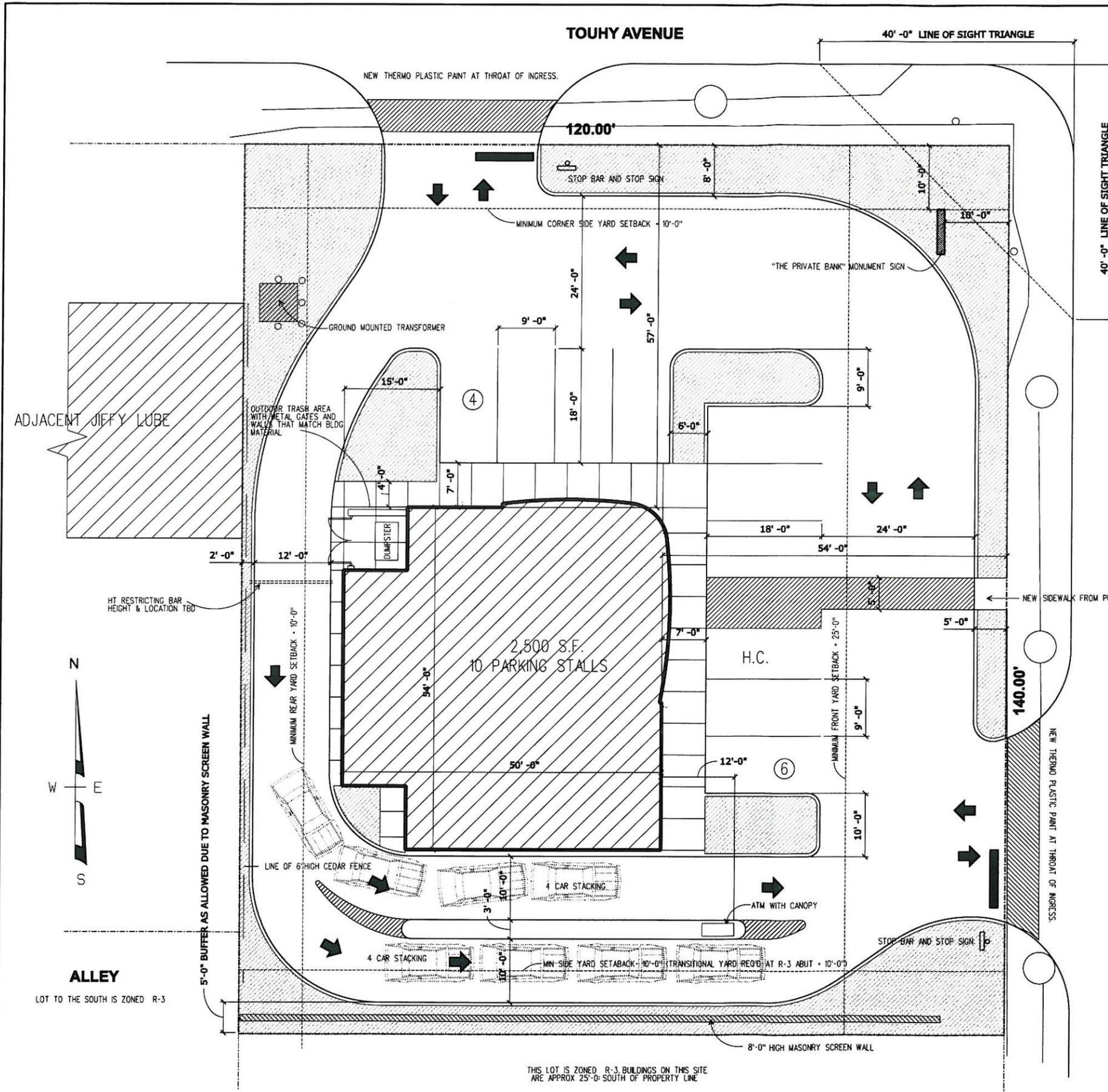
**Tree Inventory**

Tag	Size	Botanical Name	Common Name	Form	Condition	Action
1	2"	<i>Taxodium distichum</i>	Baldcypress		2/2	To remain
2	2"	<i>Taxodium distichum</i>	Baldcypress		2/2	To remain
3	2"	<i>Celtis occidentalis</i>	Hickberry		2/2	To remain
4	2"	<i>Pyrus calleryana</i>	Ornamental Pear		2/2	To remain
5	5"/5"	<i>Ulmus pumila</i>	Siberian Elm		4/3	Remove
6	7"	<i>Ulmus pumila</i>	Siberian Elm		4/3	Remove
7	10"/7"	<i>Ulmus americana</i>	American Elm		4/3	Remove
8	28"	<i>Ulmus pumila</i>	Siberian Elm		4/3	Remove

**Condition Rating Scale**

Rating	Description	General Criteria
1	Excellent	The tree is typical of the species, has less than 10% deadwood in the crown that is attributable to normal causes, has no other observed problems, and required no remedial action.
2	Good to Fair	The tree is typical of the species and/or has less than 20% deadwood in the crown, only one or two minor problems that are easily corrected with normal care.
3	Fair	The tree is typical of the species and/or has less than 30% deadwood in the crown, one or two minor problems that are not eminently lethal to the tree, and no significant decay or structural problems, but the tree must have remedial care above normal care in order to minimize the impact of future stress and to insure continued health.
4	Fair to Poor	The tree is not typical of the species and/or has significant problems such as 30-50% deadwood in the crown, serious decay or structural defect, insects, disease or other problems that can be eminently lethal to the tree or create a hazardous tree if not corrected in a short period of time or if the tree is subjected to additional stress.
5	Poor	The tree is not typical of the species and/or has over 50% deadwood in the crown, major decay or structural problems, is hazardous or is severely infested with insects, disease, or other problems that even if aggressively corrected would not result in the long term survival of the tree.
6	Dead	Less than 10% of the tree shows signs of life.

Scale 1" = 10'  
 North



Fences not more than six feet in height are allowed in rear yards. Solid masonry fences not more than eight feet in height may be allowed where a lot located in a business district abuts the lot line of a lot located in a residential district lot or another lot located in a business district. However, such fences are subject to the prior review by the Zoning Board of Appeals and the prior approval by the Village Board.

Transitional yard. Wherever a business or office district abuts a residentially zoned lot, a transitional yard shall be maintained. Transitional yards shall meet the following requirements:

- All structures adjacent to residential districts on the B-1, B-2, or B-3 zoned lot must be set back a distance that is equal to or greater than the side or rear yard setback requirement for the adjacent R-zoned lot, but not less than 10 feet.
- A landscape buffer must also be provided. This buffer shall extend the length of the lot line or segment of the lot line that abuts the residential district, and shall be at least eight feet wide or where screening consists of a masonry wall, a minimum width of five feet.
- Drive-up or through facilities shall obtain a special use permit. No more than one drive-through lane shall be permitted by special use permit in the B-1 District.
- The minimum stacking space required is four vehicles at each bay window, ordering station or machine.
- All drive-through facilities shall be located on the side or rear of the structure that is either facing the site's parking area or internal drive aisle.
- All structures adjacent to residential districts on the B-1, B-2, or B-3 zoned lot must be set back a distance that is equal to or greater than the side or rear yard setback requirement for the adjacent R-zoned lot, but not less than 10 feet.
- Minimum front yard setback = 25' - 0"
- Minimum corner side yard setback = 10' - 0"
- Minimum rear yard setback = 10' - 0"
- Every off-street parking lot or parking area containing five or more parking spaces shall be set back, buffered and screened from public view and adjacent property by a perimeter landscaped area having a minimum width of eight feet, or, where screening shall consist of a masonry wall, a minimum width of five feet.

Masonry walls used for perimeter screening shall have a minimum height of 30 inches and a maximum height of 36 inches. Such walls shall have a finished surface which is the same or closely similar to the masonry of the principal building. A planting island equal in area to a parking space shall be located at each end of a parking row and after each 20 parking spaces within a parking row. One tree shall be required for each planting island required. A minimum setback and landscape area of six feet in width shall be located immediately along the front and sides of all buildings. No loading area required for under 10,000 sf.

SITE PLAN SUMMARY:	
TOTAL LOT AREA:	16,800 SQ. FT.
PROPOSED TOTAL BUILDING AREA:	2,500 SQ.FT.
PROPOSED FLOOR AREA RATIO (FAR):	.15
TOTAL PARKING STALLS:	10 (4 PER 1,000 SF GFA FOR FINANCIAL SERVICES)
4 STACKING SPACES PER LANE OF DRIVE THRU	
LOT IS ZONED "B2" - GENERAL BUSINESS	
NO LOADING AREA REQUIRED FOR UNDER 10,000 SF OF BUILDING	
LOT TO THE SOUTH IS ZONED "R3" - RESIDENTIAL	
LOT TO THE WEST IS ZONED "B2" - GENERAL BUSINESS	

DEVELOPER INFO:  
 Urban Associates, LLC  
 NEW CHANNELS REALTY  
 2421 SIMPSON STREET  
 EVANSTON IL

The PrivateBank  
 129 S. LaSalle Street  
 Chicago, Illinois

**CT**  
 Camburas & Theodore, Ltd.  
 Architecture | Planning | Interiors | Engineering  
 304 E. Superior St., 2nd Floor, Chicago, IL  
 Tel: (773) 386-5252 Fax: (773) 386-5258  
 www.camburasandtheodore.com



ISSUE TYPE:	
TYPE:	DATE:
SUBMISSION	8/12/13

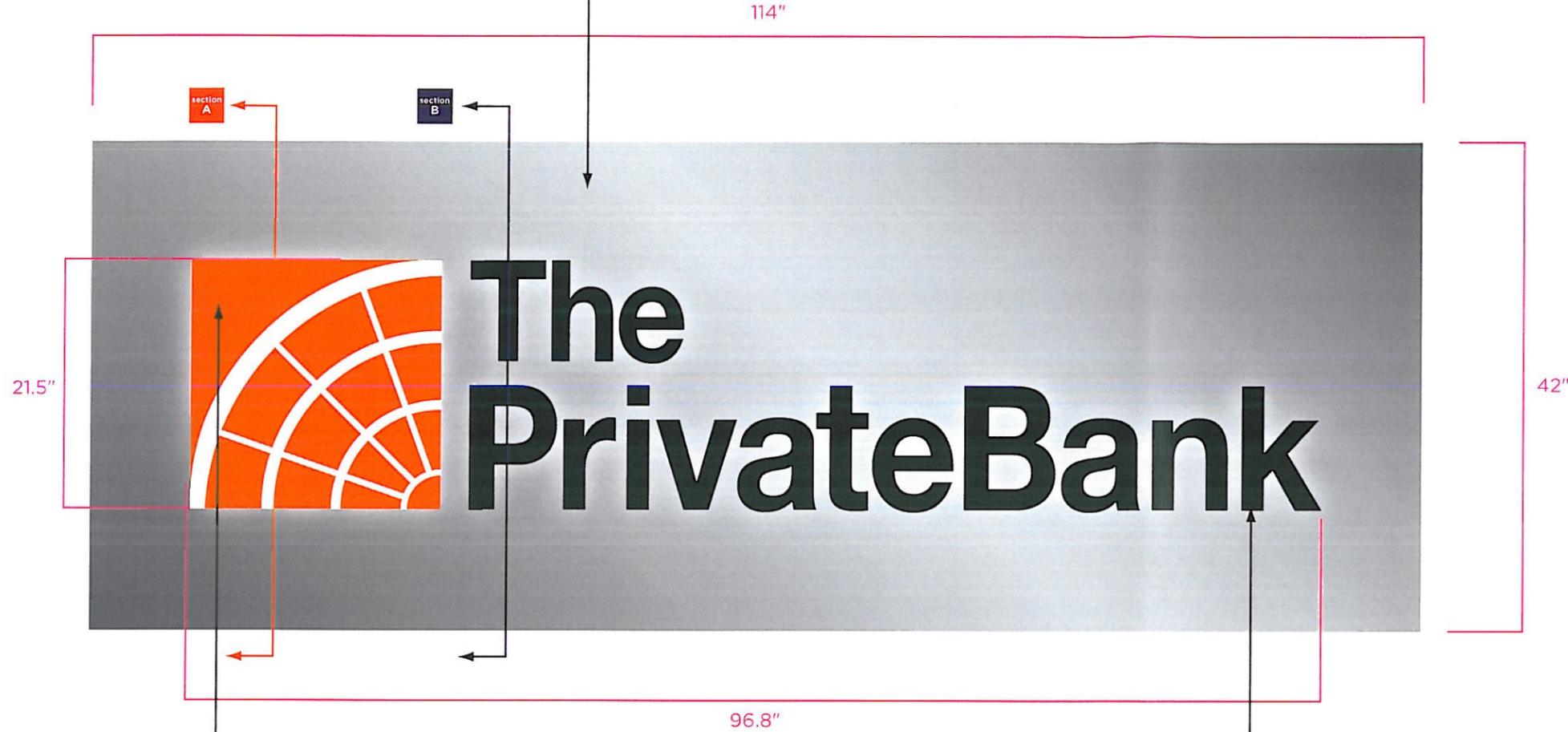
REVISIONS:	
NUMBER:	DATE:
1	-

PROFESSIONAL STAMP:	
DATE:	8-12-13
DRAWN BY:	JB
C+T JOB NUMBER:	-

TITLE SHEET:	
SITE PLAN	
SHEET NUMBER:	
<b>A-10</b>	

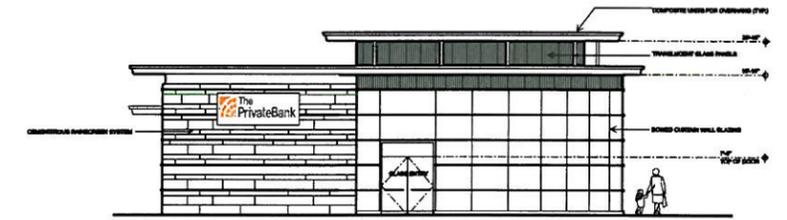
ALL DRAWINGS AND WRITTEN MATERIAL APPEARING HEREIN CONSTITUTE THE ORIGINAL AND UNPUBLISHED WORK OF THE ARCHITECT, AND THE SAME MAY NOT BE REPRODUCED, COPIED OR DISCLOSED WITHOUT THE WRITTEN CONSENT OF THE ARCHITECT. COPYRIGHT © CAMBURAS & THEODORE, LTD.

42" (h) X 114" (w) fabricated stainless steel pan with horizontal satin faces & returns. Depth approximately 4". Will house all wiring and transformers. Pan will be mechanically attached to building facade as shown on east and north elevations.



Acrylic edge-lit push-thru logo. LED illumination housed within pan. Returns will be white acrylic. Face will be white acrylic with translucent orange vinyl.

Reverse channel halo-lit fabricated stainless steel or aluminum letters, painted black. LED illumination housed within letters. Pin mounted to face of stainless steel pan with studs & standoffs.



EAST ELEVATION



NORTH ELEVATION

Proposed Night Time View:



**H.M. WITT & CO.**  
Signs  
FINE SIGNAGE SINCE 1894

3313 West Newport Ave. Chicago, IL 60618  
office | 773 250 5000 fax | 773 250 5100  
toll free | 1 888 427 0703 web | www.hm Witt.com

- APPROVED AS IS
- APPROVED W/CHANGES
- REVISE AND RESUBMIT

APPROVAL \_\_\_\_\_  
DATE \_\_\_\_\_

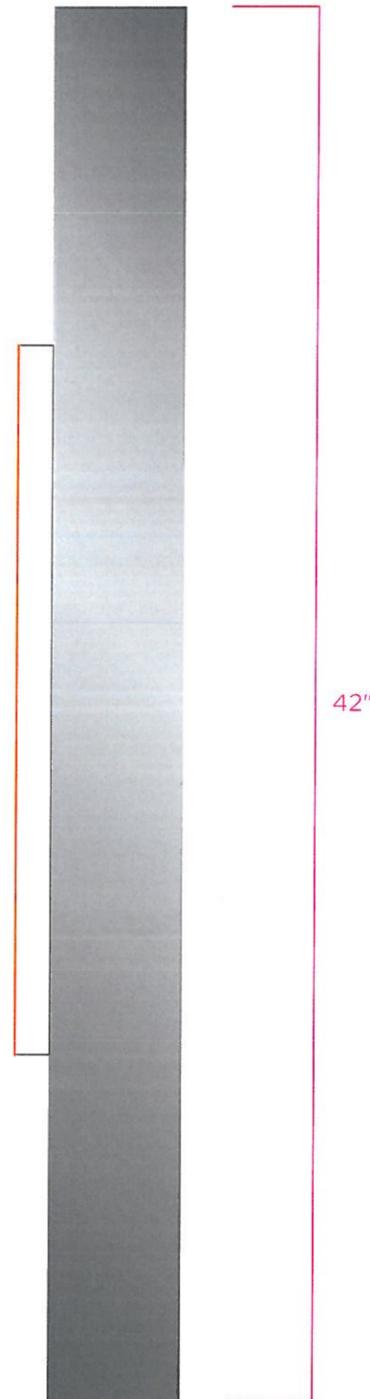
REVISIONS

NO.	DESCRIPTION

CLIENT The PrivateBank / Lincolnwood  
DATE 08-08-13  
JOB # Facade Signage - Elevation  
DRAWN BY bs  
SCALE No Scale

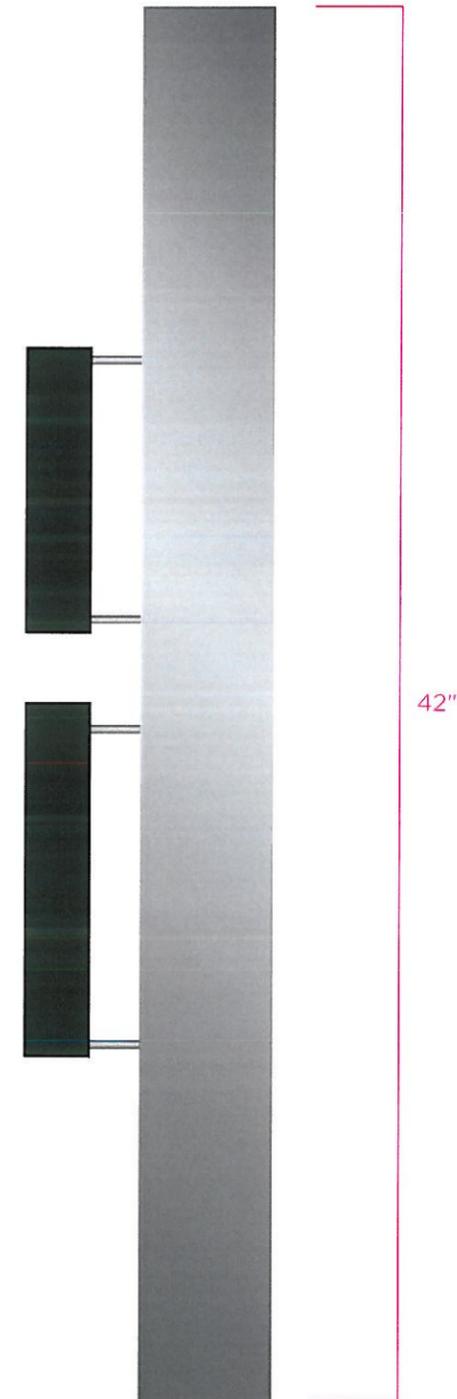
**section  
A**

Acrylic edge-lit push-thru logo.  
LED illumination housed within pan.  
Returns will be white acrylic.  
Face will be translucent white acrylic  
with applied translucent orange vinyl.  
Illumination will pour out the sides.



**section  
B**

Reverse channel halo-lit fabricated  
stainless steel or aluminum letters,  
painted black. LED illumination housed  
within letters. Pin mounted to face of  
stainless steel pan with studs & standoffs.  
Illumination will pour out the backs.



42"

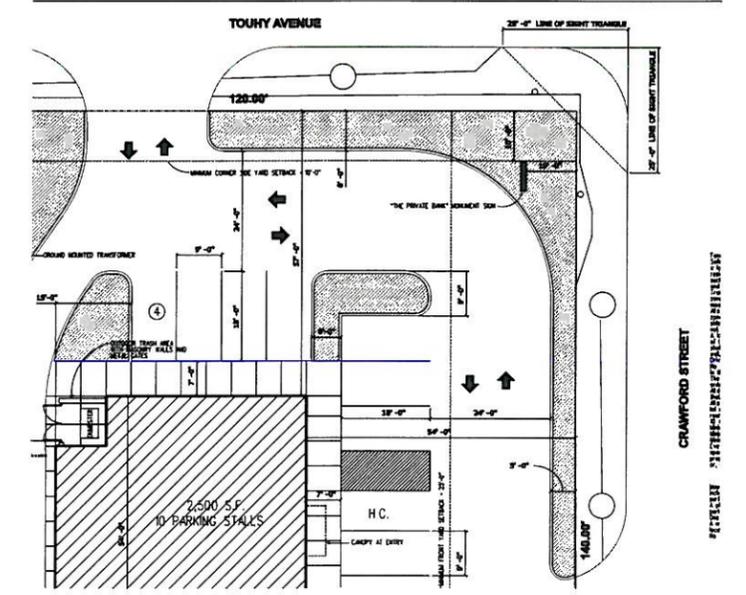
42"

**REVISIONS**

NO.	DESCRIPTION	DATE

APPROVED AS IS   
 APPROVED W/CHANGES   
 REVISE AND RESUBMIT   
 APPROVAL .....

CLIENT The PriivateBank / Lincolnwood  
 DATE 08-08-13  
 JOB # Facade Signage - Section Details  
 DRAWN BY bs  
 SCALE No Scale



Proposed Night Time View:



Internally illuminated stainless steel sign cabinet. LED illumination. Double sided routed graphics backed up with translucent white acrylic. Logo portion has translucent orange vinyl applied to face. Lettering has perforated black vinyl applied to face. Lettering will appear black in the day time and will illuminate white at night. Sign mounts to soil with a 6" (h) "skirt" at the bottom.

**H.M. WITT & CO.**  
*Signs*  
 FINE SIGNAGE SINCE 1894  
 3313 West Newport Ave. Chicago, IL 60618  
 office | 773 250 5000 fax | 773 250 5100  
 toll free | 1888 427 0703 web | www.hm Witt.com

APPROVED AS IS

APPROVED W/CHANGES

REVISE AND RESUBMIT

APPROVAL \_\_\_\_\_

DATE \_\_\_\_\_

REVISIONS	

CLIENT The PrivateBank / Lincolnwood

DATE 08-09-13

JOB # Monument Sign

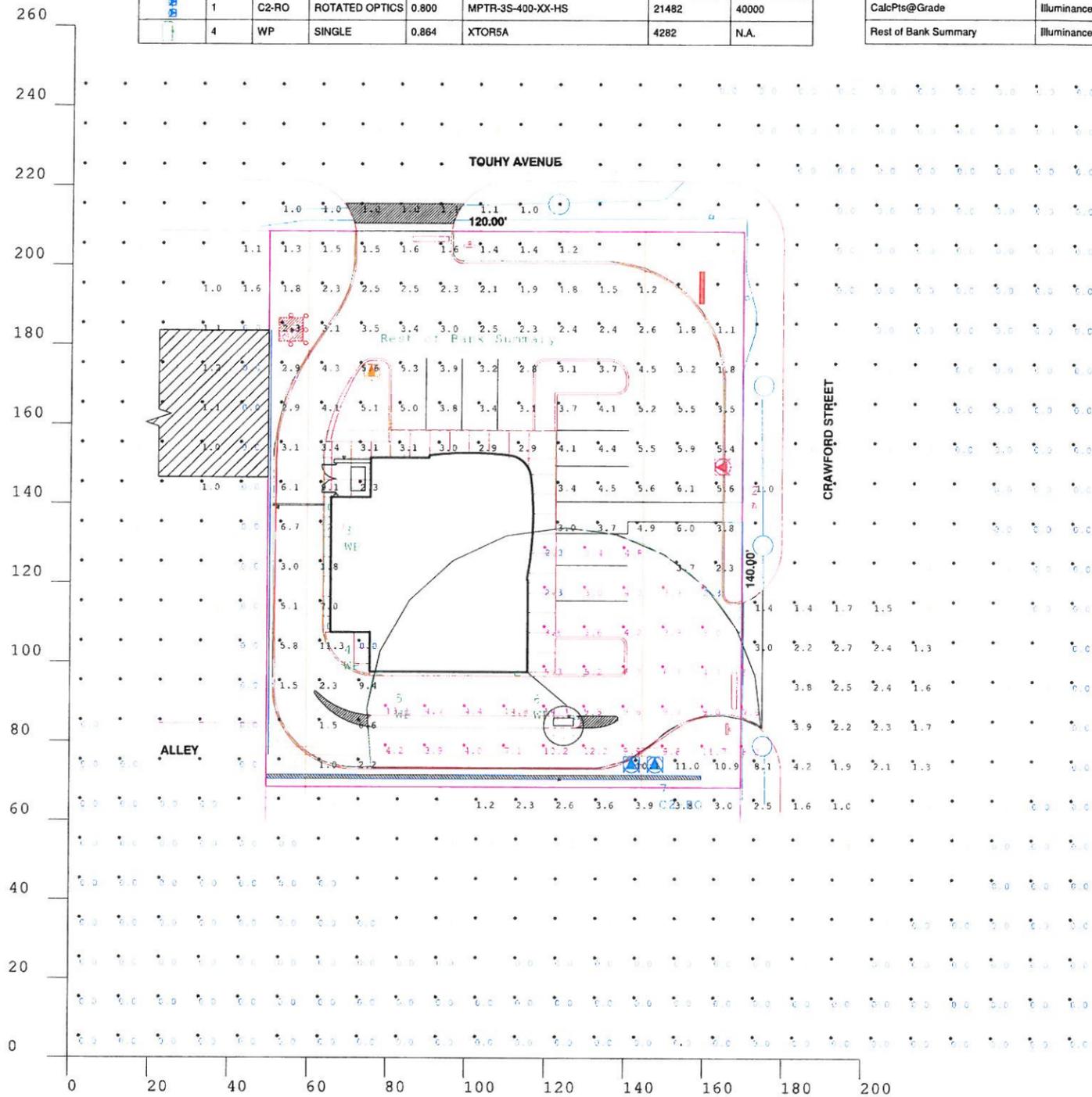
DRAWN BY bs

SCALE No Scale

Luminaire Schedule							
Symbol	Qty	Label	Arrangement	LLF	Description	Lum. Lumens	Lumens/Lamp
	1	A	SINGLE	0.800	MPTR-SL-400	23451	40000
	1	B	SINGLE	0.800	MPTR-5S-400	29333	40000
	1	C2-RO	ROTATED OPTICS	0.800	MPTR-3S-400-XX-HS	21482	40000
	4	WP	SINGLE	0.864	XTOR5A	4282	N.A.

Calculation Summary						
Label	CalcType	Units	Avg	Max	Min	Avg/Min
5' RADIUS ATM	Illuminance	Fc	11.15	12.0	10.5	1.06
50' RADIUS ATM	Illuminance	Fc	6.55	14.8	2.3	2.85
CalcPts@Grade	Illuminance	Fc	0.94	12.7	0.0	N.A.
Rest of Bank Summary	Illuminance	Fc	4.01	12.7	1.2	3.34

Luminaire Location Summary						
LumNo	Label	X	Y	Z	Orient	Tilt
1	B	75.5	170	24	90	0
2	A	167.5	149.5	24	180	0
3	WP	64.75	139	10	180	0
4	WP	64.75	109	10	180	0
5	WP	78	96.75	10	270	0
6	WP	113	96.75	10	270	0
7	C2-RO	144.8	74.25	24	90	0



	11000 N. Lincoln St. COOPER LIGHTING INC. PEACHTREE CITY, GA 30094 Phone Number: (770) 488-1144 Fax Number: (770) 488-4598 Email Address: <a href="mailto:info@cooperlighting.com">info@cooperlighting.com</a>
	Project: The Private Bank Lincolnwood, IL Date: 8/12/2013 Client: Jose Saucedo Pilipuf Grist & Associates Project No: 1302079A

Lighting Application drawings are being provided to the recipient of this disclaimer.  
 We make no representation as to its completeness, currency or accuracy because of reasons inherent to CAD and the additional digital data used to produce a lighting application.  
 All digital CAD data appear to be extremely accurate, however, this apparent accuracy is an artifact of the techniques used to generate it, and is in no way intended to imply actual accuracy. The user of this data takes full responsibility for the accuracy and correctness of all measurements, area, inventories or other data extracted from this, either manually or with the use of a computer. This light level analysis is an estimate only, and is based on estimated reflectance values for interior applications or estimated pole locations based on specified light levels for exterior applications. Any variance from reflectance values, obstructions, light loss factors or dimensional data will affect the actual light levels obtained. This analysis is a mathematical model and can be only as accurate as is permitted by the third party software and the IES standards used. In addition calculated values may vary from actual measurements in certain situations due to variances, such as but not limited to, lamp output, input voltage, ballast variances, manufacturing tolerances and application variances. The presence of objects will decrease light levels and may cause some shadowing.



---

## MEMORANDUM

---

**TO:** Chairman Eisterhold  
Members of the Plan Commission

**FROM:** Aaron N. Cook, AICP  
Development Manager

**DATE:** September 9, 2013

**SUBJECT:** Remanded Zoning Code Text Amendment: Commercial, Institutional and Civic Building Design Standards and the Lincoln Overlay District

---

On April 16, 2013, the Village Board considered a recommendation by the Plan Commission concerning proposed changes to the Village's design standards for non-residential buildings. These design standards are commonly referred to as the Village's masonry requirements. In considering this matter at the April 16, 2013 meeting, the Village Board referred this item to the Committee on Ordinances, Rules & Buildings (CORB) for further deliberation. The Plan Commission recommendation on this matter included the following:

1. Eliminating "Masonry Only" requirement from the Lincoln Avenue Overlay District;
2. Establish that the exterior walls of non-residential buildings be constructed with a minimum of 75% "high quality materials". This includes a requirement that from the ground level to the top of the first floor windows must be constructed entirely of "high quality materials";
3. Establish a list of permitted "high quality materials" which includes: 1) brick with a minimum thickness of 2.25 inches, 2) natural stone, 3) sandstone, 4) other native stone, 5) pre-cast decorative stone, and 6) glass;
4. Add definitions for Brick, Decorative Concrete Block, Masonry, and Stone; and
5. Add *Exterior Insulation Finishing Systems ("EIFS")* and *metal and vinyl lap board siding* as specifically prohibited materials.

CORB considered this matter at its June 11, 2013 meeting and directed the COTW to consider its recommendations on this matter. At its July 16, 2013 meeting, COTW in its review of the Plan Commission recommendation concurred with much of the Plan Commission and CORB recommendations. Both CORB and COTW have requested the Plan Commission to review this matter again with a narrowed focus. More specifically, at the Plan Commission meeting on September 11<sup>th</sup>, COTW has remanded this matter to the Plan Commission to consider:

1. Review and enhance proposed definitions related to building materials,

2. Review pre-cast stone to determine if it is an acceptable material,
3. Review and possibly develop administrative process for approving other alternative “high quality materials” not identified in the Zoning Code, and
4. Reconfirm the remaining elements of the Plan Commission’s recommendation made at the March 6, 2013 PC Meeting.

### 1. Proposed Definitions

In discussing building product and building material terminology, it was determined by CORB and COTW that refinements to some of the proposed definitions were in order and requested staff, in concert with a representative of the Brick Industry (who was present at the CORB meeting), to review and propose language refinements to the definitions as warranted, and to present these proposed definitions to CORB for consideration. Attached are these proposed definitions.

### 2. Pre-Cast Stone

CORB and COTW also expressed concerns regarding the durability of pre-cast stone and questioned whether it was an acceptable product to be considered as a “high quality material” and allowed to be used on at least 75% of exterior walls. On this matter, CORB and COTW recommends that the acceptability of pre-cast stone material, as a high quality material, be reconsidered by the Plan Commission and recommends that this matter be remanded back to the Plan Commission for further consideration.

Staff conducted research on the topic of pre-cast stone as a desirable or undesirable material. This research included the State of Illinois and Federal requirements for standards on building materials (specifically pre-cast stone), as well as industry interest groups and consumer groups for further information. Staff found many widely reported issues with pre-cast stone as a building material. The Federal General Services Administration lists the following as recognized problems with the use of pre-cast stone as a building material, and warns home-builders to be aware of the potential for these to occur:

#### *Natural or Inherent Problems*

- Facing Delamination: Common with dry-tamp cast stone; can result from flaws in manufacturing, or from differences in water absorption ratios combined with freeze/thaw cycles.
- Carbonation: Loss of alkalinity.
- Aggregate/Alkali Reaction.
- Freeze/Thaw: May result in surface scaling.
- Erosion: Visible as weathering of the aggregate and cement binder; surfaces look sandy, rough, with exposed aggregate and pockmarks; horizontal surfaces are especially vulnerable.
- Some types of cast stone (those containing calcareous sediments such as limestone) are sensitive to acidic environments.
- Cracking and Spalling: Typically caused by corrosion of metal reinforcement materials; visible as rust stains.

#### *Human-Induced Problems*

- **Crazing:** Hairline cracks common especially with dry-tamp cast stone; a problem often caused by volume differences between the facing and backup material, or improper proportioning of the facing mix; visible by fine hairline cracks.
- When aggregates of uniform size are used, the cast stone tends to be more porous and less durable.

It appears that most of the issues that arise with pre-cast stone are aesthetic in nature, with many of these resulting from improper construction and installation. Serious structural problems may also arise, however, if pre-cast stone is not properly fabricated or installed. For example, corrosion of embedded reinforcing steel can lead to cracking at the surface of the cast stone. This type of cracking also accelerates the process of delamination, leading to further instability of the cast stone and to an unsightly appearance.

In spite of the potential hazards, pre-cast stone is generally accepted as an appropriate building material by public and private institutions. IDOT is one example, although it also provides a comprehensive guideline with strict quality standards for the use of pre-cast concrete and stone.

#### Selected Comparative Communities' Standards

##### *Kenilworth*

The Community Development Director of Kenilworth informed staff that the Village considers pre-cast stone as one of the three acceptable building materials on exterior walls of buildings in the Business District.

##### *Glenview*

The Village of Glenview allows for pre-cast stone as one of many building materials allowed in its business district, but encourages accent colors to appropriately match any awnings or umbrellas.

### 3. Administrative Approval Process

CORB and COTW also were desirous for the Village to be responsive to new or emerging building material products that conceivably would be viewed as high quality materials. CORB and COTW were hopeful an administrative process could be created that would authorize staff to approve alternative products not specifically listed or identified as high quality materials within the Ordinance. CORB and COTW recommend that this matter concerning a possible administrative review and approval process for alternative material be discussed by the Plan Commission.

Staff researched other communities in the region and nationally and was unable to find any approval process that occurred on a staff level. If an alternative material were proposed communities handled these requests by either Text Amendment to include the new material or by an established Design Review Committee.

### 4. Reconfirm Other Recommendations

Except for refinements to the definitions and its recommendation to remand back for consideration whether pre cast stone is an acceptable high quality material and consideration of an administrative approval process for alternative high quality material, CORB and COTW was supportive of the remainder of the Plan Commission recommendation and recommends that it be approved by the Village Board. As part of any motion the Plan Commission should include affirmation of the previous recommendation made on March 6, 2013.

## **Attachments**

1. Proposed Definitions
2. What is Cast Stone? Concrete Repair Bulletin, July/August 2009

## Proposed Definitions

**BRICK:** A molded rectangular block primarily comprised of clay and/or shale, fired with natural gas or coal at approximately 2000 degrees to fuse the shale or clay into a durable building unit that is laid contiguously with joints between the units filled with mortar and ~~Either: (i) kiln-fired clay or shale brick that is not underfired and is manufactured to ASTM C216 or C652, Grade SW; or (ii) concrete brick with integral coloration, manufactured to ASTM C1634.~~ Brick shall not be painted, and shall have a minimum thickness of two and one quarter inches when applied as a veneer.

~~DECORATIVE~~ **ARCHITECTURAL CONCRETE MASONRY UNIT BLOCK:** Either architectural concrete block or brick. Architectural concrete block and concrete brick have highly-textured finishes, in a variety of styles such as indented, split, hammered, fluted, or ribbed or similar architectural finish. Natural or synthetic pigmentation is added during the manufacturing process to produce color variations.

**MASONRY:** Clay brick, stone, rock, or other substantially similar materials, laid contiguously with joints between ~~installed up units by unit set in~~ filled with mortar.

**NATURAL STONE:** Naturally-occurring granite, marble, limestone, slate, river rock, sandstone, other native stone and other similar hard and durable all weather stone that is customarily used in exterior building construction.

**MANUFACTURED STONE** may also include cast, pre-cast decorative or ~~manufactured~~ simulated stone product, provided that such product is not painted, yields a highly textured stone-like appearance, with coloration that is added during the manufacturing process to produce color variations integral to the masonry material, and is highly durable and ~~maintenance free.~~ **Manufactured** ~~Natural or man-made~~ stone shall have a minimum thickness of two and five eighths inches when applied as a veneer.

~~Strikethrough:~~ Deleted Language

Red Text: Added Language

# What is Cast Stone?

By Matthew C. Farmer

**C**ast stone masonry is a form of precast concrete that attempts to replicate the texture, appearance, and workability of natural dimension stone. Cast stone has a rich and successful history of use in construction. It also possesses many unique features inherent with its fabrication and raw material selection that can greatly enhance the beauty of a project in a cost-effective manner.

As with other man-made construction materials, careful attention must be paid during the fabrication and installation processes to avoid defects and deficiencies in the finished product. Through an understanding of the standards governing the industry, careful control of raw materials, and stringent quality control testing, cast stone can be an attractive and durable alternative to natural stone. Too often, however, inferior cast stone is produced and delivered to a project site, detracting from the appearance of the structure and increasing maintenance requirements to the building owner.

## What is Cast Stone?

Cast stone is defined by the Cast Stone Institute (CSI)<sup>1</sup> as, "...a highly refined architectural precast building stone manufactured to simulate natural cut stone." Properly manufactured cast stone is dense and well consolidated. CSI recommends a compressive strength in excess of 6500 psi (45 MPa) (ASTM C1194)<sup>2</sup> and maximum absorption rates of 6 and 10% for cold water and boiling methods, respectively (ASTM C1195)<sup>3</sup>. The combination of low absorption and high-compressive strength makes the material generally durable and resistant to freezing-and-thawing distress. The compressive strength of cast stone is usually far greater than is necessary for the application; however, it can serve as an indicator of good quality control and future durability. Cast stone with inferior physical properties, though perhaps adequate for the particular application, may not possess the same service life of a higher quality material whose physical properties are consistent with cast stone industry recommendations.

---

<sup>1</sup>The Cast Stone Institute is an organization composed primarily of cast stone fabricators and other construction professionals involved in the specification, manufacture, and use of cast stone. According to the Cast Stone Institute's *Technical Manual with Case Histories*,<sup>1</sup> fourth edition, "The purpose of the Cast Stone Institute is to improve the quality of cast stone and to disseminate information regarding its use."

As a material, cast stone is really a variation of precast concrete. Besides sharing common constituents, cast stone is typically mixed, formed, cured, and stored in a plant environment like precast concrete, which enables rapid, consistent, and controlled fabrication. As with other concrete products, cast stone can be reinforced to increase its ability to withstand flexural and tensile loads. Despite its many similarities, cast stone does differ from precast concrete in a few ways: the mixtures integrate finer aggregates to more closely simulate the appearance of natural stone, the method of fabrication can involve very little water, and the product is virtually always used in nonstructural applications.

Cast stone can also be subject to similar quality control concerns as precast concrete. These can include a lack of consistency in mixture design causing variations in appearance, premature cracking as a result of inadequate curing or insufficient strength gain prior to form stripping, damage as a result of impact during storage, transport or erection, and contaminants or reactive aggregates in the raw materials that can cause internal distress.

When properly fabricated, cast stone can be a durable and cost-effective substitute for natural stone, but it may not always look like natural stone. Over time, cast stone can develop characteristics such as cracks, crazing, and discoloration that make it appear less like natural stone as it ages. If quality control is poor, these defects can be more apparent and appear earlier in the service life of the material. Manufacturers should be candid with architects and owners about the potential risks associated with cast stone. In essence, it is a good substitute for natural stone, but not an equal.

## History

Cast stone was developed for use in wall construction as a cost-effective alternative to natural stone, primarily as trim, ornamentation, or ornate building façade elements (refer to Fig. 1). It has also been widely used as wall cladding panels. Its cost advantage is primarily due to reduced cost of the raw materials, the ability to mass produce pieces quickly, and the ability to create complex detailing with formwork and casting as opposed to labor-intensive carving or shaping. An extensive range of colors and textures of cast stone are available through the use of varied aggregates, coloring agents, and modifications to

the formwork used for casting. Many designers favor cast stone not only because of the wide range of aesthetic qualities but also because of the uniformity of appearance that can be achieved with a controlled plant fabrication process.

Cast stone was first used in the year 1138, and used extensively in England and France during the 19th century. Several proprietary systems were developed during this period that used unique combinations of natural cements, hydraulic lime, and other binders. These systems were first used in the U.S. during the middle of the 19th century; many have since been abandoned in favor of the components we commonly see today: a combination of portland cement and carefully selected aggregate.

## Fabrication

Two processes are typically used to produce cast stone: the vibratory dry tamping (VDT) method and wet casting. Both have potential advantages and disadvantages.

The VDT method is unique to cast stone fabrication. To achieve the appearance of natural stone, very dry mixtures of fine aggregates, cement, and water are pounded or compressed into a form on the side that will become exposed in the finished structure. This material is referred to as the face mixture. Depending on the depth of the face mixture required and the complexity or relief of the form, the face mixture is placed in layers called lifts to ensure full compaction of the material into the form. A backup mixture, consisting of coarser aggregate, cement, and water is then poured or rammed into place over the face mixture to fill the remaining portion of the form. The material is allowed to harden and cure, and then the form is stripped and the material stored until it is needed on the project site.

Cast stone produced using the VDT method can replicate stone quite accurately and is less susceptible to surface disruption as a result of free water against the form. Quality control, however, is critical to maintain consistency of both the face and backup mixtures. Changes in thickness of the face mixture can result in variations in density and appearance of the face mixture, as well as cracks due to differential shrinkage between the drier face mixture and the wetter backup mixture. Backup mixtures are usually highly variable in content because they are not visible when the finished product is used.

Wet casting of cast stone is virtually identical to the process used for precast concrete: a form is constructed and then filled with a mixture of aggregates, cement, some additives, and water. Some wet-cast methods can involve multiple lifts of material, or variations between the face mixture and backup mixture. It is allowed to harden and cure for a period of time, and then the form is



*Fig. 1: An example of cast stone used as ornament at quoins, belt course, and window surrounds on this circa 1920s building*

removed. The formed product is then stored until it is transported to the building site where it will be used. Its principle advantages include greater consistency in physical properties through the material's thickness and better quality control of the material. Its principle disadvantages include lower production rates due to the longer curing time required before stripping, susceptibility to plastic drying shrinkage if not properly cured, and disruption of the finished faces as a result of trapped water at the form/mixture interface.

Curing methods for either technique are also highly variable depending on the cast stone manufacturer. Some cure their product using water misting, steam, curing compounds, or damp curing. The amount of curing also varies, depending on the fabrication process and storage practices, as well as the demand for the product on the job site. If the cast stone is insufficiently cured, then it can experience excessive shrinkage, causing cracking of the surface and increased water absorption.

## Common Problems

There are a number of common problems that can occur with cast stone. Whereas some of these occur in cast stone produced using both wet casting and the VDT method, the majority of problems observed in modern construction are associated with the VDT method of manufacture. Unless specifically stated otherwise, these discussions will focus on cast stone created using the VDT method.

These problems range in importance from those that may simply affect appearance or accelerate the need for routine maintenance to those that impact the structural integrity of the material and put the public at risk. Several of the more commonly observed deficiencies found with cast stone are discussed in the following.

### Excessive Soiling

Excessive soiling of cast stone surfaces can result from exposure to pollution, soot, and airborne dust. It can also be a result of these materials washing down from other adjacent building surfaces onto the cast stone (refer to Fig. 2). Because cast stone is absorptive as well as somewhat rough in texture, particulates can settle into the cast stone surface, or be deposited there by water. Cast stone with higher absorption and lower surface densities can become soiled more quickly since the surface structure is more open.

### Crazing

Crazing, or craze cracking, is a network of interconnected hairline cracks (refer to Fig. 3). These cracks usually extend only a few millimeters into the cast stone; however, severe crazing can merge

together to form deeper cracks that can allow moisture to reach the interior and, in extreme cases, cause loss of strength or instability. At the very least, these cracks take in moisture and dirt, causing them to discolor. Despite the objectionable appearance and potential for more severe damage, crazing is considered a nonstructural concern and not cause for rejection of cast stone by CSI.<sup>†</sup>

Crazing is thought to be generally caused by shrinkage occurring at the outermost surface prior to the interior portions of the piece. It can be attributed to curing practices, variable cement content at the surface, excessive wetting and drying, or inadequate ventilation behind the cast stone. The process of tamping also contributes to crazing by creating centers of high compaction (where the tamper impacted the material) surrounded by rings of lower compaction. Crazing appears to be more concentrated in the areas of lower compaction or density. The amount of crazing is also more prevalent at locations where the face mixture is thin. The variations in thickness lead to differential drying shrinkage in the face mixture, as well as variations in density that leads to the formation of craze cracking at the surface.

Racking is probably the most common problem associated with cast stone. CSI recommends that pieces containing cracks in excess of 0.005 in. (0.127 mm) not be accepted in a quality cast stone installation.<sup>‡</sup> Cracking can develop as a result of many different conditions and range in impact from cosmetic to a loss of structural capacity. Several of these are described in the following.

### Restraint of Volume Change

Often cast stone is rigidly attached to the backup structure for support, with no allowance for volume change of the material. Although VDT-cast stone is manufactured with a low water content and experiences less shrinkage than wet-cast products, shrinkage does occur and continues for several years after fabrication. The cast stone is also subject to volume changes due to thermal cycling and will typically experience a greater temperature swing than the unexposed backup structure whose temperature range in service is often moderated by the thermally controlled building interior. If the ends of the cast stone are restrained, the differential volume change between the cast stone and the backup structure results in cracks forming across the face of the cast stone (refer to Fig. 4).

Some manufacturers will attempt to control cracking resulting from volume changes by introducing reinforcing steel in both directions across the face of the cast stone piece. Unfortunately, this is often ineffective because of the difficulty in



Fig. 2: Heavily soiled cast stone at a water table due to water run-off

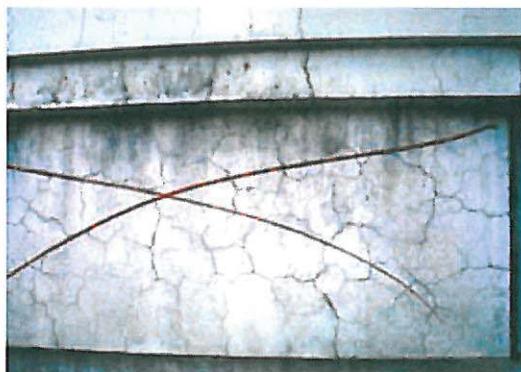


Fig. 3: Heavy crazing in a cast stone façade panel

<sup>†</sup> CSI Technical Bulletin #32, "Crazing."

<sup>‡</sup> CSI Bulletin #36, "Inspection and Acceptance."



Fig. 4: Crack resulting from restraint from shrinkage

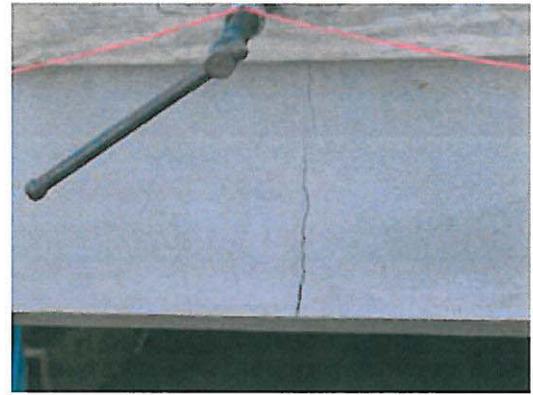


Fig. 6: This crack was formed as a result of excessive loading in flexure



Fig. 5: Cracking that resulted from corrosion of reinforcing steel



Fig. 7: Poor storage and handling cast stone on site can result in improper loading and damage

achieving adequate consolidation of the material around the reinforcing steel to control and distribute the cracking. Poor consolidation around reinforcing steel in the transverse direction (perpendicular to its span) can actually form weak planes through the thickness where cracks are more likely to form.

#### Corrosion

Corrosion of embedded reinforcing steel can lead to cracking at the surface of the cast stone. This type of cracking is often accompanied by delamination of the material at the depth of the reinforcement, leading to further instability of the cast stone in the form of spalls (refer to Fig. 5).

#### Insufficient Strength

Although cast stone is typically not heavily loaded in most building applications, pieces can develop relatively high flexural stresses if they are spanning across openings, not fully bedded along their length, or in any other orientation with just two points of support. The flexural stresses are exacerbated if the piece is long and slender. Flexural cracking will

typically form in the middle third of the span and run from the top to bottom edge of a horizontally oriented piece (refer to Fig. 6) or across a piece that is oriented vertically, such as a window jamb.

#### Handling

Cracks can also develop as a result of mishandling or unintended loading during transportation or erection. Cast stone is usually stripped shortly after forming, moved to a curing facility, moved for cleaning, and moved again to yard storage. It will often be transported to the project site prior to gaining full strength, making it more susceptible to damage while being loaded and unloaded. Cracks can develop if a piece is picked up or stored in a manner not intended (refer to Fig. 7); many cast stone fabricators will furnish lifting hardware cast in larger pieces to avoid damage during lifting operations.

#### Patching Failures

Patching is the process of repairing a spall or chip in cast stone by placing into the defect a fresh,

formable cementitious material mixed to match the damaged cast stone as closely as possible. Patching is usually performed by the manufacturer of the cast stone, after the piece is installed.

Despite duplicating the cast stone components and their proportions, patches rarely match in all environmental conditions because the density and absorption of the original material cannot be replicated when the patch is installed. Because it is so difficult to obtain a good match between the patching material and the cast stone, only damage



*Fig. 8: A patch relying solely on cementitious bond in an overhead condition. Note the failure of the feathered edge at left*



*Fig. 9: Patches that have failed and have become unstable should be removed*

truly noticeable should be repaired (refer to Fig. 8). Pieces with spalls or chips greater than 8 in. (203 mm) square should be replaced. No matter the size, an acceptable patch must not be visible from more than 20 ft (6 m) away.<sup>8</sup>

Patches fail by shrinking excessively or debonding from the substrate. Water can work between the patch and the cast stone substrate through separations at the bondline resulting from shrinkage; water can further degrade the patching material, become trapped and freeze, promote efflorescence, or simply make the patch more visible and detract from the overall appearance of the cast stone. Unstable patches that are debonded from the substrate or severely cracked should be removed to prevent them from falling out on their own, particularly where pedestrians or traffic could be impacted (refer to Fig. 9).

### Corrosion of Reinforcing Steel

Whereas most cast stone is not used in load-bearing applications and does not require reinforcement, reinforcing steel or reinforcing bar can be used to increase its strength in flexure or enhance its ability to be handled or transported without damage. Placement of reinforcing steel is of particular concern in VDT-cast stone panels and should be avoided. Usually the face mixture is placed first, then the steel is set, and the backup mixture poured around it. Therefore, this process does not allow for the reinforcing steel to engage the face mixture. If the backup mixture is dry-tamped as well, it is nearly impossible to achieve adequate consolidation around the reinforcing steel that is sufficient to develop its strength.

If it must be used, wet-cast methods of fabrication are preferable so that the reinforcement can be fully encapsulated in cementitious material. It is also important to provide adequate cover over the steel to increase the time it takes for carbonation to reach the depth of the steel. Reinforcement that is less susceptible to corrosion, such as galvanized or epoxy-coated bars, also help to reduce the risk of corrosion-induced distress.

### Spalling

Spalling and incipient spalling can occur for a multitude of reasons. Spalls can develop at anchor points where stresses are high and the cast stone is cut to receive the anchors. It can also occur as a result of setting procedures—pry bars are often used to position the stone, and the weaker edges and corners can break due to the applied pressure.

### Delamination/Separation of Lifts

Cast stone manufactured using the VDT method is compacted into forms as layers called lifts. These lifts are intended to bond to each other, with the

<sup>8</sup> CSI Technical Bulletin #38, "Patching."

tamper forcing the layers into intimate contact. When the material is spread in the forms, however, the material being the most highly compacted is that closest to the tamper and furthest away from the layer below. This creates zones of lower compaction at the lift lines that can be more absorptive and break down more quickly if exposed to the environment; exposed lift lines can take in a substantial amount of water and erode, leaving fissures on the surface that are visually unappealing and increase the amount of water able to reach the interior of the cast stone.

Often bond at the interface between lifts is lacking. Bond can also be reduced over time if the lift interface is exposed to the environment. Without adequate bond or mechanical engagement, the outer lift (often the face mixture) can separate from the back-up mixture and become unstable (refer to Fig. 10). This condition, in combination with the presence of cracks in the face mixture, can allow portions of the face mixture to fragment and spall.

## Solutions

Over the years, many creative approaches have been developed to restore, repair, and maintain cast



*Fig. 10: Delamination of the face mixture from a large spandrel panel. Round patches are patches from prior core sampling*

stone. The cast stone industry and professionals engaged to correct deficiencies in building materials have developed repair materials and methods to address many of the problems previously mentioned. Some of the more common repair/maintenance approaches are discussed in the following.

### Cleaning

Most soiling can be treated successfully with conventional water rinses, detergents, or chemical

cleaners. The use of more aggressive cleaners, such as those containing acids, should be avoided or used judiciously since they can dissolve the cementitious binders in the material and lead to erosion and roughness. Cleaning with high-pressure water (greater than 300 psi [2 MPa]) should also be avoided as it can remove the paste surrounding the aggregate, roughening the surface. If the surface of the cast stone is rough or cracks are present, more debris is retained, making the material "dirtier" in appearance. Cracks are more difficult to clean because the soiling material is drawn more deeply into the crack where conventional surface cleaning may not reach.

### Water Repellent Application

The application of penetrating water repellents such as silane and siloxane blends to cast stone can reduce its absorption and improve its resistance to soiling by making the surface hydrophobic and less able to absorb contaminants deposited by water. Low water absorption is critical to maintain durability, reduce the appearance of crazing, and reduce soiling overall.

If cast stone is exhibiting visible crazing, water repellents can be applied after cleaning to help prevent the crazing from becoming more pronounced. It prevents contaminants from being redeposited in the cracks and also prevents water from wicking into the body of the cast stone.

### Re-etching

When originally fabricated, a cleaning solution most often containing muriatic acid is used to remove the excess paste at the surface and to expose the brighter stone aggregate. Occasionally, if soiling is severe, or if crazing is visible and darkened by contaminants filling the surface of the fine cracks, a similar acid-based wash can be used to improve the appearance. The stronger cleaning solution aggressively attacks the material in the cracks and surface irregularities. The author's experience suggests that the appearance of shallow crazing can be improved with this method and is worth attempting; however, older crazing that penetrates more deeply into the surface is not typically improved by the application.

One must also consider, however, that it is far more difficult to apply an acid wash to cast stone once it is in place, particularly if it is oriented in a vertical position. Adjacent surfaces often must be protected from damage by the caustic cleaners, and run-off must be collected and neutralized or otherwise controlled.

### Architectural Coating

When the cast stone is severely crazed, soiled, discolored, or contains a number of poorly matched patches, an architectural coating or pigmented sealer can be an attractive option. Although the original

appearance simulating natural stone is lost, coating and pigmented sealers offer a consistent, fresh appearance (refer to Fig. 11). Coatings will bridge small cracks and surface irregularities, and provide a water-resistant finish for the cast stone, reducing future concerns about absorption.

Coatings can range in formulation from acrylic elastomeric to potassium silicate-based materials. The most critical characteristic is breathability, or its ability to allow water vapor to pass from the cast stone to the exterior. Coatings that are not sufficiently breathable will trap moisture, peel, blister, and encourage freezing-and-thawing deterioration within the cast stone. Coatings and sealers do require reapplication; our experience suggests recoating should be anticipated every 5 to 10 years, depending on the product and its environmental exposure. Most coatings can be easily cleaned with mild detergents.

### Crack Treatments

Cracks that are nonstructural but can allow excessive water penetration to reach the interior of the cast stone should be sealed. Cast stone producers will often rub a cementitious slurry or grout into the crack, filling up the surface; however, the crack quickly reforms through the thin brittle application. To more successfully seal cracks, the surface of the crack should be widened and deepened to accept an appropriate amount of material. Elastomeric sealant, cementitious grout, and structural adhesives have all been used with mixed results. The more rigid materials have a more pleasing appearance but can fail even if the crack is considered stable. Sealants are more forgiving to movement but can be more visible due to the textural differences between them and the surrounding cast stone.

### Patching

As mentioned previously, patching of cast stone is inevitable. Often, the first priority for the manufacturer is to install a patch that minimizes the impact to the cast stone and matches well. Unfortunately, these patches often fail due to poor surface preparation and reliance on bond strength of the patch material to hold it in place. Because the patch will shrink after its placement in the cast stone, the bond can be broken as a result of the volume change. The manufacturer will also typically taper the patching material out to the edge of the chip or spall, producing a thin, fragile edge. These feathered edges do not have the integrity to stay bonded and break off, leaving the rest of the patch vulnerable to increased water penetration.

Proper patch installation must make compromises in the appearance. A spall must sometimes be broadened and deepened so that the patch material will be firmly engaged into the surface. The edges



Fig. 11: Example of architectural coating applied over supplemental anchors

must be cut to eliminate feather edging, and mechanical anchoring is necessary to ensure that if the patch does lose bond, it will stay engaged in the substrate (refer to Fig. 12). These practices will produce a patch that is more noticeable, but one that will be far more durable.

## Industry Quality Control

The document most referenced by architectural specifications today with respect to the design, use, and manufacture of cast stone is the CSI *Technical Manual*; it has become the most widely accepted standard for the industry. Most specifications will defer to the requirements for cast stone expressed in its standard specifications. This document in turn references ASTM C1364, "Standard Specification for Architectural Cast Stone,"<sup>4</sup> which is maintained by ASTM Committee C-27, Precast Concrete Products, and the direct responsibility of Subcommittee C27.20, Architectural and Structural Products. The language between these two documents is quite similar, with the CSI specification inclusive of the ASTM requirements.

Although many improvements have been made recently regarding the quality of cast stone as a result of more stringent requirements for quality control in the industry, there are still some areas where the standard specification and industry requirements could be improved to better ensure a quality product will be delivered and the material's end user will be satisfied.

Cast stone is a unique material that offers modern designers the appearance of natural stone, but with all the advantages of a manufactured product. Conversely, proper manufacturing processes and quality control are critical to



Fig. 12: Preparation for patch repairs. Note the saw cut edges and supplemental anchors to engage the new patch material

providing a good cast stone product. A better understanding of the material's advantages and limitations are essential to make certain that all parties involved in the cast stone application are pleased with the final installation.

## References

1. *Technical Manual with Case Histories*, fourth edition, Cast Stone Institute, Lebanon, PA, 2005. 80 pp.
2. ASTM C1194-03, "Standard Test Method for Compressive Strength of Architectural Cast Stone," ASTM International, West Conshohocken, PA, 2003. 2 pp.
3. ASTM C1195-03, "Standard Test Method for Absorption of Architectural Cast Stone," ASTM International, West Conshohocken, PA, 2003. 2 pp.
4. ASTM C1364-02, "Standard Specification for Architectural Cast Stone," ASTM International, West Conshohocken, PA, 2002. 3 pp.
5. Pieper, R., "The Maintenance, Repair, and Replacement of Historic Cast Stone," *Preservation Brief No. 42*, Technical Preservation Services, National Park Service, U.S. Department of the Interior, Washington, DC, 2001. 16 pp.
6. Russell, B., "Specifying Cast Stone for Masonry Construction," *Structure Magazine*, May 2005, pp. 13-16.



**Matthew C. Farmer** joined the New Jersey office of *Wiss, Janney Elstner Associates, Inc. (WJE)*, in 1986. He served as *Manager of the Fairfax office* from 1994 until 2006, when he became a *Principal with WJE*. He has been involved with numerous evaluations of concrete, steel, and timber structures, as well as those involving clay, concrete, stone, and cast stone masonry. He has concentrated his practice in the area of building envelope cladding systems design, investigation, analysis, and repair; including numerous engagements as an expert witness. He is a graduate of the *University of Colorado and Cornell University* and is licensed as a *Professional Engineer in the District of Columbia, Virginia, and Maryland*.



---

## MEMORANDUM

---

**TO:** Chairman Eisterhold  
Members of the Plan Commission

**FROM:** Aaron N. Cook, AICP  
Development Manager

**DATE:** September 9, 2013

**SUBJECT:** **Health Club or Private Recreation and Firearms Shooting Ranges – Zoning Code Text Amendment** *(Continued from July 10, 2013)*

---

Unfortunately, due to the unexpected departure of the Community Development Department Intern, staff was unable to complete the requested research for the pending Text Amendment relative to Health Club or Private Recreation and Firearms Shooting Ranges. As a result, staff recommends that the matter be continued to October 2, 2013 in order to provide time for remaining staff to complete the research as requested. Staff apologizes for need to continue the matter.

For the Plan Commission's information, attached is Resolution R2012-1710 which referred this matter to the Plan Commission for discussion. The Resolution was referenced in the July 10, 2013 Staff Report to the Plan Commission (also attached) however the document was not included.

**Attachments:**

1. Resolution No. R2012-1710
2. Staff Report: July 10, 2013 Plan Commission Meeting

VILLAGE OF LINCOLNWOOD

RESOLUTION NO. R2012-1710

A RESOLUTION INITIATING AND REFERRING TO  
THE PLAN COMMISSION A PROPOSED ZONING ORDINANCE AMENDMENT  
REGARDING FIREARMS SHOOTING RANGES AND THE DEFINITION OF  
"HEALTH CLUBS AND PRIVATE RECREATION"

WHEREAS, the Village of Lincolnwood is a home rule municipal corporation in accordance with Article VII, Section 6(a) of the Constitution of the State of Illinois of 1970; and

WHEREAS, the Village has the authority to adopt ordinances and to promulgate rules and regulations that pertain to its government and affairs, and to review, interpret and amend its ordinances, rules and regulations; and

WHEREAS, Section 2.02 of "The Village of Lincolnwood Zoning Ordinance," as amended ("*Zoning Ordinance*"), defines "Health Club or Private Recreation" as "A building or portion of a building designed and equipped for the conduct of sports, exercise, leisure time activities, or other customary or usual recreational activities, operated for profit or not-for-profit and which can be open only to members and guests of the organization or open to the public for a fee."; and

WHEREAS, The Zoning Ordinance allows health clubs and private recreation facilities as a permitted use in the B-2 and B-3 Districts of the Village, and as a special use in the B-1, O-1, and M-B Districts of the Village; and

WHEREAS, the Village Staff has historically and consistently interpreted the term "Health Club or Private Recreation" to exclude firearms shooting ranges; and

WHEREAS, the Village Board of Trustees concurs with the interpretation by Village Staff of the term "Health Club or Private Recreation"; and

WHEREAS, in connection with a recently withdrawn application for zoning relief for the operation of a firearms shooting range, the Village Board had the occasion to consider the issue of shooting ranges, and the appropriate location and zoning of such uses in the Village, and determined that further deliberation and action on this issue is necessary; and

WHEREAS, the Village Board now desires to initiate an amendment to the Zoning Ordinance to: (i) formally codify the Village's determination that "Health Club or Private Recreation" excludes firearms shooting ranges; and (ii) establish that firearms shooting ranges may be allowed as a special use in the M-B District of the Village, subject to distance, location, and other restrictions that may be determined to be necessary and desirable during the public hearing, report and recommendation process (collectively, the "*Proposed Amendment*"); and

WHEREAS, pursuant to Sections 5.16(2) and (3) of the Zoning Ordinance, the Village Board has determined that it is appropriate and in the best interest of the Village to initiate the Proposed Amendment and forward it to the Plan Commission for a public hearing and a report and recommendation back to the Village Board;

NOW, THEREFORE, BE IT RESOLVED BY THE PRESIDENT AND BOARD OF TRUSTEES OF THE VILLAGE OF LINCOLNWOOD, COOK COUNTY, ILLINOIS, as follows:

SECTION 1. RECITALS. The facts and statements contained in the preamble to this Resolution are found to be true and correct and are hereby adopted as part of this Resolution.

SECTION 2. INITIATION AND REFERRAL OF PROPOSED AMENDMENT TO PLAN COMMISSION. Pursuant to Sections 5.16(2) and (3) of the Zoning Ordinance, the Village Board shall, and does hereby, initiate the Proposed Amendment and refer it to the Plan Commission for a public hearing and a report and recommendation back to the Village Board.

SECTION 3. EFFECTIVE DATE. This Resolution shall be in full force and effect from and after its passage and approval as provided by law.

PASSED this 9<sup>th</sup> day of November, 2012.

AYES: Trustees Patel, Swanson, Heidtke, Elster, Sprogis-Marohn

NAYS: None

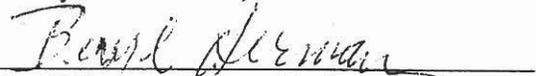
ABSENT: Leftakes

ABSTENTION: None

APPROVED by me this 9<sup>th</sup> day of November, 2012.

  
\_\_\_\_\_  
Gerald C. Turry, President  
Village of Lincolnwood, Cook County, Illinois

ATTESTED and FILED in my office this  
9<sup>th</sup> day of November, 2012

  
\_\_\_\_\_  
Beryl Herman, Village Clerk  
Village of Lincolnwood, Cook County, Illinois

**Agenda Item #5**



**Staff Report  
Plan Commission**

**July 10, 2013**

*Continued from May 1, 2013, March 6, 2013 & February 6, 2013*

**Subject Property: N/A (Text Amendment)**

**Zoning District:** Requested action is applicable to the B1, B2, B3, O-1, and M-B Districts.

**Requested Action:** Text Amendment to Various Sections of the Zoning Ordinance including but not limited to the Definitions Section, Land Use Table, Off-Street Parking Schedule, etc. relative to modifying the definition of Health Club or Private Recreation, Firearms Shooting Ranges.

**Nature of Request:** Consideration to Modify and/or add certain definitions including but not limited to Health Club or Private Recreation and Firearms Shooting Ranges; Consideration of Firearms Shooting Ranges as a Special Use in the M-B District subject to certain restrictions; and the Addition or Modification of Off-street Parking Requirements for Firearms Shooting Ranges, Commercial Recreation Facility, or Other Similar Uses

**Petitioner:** Village Board

**Summary**

*Note: Below is the summary provided at the May 1, 2013, March 6, 2013 and February 6, 2013 Plan Commission meeting. This matter was continued.*

On November 9, 2012, the Village Board adopted Resolution No R2012-1710, initiating a referral to the Plan Commission for text amendments to the Zoning Ordinance. These proposed text amendments concern Firearms Shooting Ranges and the existing definition found in the Zoning Ordinance for "Health Clubs and Private Recreation". The purpose of these text amendments is to:

1. Specifically exclude from the definition of "Health Clubs and Private Recreation", Firearms Shooting Ranges, and
2. Establish in the Zoning Ordinance, "Firearms Shooting Ranges" as a Special Use in the M-B Zoning District.

The adopted Village Board Resolution suggests, but does not identify, that other restrictions may be determined to be necessary and desirable in establishing Firearms Shooting Ranges as a Special Use in the M-B Zoning District.

## Definitions

Currently, within the Zoning Ordinance the pertinent definition reads:

### **HEALTH CLUB or PRIVATE RECREATION**

A building or portion of a building designed and equipped for the conduct of sports, exercise, leisure time activities, or other customary or usual recreational activities, operated for profit or not-for-profit and which can be open only to members and guests of the organization or open to the public for a fee.

For public hearing consideration, staff suggests that this definition be broken down into three separate definitions as follows:

**Firearms Shooting Range:** A specialized indoor soundproof facility, supervised by a Range Safety Officer and designed for the safe shooting practice of firearms, such as handguns and rifles.

**Health Club:** A business establishment which promotes physical fitness and which contains equipment for body exercising or other facilities intended to improve physical fitness, diet, weight control and/or health and which is generally utilized by members who pay a periodic fee for facility access and use.

**Commercial Recreational Facility:** A privately owned for-profit commercial facility designed and equipped to provide customary leisure time or recreational activities, such as bowling, swimming, miniature golf, ice skating, tennis, racquetball and like activities.

If the Plan Commission concurs with this definitional breakdown, staff would further recommend that both “Health Club” and “Commercial Recreational Facility” be added to the Use table in the Zoning Ordinance to replace the current listing for “Health Club and Private Recreation” and that this definition be eliminated altogether from the Ordinance.

## Parking Standards

With the addition of a separate listed use in the Use Table for Firearms Shooting Ranges, staff recommends the addition of a parking standard for this use. Previously, based on research, staff recommended a parking standard for shooting ranges as follows:

2 parking spaces per shooting range station plus 3 spaces per 1,000 square feet of floor space not devoted to indoor shooting range.

Should the Plan Commission concur with the separation in the Use Table for “Health Club” and “Commercial Recreation Facility”, staff would also like to review appropriate parking standards for these uses for possible amendment. Currently, the parking standard for “Health club or recreation facility, private” is 1 space per 3 persons+ 1 space per /2 full time employees. Staff notes that currently a separate parking standard already exists for bowling alleys (5 spaces per lane).

## Firearms Shooting Range Use

The Village Board initiated text amendment proposes that Firearms Shooting Ranges be allowed in the M-B District as a Special Use. Last June, as part of the Shore

consideration, a proposed text amendment was presented for Firearms Dealers as a Special Use in the M-B District. This proposal also included:

- an 800 foot minimum distance requirement from any "P" Park zoned property or,
- any "R" Residentially zoned property

The effect of enacting a similar 800 foot minimum distance for Firearms Shooting Ranges from "P" or "R" zoned property would be a significant reduction in the area of the M-B zone in which a shooting range could be located (see attached map). Staff believes that this additional locational restriction within the M-B District has merit for Firearms Shooting Ranges and suggests its consideration as part of these text amendments.

Staff notes that this proposal for an 800 foot minimum distance from R or P zoned property is just that, a minimum distance from property which is *zoned* for Park or Residential use, rather than from how a property might be actually used. This distinction is made since previously some in the community mistook the minimum distance proposal from P zoned property as meaning a required minimum distance from any type of park or open space, such as the proposed bike trail (which is not zoned P). Staff recommends using Zoning Districts and the Village Zoning Map for determining any minimum distance, since this is very objective.

In addition to considering this minimum distance requirement, staff suggests that performance standards for Shooting Ranges be considered for this use. Surprisingly, while there is an estimated 16,000-18,000 indoor shooting ranges in operation in the United States; staff has found few local requirements for them. In reviewing the literature on shooting range uses, the following four areas appear to represent the areas of greatest concern with this use:

1. Customer/Employee Safety
2. Health concerns (lead exposure)
3. Property/Business Security Plan
4. Noise concerns

To respond to these areas of concern, staff recommends that the following Performance Standards for Firearms Shooting Ranges be considered for inclusion in the Zoning Ordinance:

1. Range design must conform to US Department of Energy's Range Design Criteria, dated June 4, 2012 or subsequent editions.
2. Range must conform to US Department of Labor Occupational Safety and Health Administration (OSHA) requirements
3. Range must be in conformance with an established and Police Chief approved Range Safety Plan, which shall specify range safety requirements and procedures for customers and workers.
4. Range must be in compliance with a Police Chief approved Security Plan for the business and property.

5. Range must comply with any and all applicable US or Illinois Environmental Protection Agency requirements concerning lead and noise
6. Range must comply with Village noise restrictions.

Attached is the US Department of Energy Range Design Criteria as well as a recent ordinance which regulates shooting ranges in Richland Township Pennsylvania. To provide additional background, also attached is an August 2012 memorandum prepared by the City of Urbana Illinois concerning a proposed text amendment in that city concerning firearms dealers and shooting ranges.

**Attachments**

1. Map- M-B Parcels not within 800 feet of P or R Zoned Property
2. Department of Energy Range Design Criteria
3. Richland Township Shooting Range Ordinance
4. City of Urbana August 2012 Text Amendment Memo

**Agenda Item #7**



**Staff Report  
Plan Commission**

**September 11, 2013**

*Continued from July 10, 2013, June 5, 2013, May 1, 2013, March 6, 2013 & February 6, 2013*

**Subject Property: N/A (Text Amendment)**

**Requested Action:** Text amendment to consider requirements for Balconies, Porches, and Open Patios or Terraces as Found in the Permitted Obstructions in Yards Table 3.10.01 of the Zoning Ordinance.

**Nature of Request:** A text amendment is proposed to consider modifying or additional bulk requirements for Open Balconies in the Front Yard, Open Patios, and Attached and Covered Porches.

**Petitioner:** Village Board

**Summary**

*Note: Below is the summary provided at the July 10, 2013 Plan Commission meeting. This matter was continued.*

Open Patios – At the June 5<sup>th</sup> meeting, the Plan Commission discussed the intent of the Zoning Code regulation of Open patios or terraces. The Plan Commission requested examples of open patios and terraces. Attached are images of elevated patios and terraces as well as at-grade patios.

As discussed, the current language is unclear as to permitted size and location.

Open Patios or terraces are permitted as follows:

Open patios or terraces in a residential district, provided that they are at least four feet from all side and rear property lines, not over four feet (4') above the average level of the adjoining ground and do not project over ten feet (10') (excludes covered porches)

The Plan Commission deliberated the regulation and provided direction to staff that the requirement is intended to regulate elevated patios and terraces. The Plan Commission discussed that at-grade patios would not be subject to this standard.

Based on the Plan Commission discussion, below is draft language incorporating the comments made by the Plan Commission at the June 5<sup>th</sup> meeting.

Open patios or terraces **in excess of one foot above grade** in a residential district (**excludes at-grade patios**), provided that they are at least four feet from all side and rear property lines, not over four feet (4') above the average level of the adjoining ground and do not project over ten feet (10') **from the front façade** (excludes covered porches)

This draft language provides clarity on the regulation of elevated patios and terraces. Staff raises the question given this clarification on the appropriate regulation, if any, for at-grade patios.

Open Balconies – Table 3.10.01 of the Zoning Ordinance identifies the permitted obstructions in yards, i.e. where items such as electrical generators, detached buildings, decks, and other accessory structures are permitted. Open Balconies are permitted not to exceed 4 feet from the building as per Table 3.10.01 of Zoning Ordinance. Staff notes however, that Open Balconies in the front yard are the only accessory structure which requires a Special Use. Staff seeks guidance on the intent of the bulk regulations for Open Balconies. More specifically:

1. Do all balconies on the front elevation require Special Use approval?
2. Are all balconies permitted as long as they do not exceed 4 feet from the building? In the event a balcony on the front elevation exceeds 4 feet, is a Special Use required to permit such a balcony?
3. If the Principal Structure is located at the minimum front setback line and a balcony is on the front elevation does this balcony at any distance from the building require a Special Use?

Staff's recommendation is to identify the permitted dimension of a balcony (currently extending 4 feet from the building) and identifying which elevations such a balcony complying with the dimension is permitted. Once this is determined it is staff's recommendation that all other balconies be subject to approval by the variation process and hardship standards.

Porches – Attached and covered porches are permitted in all yards provided they are at least four feet from all side and rear property lines. This regulation appears to leave it open for a property owner to construct a nearly unlimited covered porch in their front and rear yards. Staff does not believe that this is the intention of the standard so at the February 6<sup>th</sup> meeting staff seeks discussion on what the reasonable limitation should be for covered porches. It is likely that staff will then need to draft language for review at a future meeting.

**Attachments:**

1. Zoning Ordinance Excerpt: Article III Table 3.10.01
2. Pictures of Elevated Patios, Terraces, and At-grade patios

**PART C: YARDS, ALLOWABLE OBSTRUCTIONS**

**3.09 YARDS, GENERAL**

- (1) The minimum yard space required under the terms of this Zoning Ordinance for one structure shall not again be considered as yard space for another adjoining structure.
- (2) No lot shall be reduced in area so that the yards or other open space and lot area become less than required by this Zoning Ordinance.
- (3) On streets where a front yard setback of more or less depth than required by Article IV has been maintained for buildings existing on lots or tracts having a frontage of forty percent (40%) or more of the total frontage on one side of a block, the front yard setback line for each remaining vacant interior lot along the block shall be the average distance of the existing front yard setbacks on that side of the block.
- (4) On a vacant through lot, both lot lines adjacent to a street shall be established as the front line, except that where two (2) or more through lots are contiguous and a front line has been established by an existing principal building on the contiguous through lot, the same street line shall thereafter be deemed to be the front lot line.

**3.10 PERMITTED OBSTRUCTIONS IN REQUIRED YARDS**

Table 3.10.01 below (Permitted Obstructions in Yards) list items which are permitted in yards and the location (e.g. front yard) where they are permitted. A “P” denotes that an obstruction is permitted; an “S” denotes that an obstruction is considered a special use and may be permitted in the subject district only after review and approval in accordance with Article V of this Ordinance; a “-”denotes that the obstruction is prohibited. (Ordinance No 2012-2987)

Table 3.10.01 Permitted Obstructions in Yards	Yard			
	Front	Rear	Side	Corner Side
Air conditioning window units, provided they do not extend more than two feet from window	P	P	P	P
Air conditioning condensers and equipment, other than window units, provided they are at least ten feet from side property lines and at least twenty feet from an adjoining residential property owner’s window	-	P	-	P
Arbors and trellises, not in excess of 8 feet in height	-	P	P	P
Architectural projections of sills, belt courses, cornices and ornamental features projecting nor more than eighteen (18) inches into a yard	P	P	P	P
Awnings and canopies, projecting into a yard not more than 10% of the depth of front or rear yard or 25% of the width of side yard	P	P	P	P
Backup electrical generator, provided they are at least ten feet from all property lines.	-	P	-	-
Balconies, open, not to exceed 4 feet from building	S	P	-	-

Table 3.10.01 Permitted Obstructions in Yards	Yard			
	Front	Rear	Side	Corner Side
Basketball hoop on non-recreation land use, limited to one pole- or garage-mounted goal and shall be at least five feet from all property lines	P	P	-	P
Bay windows, one-story and projecting three (3) feet or less in the yard	P	P	P (Note 1)	P
Chimneys projecting twenty-four (24) inches or less into the yard	P	P	P	P
Covered entry structure, located as part of a primary or secondary entrance, open on 3 sides, not to exceed 3 feet from building	P	P	P	P
Decks, in a residential district, provided that they do not exceed required building setbacks	-	P	-	-
Detached garages, provided that such accessory building shall not cover more than 30% of the required area of a rear yard	-	P	-	-
Dish antennas – greater than 30" diameter	-	P	-	-
less than 30" diameter	-	P	P	-
Fallout shelters, attached or detached	-	P	-	-
Fences (pursuant to Section 3.13 of this Article)	-	P	P	P
Flagpoles	P	P	P	P
Open patios or terraces in a residential district, provided that they are at least four feet from all side and rear property lines, not over four feet (4') above the average level of the adjoining ground and do not project over ten feet (10') (excludes covered porches)	P	P	P	P
Overhanging eaves and gutters projecting 36 inches or less into the yard and are not less than two (2) feet from any lot line	P	P	P	P
Playground equipment	-	P	-	-
Porches, attached and covered and provided they are at least four feet from all side and rear property lines	P	P	P	P
Ramps for use by disabled persons	P	P	P	P
Sheds, storage buildings, gazebos, etc.	-	P	P	-
Solar Panels	-	P	P	P
Steps, open without roof	P	P	P	P
Swimming pools, private, located not less than five (5) feet from a lot line and ten (10) feet from the nearest overhead utility; swimming pools must be completely enclosed by fencing	-	P	-	-
Television, radio towers or antennas, provided that they are at least five feet from all property lines as permitted in Section 3.12 of this Article.	-	P	P	-

Elevated Patios and Terraces







At-Grade Patios







---

## MEMORANDUM

---

**TO:** Chairman Eisterhold  
Members of the Plan Commission

**FROM:** Aaron N. Cook, AICP  
Development Manager

**DATE:** September 6, 2013

**SUBJECT:** **Comprehensive Plan Amendment – Review of Lincoln Avenue Task Force Report**

---

At the Committee of the Whole (COTW) meeting of July 16, 2013, the Committee discussed multi-family housing in the Lincoln Avenue corridor. The Zoning Code regulations for housing along Lincoln Avenue have their origins in the recommendation and vision adopted as part of the Lincoln Avenue Task Force (LATF) Report. Certain other recommendations of the LATF Report such as land use, design requirements, parking requirements, etc. have been incorporated into the Zoning Code. The LATF Report was adopted and made part of the Comprehensive Plan in May 2006.

At the July 16<sup>th</sup> COTW meeting, discussion included a possible prohibition on residential developments along Lincoln Avenue which is not consistent with the Zoning Code or the Comprehensive Plan. Trustee Cope indicated his concern over residential developments along Lincoln Avenue include excessive building height, increased traffic generation, and decreased property values. Rather than focus on the housing recommendations only, several trustees recommended a full review of the LATF Report. At the conclusion of the July 16<sup>th</sup> discussion, the COTW directed the Plan Commission to hold a public hearing to fully review the Lincoln Avenue Task Force Report. As part of this referral, the Village Board requests consideration by the Plan Commission to place a moratorium on residential development along the Lincoln Avenue corridor while the LATF Report is under review.

At the September 11<sup>th</sup> Plan Commission meeting the members should be prepared to 1) recommend to the Village Board on adopting a moratorium on residential development along the Lincoln Avenue corridor and 2) discuss the LATF Report beginning with the recommendations relative to residential development but also the report as a whole. Upon completion of the review of the LATF Report, if recommendations are made to change the findings of the Report,

it is likely that the text of the Zoning Code will need to be amended to ensure consistency between the Comprehensive Plan and the Zoning Code.

**Attachments:**

1. July 16, 2013 Committee of the Whole Minutes
2. Ordinance No. 2006-2710: An Ordinance Amendment the Village of Lincolnwood's Comprehensive Plan to Incorporate the Report and Recommendations of the Lincoln Avenue Task Force

indicated that NCG does not object to expanding the TIF District. Mr. Elrod informed the Village Board that his staff would prepare a Resolution for consideration at the August 20 Village Board meeting which would confirm that NCG is paying for the study and authorizing staff to proceed.

### **3. Discussion Concerning the Purple Hotel Site Restoration Standards**

The item was presented by Mr. Cook who indicated that the Pre-Development agreement between NCG and the Village for the Purple Hotel expired on July 1, 2013. NCG is requesting reinstatement of the agreement plus additional items. Staff is recommending that NCG be required to submit for land-use relief no later than December 1, 2013, receive land-use approval no later than July 1, 2014, and be permitted to extend site restoration waivers for the adjoining properties until July 1, 2014. Mr. Firsell reported that all asbestos had been abated from the Purple Hotel and the demolition contractor is ready to begin demolishing the building. Mr. Stein indicated that the only items which are pending are the pre-development agreement and the Illinois Department of Transportation's approval. Discussion ensued. The Village Board directed that the subcommittee of Trustees Elster and Patel convene a meeting to discuss the terms of the new pre-development agreement

### **4. Discussion Concerning the Design Standards for Non-Residential Structures**

The item was presented by Mr. Cook. On April 16, 2013, the Village Board considered a recommendation by the Plan Commission concerning proposed changes to the Village's design standards for non-residential buildings. These design standards are commonly referred to as the Village's masonry requirements. In considering this matter at the April 16, 2013 meeting, the Village Board referred this item to the Committee on Ordinances, Rules & Buildings (CORB) for further deliberation. The Plan Commission recommendation on this matter included the following:

1. Eliminating "Masonry Only" requirement from the Lincoln Avenue Overlay District
2. Establish that the exterior walls of non-residential buildings be constructed with a minimum of 75% "high quality materials". This includes a requirement that from the ground level to the top of the first floor windows must be constructed entirely of "high quality materials"
3. Establish a list of permitted "high quality materials" which includes:
  - 1) Brick with a minimum thickness of 2.25 inches
  - 2) Natural stone
  - 3) Sandstone
  - 4) Other native stone
  - 5) Pre-cast decorative stone
  - 6) Glass
4. Add definitions for Brick, Decorative Concrete Block, Masonry, and Stone; and
5. Add *Exterior Insulation Finishing Systems ("EIFS")* and *metal and vinyl lap board siding* as specifically prohibited materials.

CORB considered this matter at its June 11, 2013 meeting and now seeks to have the COTW consider its recommendations on this matter. Discussion ensued. The Village Board referred CORB's recommendation to the Plan Commission for their consideration.

### **5. Discussion Concerning Multi-Family Housing Zoning in the Lincoln Avenue Corridor**

The item was introduced by Mr. Wiberg who indicated that Trustee Cope requested the discussion of multi-family on Lincoln Avenue be discussed at the Committee of the Whole. Mr. Cook indicated that zoning for multi-family housing was the result of the Lincoln Avenue Task Force Plan and provided further background on the matter. Trustee Cope indicated his opposition to allowing the condo development on 6535 North Lincoln Avenue with concerns relating to height, traffic, and affecting other area property values. He proposed prohibiting such developments from Lincoln Avenue in the future and for the Village to place a moratorium in the immediate term. Trustee Patel suggested that the Plan Commission review the Lincoln Avenue Task Force Plan in its entirety. Mr. Cook recommended that the Comprehensive Plan be reviewed first. The Village Board concurred. The matter of reviewing the Plan and placing a moratorium on multi-family housing on Lincoln Avenue was referred to the Plan Commission

### **Adjournment**

VILLAGE OF LINCOLNWOOD

ORDINANCE NO. 2006 2710

AN ORDINANCE AMENDING THE VILLAGE OF LINCOLNWOOD'S COMPREHENSIVE  
PLAN TO INCORPORATE THE REPORT AND RECOMMENDATIONS OF THE LINCOLN  
AVENUE TASK FORCE

ADOPTED BY THE  
PRESIDENT AND BOARD OF TRUSTEES  
OF THE VILLAGE OF LINCOLNWOOD  
THIS 18TH DAY OF MAY, 2006

ORDINANCE NO. 2006-2710

**AN ORDINANCE AMENDING THE VILLAGE OF LINCOLNWOOD'S COMPREHENSIVE PLAN TO INCORPORATE THE REPORT AND RECOMMENDATIONS OF THE LINCOLN AVENUE TASK FORCE**

WHEREAS, the Village of Lincolnwood (the "Village") is a home rule municipality in accordance with Article VII Section 6(a) of the Constitution of the State of Illinois of 1970;

WHEREAS, the Village has the authority to adopt ordinances and to promulgate rules and regulations that pertain to its government and affairs;

WHEREAS, on November 1, 2001, the Village Board adopted the Village of Lincolnwood Comprehensive Plan (the "Comprehensive Plan");

WHEREAS, the Comprehensive Plan includes, but is not limited to, a policy guide for future Village Board decisions concerning land use within the Village;

WHEREAS, subsequent to the adoption of the Comprehensive Plan, the Village created the Lincoln Avenue Task Force (the "Task Force") to propose a plan for retail redevelopment of properties along the Lincoln Avenue Corridor (the "Corridor") that would be attractive and image enhancing, providing residents with a safe, convenient and pedestrian-friendly environment;

WHEREAS, in November, 2005, the Task Force completed its report and recommendations, with exhibits (the "Report") for enhancement and redevelopment of the Corridor;

WHEREAS, on April 19, 2006, pursuant to a timely, legal notice of public hearing published in the *Lincolnwood Review*, the Task Force presented the Report to the Plan Commission for its review, consideration and recommendation to the Village Board regarding incorporating the Report into the Comprehensive Plan;

WHEREAS, at the April 19, 2006, public hearing all interested persons were allowed the opportunity to present their views regarding the Report, pro and con, to the Plan Commission;

WHEREAS, subsequent to the April 19, 2006, public hearing, the Plan Commission made a recommendation to the Village Board to incorporate the Report into the Comprehensive Plan;

WHEREAS, the Report is attached hereto and incorporated herein by reference as Exhibit A;

WHEREAS, the Village Board reviewed the Report and recommendation of the Plan Commission and determined that it is in the public interest and will promote the public health, safety and welfare of the Village and its residents if the Report is incorporated into the Comprehensive Plan.

NOW, THEREFORE, BE IT ORDAINED by the President and the Trustees of the Village Board as follows:

Section 1. The facts and statements contained in the preamble to this Ordinance are found to be true and correct and are hereby adopted as part of this Ordinance.

Section 2. The Report attached hereto as Exhibit A is hereby incorporated into and made a part of the Comprehensive Plan as Appendix G.

Section 3. In the event there are any inconsistencies between the Comprehensive Plan and the Report, the Report shall control.

Section 4. In all other respects, the Comprehensive Plan shall remain the same.

Section 5. If any section, paragraph or provision of this Ordinance shall be held to be invalid or unenforceable for any reason, the invalidity or unenforceability of such section, paragraph or provision shall not affect any of the remaining provisions of this Ordinance.

Section 6. This Ordinance shall be in full force and effect from and after its passage and approval according to law.

PASSED this 18th day of May, 2006.

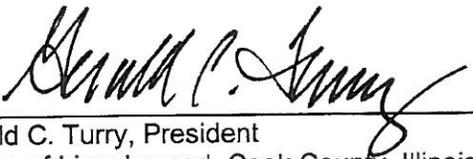
AYES: Trustees Froman, Heidtke, Lebovits, Elster, Sprogis-Marohn  
and Leftakes

NAYS: None

ABSENT: None

ABSTENTION: None

APPROVED by me this 18th day of May, 2006.



Gerald C. Turry, President  
Village of Lincolnwood, Cook County, Illinois

ATTESTED and FILED in my office the  
18th day of May, 2006.

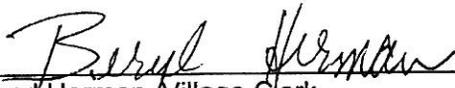
  
Beryl Herman, Village Clerk  
Village of Lincolnwood, Cook County, Illinois

EXHIBIT A

Copy of the Lincoln Avenue Corridor Report and Recommendations  
of the Lincoln Avenue Task Force

(See Attached)

# **Lincoln Avenue Corridor**



## **Report and Recommendations of the Lincoln Avenue Task Force**

**Village of Lincolnwood  
November 2005**

# **Lincoln Avenue Task Force Members**

## **James V. Persino, Chairman**

*Member and Chair, Lincolnwood Economic Development Commission*

## **Mary Couzin**

*Member, Lincolnwood Plan Commission and past Village Trustee*

## **Maureen Ehrenberg**

*Member, Lincolnwood Economic Development Commission*

## **Lawrence Elster**

*Village Trustee*

## **Kenneth Klint**

*Member, Lincolnwood Plan Commission*

## **Renee Silberman**

*Resident at-large*

## **Renee Sprogis-Marohn**

*Village Trustee*

## **Staff Assistance**

Timothy M. Clarke, AICP  
Community Development Director

## **Consultation Provided**

Walter J. Magdziarz, AICP  
Mahender Vasandani  
Land Vision, Inc.

# **Table of Contents**

<b>EXECUTIVE SUMMARY .....</b>	<b>1</b>
<b>INTRODUCTION .....</b>	<b>3</b>
<ul style="list-style-type: none"><li>• Background of Task Force</li><li>• History of Corridor</li><li>• Existing Development Pattern</li><li>• Challenges and Assets of the Corridor</li></ul>	
<b>VISION for the New Lincoln Avenue Corridor .....</b>	<b>6</b>
<b>RECOMMENDATIONS</b>	
<b>I. Public Streets and Spaces .....</b>	<b>7</b>
<ul style="list-style-type: none"><li>• Complete Median Landscaping</li><li>• Continue Parkway Restoration</li><li>• Improve Pedestrian Crosswalks</li><li>• Create a Designated Parking Lane</li><li>• Reduce Traffic Speed</li><li>• Reduce and Minimize Parkway Curb Cuts</li><li>• Create a Unified Corridor Streetscape Plan</li><li>• Consider Vacating Portions of Intersecting Streets</li><li>• Create Public Space Focal Points</li></ul>	
<b>II. Development Regulations and Design Standards.....</b>	<b>12</b>
<ul style="list-style-type: none"><li>• Modify Land Use Codes to Encourage New Uses</li><li>• Encourage Central Business Hubs</li><li>• Create Form Based Development Regulations</li><li>• Create Building Design Guidelines</li><li>• Regulate Off-Street Parking Locations</li><li>• Improve Business Signage</li></ul>	
<b>PLAN IMPLEMENTATION .....</b>	<b>17</b>
<b>APPENDIX: CORRIDOR URBAN DESIGN PLAN AND GUIDELINES</b>	

## **Executive Summary**

We present to you a bold and dynamic plan for Lincoln Avenue. When fully implemented, we believe this plan will result in an attractive corridor and desirable main street for our community. This report is intended to set out the necessary public actions and to guide private reinvestment and redevelopment in order to achieve the dynamic vision we have for the Corridor.

In formulating our recommendations, the Lincoln Avenue Task Force studied the redevelopment and revitalization efforts of a number of communities, including Park Ridge, Deerfield, Mount Prospect and Glenview. While our recommendations embody a bold and dynamic vision for the future of Lincoln Avenue, one which incorporates cutting-edge planning principles, these measures we recommend have been tested and proven successful elsewhere.

The central focus guiding our work has been to foster redevelopment of properties along the corridor which are both attractive and image enhancing, creating retail districts which provide residents with a safe, convenient, attractive and pedestrian friendly environment.

First, our plan calls for a dramatically different land use pattern for the corridor and we recommend significant land use changes to Village code. Specifically, we recommend creating three business districts along the Lincoln Avenue Corridor: one at Devon Avenue, another at Pratt and Crawford Avenues, and a third at Touhy Avenue. Pedestrian oriented retail is called for as the primary ground-floor use in these areas. We envision businesses such as bookstores, galleries, bistros, restaurants, coffee and ice cream shops, boutiques and other specialty retail stores enlivening these business nodes. Our plan also calls for mixed-use development in these business nodes, with upper floors of residential or office uses, to foster redevelopment and to encourage a lively, pedestrian-friendly atmosphere.

At Touhy Avenue, our plan envisions creation of a downtown center for our community, having ground floor retail, community gathering spaces and such anchor uses as a boutique hotel, sit-down restaurants, or entertainment venues. On upper floors, as well as between the three business nodes, our plan calls for introduction of residential development in the corridor.

Second, our plan calls for enhancements to the public right-of-way to improve public safety, create ambiance, and foster a strong sense of place. We recommend designating a parking lane on Lincoln Avenue; improving pedestrian crosswalks; and continuing efforts to restore parkways and to complete the median landscaping project. Most important, we call for a specific streetscape plan to be developed, which would include new streetlights and identifying street furniture and other urban design amenities and elements for the corridor. A bold but longer range component of our plan calls for creating new open spaces and focal points, such as towers or monuments in the corridor at key locations.

Third, we believe that the shape and form of new development in the corridor is critical. Therefore, our plan embraces neo-traditional planning and new urbanism concepts for new development along Lincoln Avenue. In this regard, we call for a set of new development regulations which will regulate the shape and the form of new buildings and private development in the corridor. These “form-based” recommendations for instance call for new buildings along Lincoln Avenue to be parallel to and positioned close to the street, with new off street parking located toward the rear. We recommend additional form based regulations in our report and to assist developers in meeting our vision, we also call for the creation of an illustrated architectural design handbook to guide new building development in the corridor.

Our vision for Lincoln Avenue is exciting and our work has set out many recommendations, both broad and specific. Execution will require a number of actions in order for our vision to become a reality. With Village government dedicated to the vision we present, we believe the Lincoln Avenue Corridor can truly become a main street for our community.

# **Report and Recommendations for a New Lincoln Avenue Corridor**

## **Introduction**

### **Background of Task Force**

The Lincoln Avenue Task Force is an outgrowth of discussions first held in 2002 by the Village's Economic Development Commission. These discussions quickly expanded to include the Village's Plan Commission and a joint meeting of these bodies was held in the fall of 2002 to discuss Lincoln Avenue and its future. The result was a recommendation that a task force be established to consider the challenges, assets and possibilities for the Lincoln Avenue Corridor. President Peter Moy, with consent of the Village Board, subsequently established the Lincoln Avenue Task Force to undertake this work.

The seven-member appointed Task Force is comprised of two members from each of the following bodies: the Village Board, the Plan Commission, and the Economic Development Commission, with one citizen-at-large member completing the roster. Primary staff assistance to the Task Force has been provided by the Community Development Director, with technical and field support provided by key members of the Building Department. The work of the Task Force has been further assisted with an engagement of Land Vision Inc., an urban planning and design firm.

In performing its charge, the Task Force examined many elements. These ranged from conducting a windshield survey of current conditions along Lincoln Avenue; studying the redevelopment efforts of other communities such as Glenview, Deerfield, Park Ridge and Mount Prospect; seeking community input through an Image Preference Survey and conducting and soliciting additional public input in a Town Meeting setting.

The work of the Task Force has resulted in a bold and dynamic plan which will transform the Lincoln Avenue Corridor into a valuable asset for the community.

### **History of Corridor**

Little is written about the origins of Lincoln Avenue. It is known that Lincoln Avenue began as one of the early roads linking Chicago northward to the rural farm areas of Illinois and beyond. Some of the very first businesses in the Village opened along this road, some reportedly to be out of the reach of city restrictions. As Chicago's population expanded outward and traffic increased, so did use of Lincoln Avenue and over time it became a major artery into and out of the City.

Although records are limited, most initial development along Lincoln Avenue in the Village appears to have occurred between the end of World War II and the mid 1960's. With the opening of the Edens Expressway in the 1950's however, Lincoln Avenue's role as a major thoroughfare declined. Just south of the Village, a string of 1950's-1960's era motels still can be found, testament to Lincoln Avenue's history as a major artery before the advent of the Edens.

## **Existing Development Pattern**

The existing development pattern along Lincoln Avenue can be characterized as an auto dependent form of commercial strip. Much of the initial development of this strip occurred at a time when the automobile was beginning to exert a dominant force on land use in America. Unlike areas of Chicago, Evanston, or rail towns like Park Ridge, Lincolnwood and Lincoln Avenue developed without street cars, rail stations, mass transit or the influence of great numbers of pedestrians. With the lack of any significant mass transit, specific business hubs, or a semblance of a central gathering location, the corridor became a long, nearly continuous stretch of commercial development, dependent mainly on access by automobile.

This development pattern occurred prior to the now common phenomenon of multi-car families. Traffic and parking demands in the corridor at the time of its initial development were much less than they are today. Developments during this period often had only a limited number of off street parking spaces, generally arranged along the front of the building and sometimes evolving into a continuous curb cut onto Lincoln Avenue.

Although the Lincoln Avenue Corridor is nearly a continuous strip of businesses from Devon Avenue to Jarvis Avenue, the character of the street changes significantly, albeit gradually, from south to north. At its south end, near the border with Chicago, land uses along Lincoln Avenue tend to be more varied, including auto repair shops, older strip retail centers with continuous curb cuts, and a storage facility. Developments along this stretch of Lincoln Avenue tend to be older, more functionally obsolete in nature and contain more marginal and less intensive uses than areas north. The middle section of Lincoln Avenue, roughly from the rail crossing near Hamlin Avenue to Touhy Avenue, contains a mix of uses, including more current and intense development. This area has multi-story office and bank buildings, automobile dealerships, restaurants, renovated strip centers and community institutional uses such as Village Hall, the Library and Proesel Park. North of Touhy Avenue, to the Village's boundary with Skokie, Lincoln Avenue is primarily comprised of office buildings.

The Task Force found that, except for the office corridor north of Touhy Avenue, the existing development pattern on Lincoln Avenue was aging, many uses were marginal, and the corridor was not functioning as a vibrant main street of the community.

## **Challenges and Assets of Corridor**

Lincoln Avenue presents a number of challenges and assets to its revitalization. First, Lincoln Avenue runs through the Village on a diagonal to the overall grid system of

streets. This results in a number of odd or irregularly shaped parcels at various corners which can inhibit typical development. Moreover, many of the parcels fronting along Lincoln have rather short depths, limiting to a degree their usefulness and redevelopment potential. This is compounded by the close proximity of single family homes to the corridor, some of which utilize shared alleyways with Lincoln Avenue properties and others which have no alley whatsoever.

Much of the existing development along the street was developed without adequate barriers or buffering measures with residential areas. This lack of buffering combined with short parcel depths and irregular parcel configurations can and often has produced land use conflicts. Recently however, the Village has strengthened its zoning code to require landscape buffering of business properties from residential areas. Where space is too constrained for landscape buffers, barrier walls are now required for new development. The implementation of these new standards for buffering should be continued and should help reduce land use conflicts within the corridor.

Lincoln Avenue itself is a relatively wide right-of-way corridor, averaging between 90 – 100 feet in width throughout its length in the Village. Existing conditions in the right-of-way, coupled with its physical width, currently inhibit safe pedestrian crossings and help aid in the dominance of the auto in the corridor. The conditions and width of this right-of-way, currently an impediment to a pedestrian friendly environment, can however be smartly designed to create a unique sense of place and a friendly pedestrian environment. With the creation of a designated parking lane for instance and a thoughtful streetscape design, this wide right-of-way can be utilized and tapped as an asset in the redevelopment of the corridor.

Another untapped asset of the corridor is the immediate market. Lincolnwood is an affluent community and with Lincoln Avenue running through its center, an attractive local market exists. Nonetheless, many marginal uses currently exist in the corridor. With changes to the Village's zoning code to encourage, for instance, restaurants and higher end retailers to locate in the corridor, this local market can be tapped to both serve the community and assist in the corridor's redevelopment.

A further asset which the Village can play upon is the recent and ongoing redevelopment and revitalization efforts occurring immediately south of the Village, along the corridor in the City of Chicago. Recently, a number of existing developments, including the Lincoln Village Shopping Center as well as the conversion of a Kmart store into a Home Depot, have resulted in significant revitalization. Moreover, the City of Chicago has recently acquired a few of the older motel properties, converting these to parks or other uses. And south of Peterson Avenue, new housing in the corridor has recently been constructed.

## **Vision for the New** **Lincoln Avenue Corridor**

We envision the Lincoln Avenue corridor as a vibrant Main Street through our Village. We see it not only as the centerpiece and spine of the community; it is also the entryway into the many residential neighborhoods of the Village. Major Village institutions, such as Village government, the public library, community center and local schools, are located along the street or very nearby. Lincoln Avenue is central to life in the Village.

We envision that as Lincoln Avenue enters the Village from Chicago, the character of the street changes dramatically, being readily apparent to the passerby that they have left behind the big city and have entered a high quality suburb. A landscaped median, new street lights, attractive signs, public sculpture and pedestrian friendly features including easy to use crosswalks and a marked parking lane, are evident and add to the sense that Lincoln Avenue is the Main Street of an upscale community.

Instead of a long continuous commercial strip as presently exists, hubs of activity have been created along Lincoln Avenue, primarily located at major intersections. These activity hubs include ground level retail in high quality, architecturally significant buildings with adequate customer parking, tasteful business signs and attractive landscaping. Comprised of restaurants, ice cream and coffee shops and boutique retail stores, these areas serve the immediate neighboring residential areas and the community as a whole and may include second or third story residential units.

Between these hubs of retail dotting Lincoln Avenue, new housing has been added to already existing bank buildings and office complexes, to replace tired strip centers, auto garages, storage facilities or other obsolete buildings and uses. We envision this in-fill housing as no higher than three stories, with appropriate green space and comprised of attractive townhouse, 3-flat, or small condominium buildings and developments. We see this new low density in-fill housing as providing on-the-one-hand, an attractive entryway into the adjoining single family residential areas, and on-the-other hand, a buffering and transitional use from the business areas and Lincoln Avenue itself to the nearby single family neighborhoods.

While Lincoln Avenue is still a major thoroughfare through the community, through much work, it has been transformed to be more pedestrian friendly and less auto dominated. Instead of a rapid auto path akin to an expressway and a barrier separating Lincolnwood's neighborhoods, the new Lincoln Avenue links neighborhoods together, bringing the Village together at its major institutions and becoming the main street of our community.

## **Recommendations**

The Task Force's recommendations below are divided into two major sections. The first section, Public Streets and Spaces, recommends actions and improvements in the public right-of-way which can help to create a pedestrian-friendly environment and foster a real sense of place. In this section, improved green space, wider sidewalks, new street lights, various urban design elements, as well as slower traffic speeds and eye-catching focal points are recommended to enhance the streetscape and invite residents and shoppers to the corridor. These are improvements which can be undertaken in the public right of way, by the Village in concert with the Illinois Department of Transportation (IDOT).

The second major section of our recommendations, Development Regulations and Guidelines, relates to controls on private property and new development in the corridor. The appearance, location and mix of businesses and residences along the corridor shape perceptions and use. To achieve our vision, this section recommends changes in the Village's existing land use regulations, creation of new form-based regulations and creation of design guidelines which will not only encourage private reinvestment, but will aid in shaping it. Together and in tandem, we believe these set of recommendations will help the Village create a new sense of place and achieve the vision we have for the Corridor.

### **I. Public Streets and Spaces**

Creating an attractive, vibrant and pedestrian-oriented environment in the public right-of-way is an essential element of the Lincoln Avenue Corridor Plan. This plan reinforces many of the Village's ongoing and recently begun initiatives, such as median landscaping and parkway restoration. It also recommends a number of new initiatives, such as creating a designated parking lane and the creation of a coordinated streetscape, including installation of new street lights and street furniture. Long term, the plan calls for creating three public plazas and/or visual focal points along the corridor.

While the plan emphasizes pedestrian orientation and creating pedestrian spaces, it recognizes that Lincoln Avenue is and will remain a major and important thoroughfare in the community. By undertaking attractive pedestrian enhancements, along with the other recommendations of this plan, Lincoln Avenue can become the main street for the Village. Since it is a state controlled street, coordination with the Illinois Department of Transportation (IDOT) is essential and will be required to undertake and implement these recommendations to the right-of-way.

The following are specific recommendations regarding public streets and spaces.

- **Complete Median Landscaping**

In the late 1990's, the Village initiated landscape improvements for the medians of Lincoln Avenue. This multi-year project is continuing. Median landscaping enhances the attractiveness of the corridor and creates positive perceptions of the corridor and community.

*Recommendation: Continue and complete median landscaping throughout the corridor.*

- **Continue Parkway Restoration**

In 2002 the Village created standards for parkways which include the placement of trees every 25 feet in the parkway and a pervious surface of sod or vegetative ground cover. Until these standards were instituted, parkways in the Village's business areas, including Lincoln Avenue, were essentially allowed to be treated by the adjoining owner as they so desired, providing no cohesion in the corridor and producing a visually cluttered, haphazard, disjointed and unkempt appearance. Moreover, in some sections, parkway areas have been essentially confiscated and turned into long continuous driveway curb cuts that directly access off-street private parking spaces, creating large stretches of impervious surfaces devoid of street trees. This situation conflicts with pedestrian use of public sidewalks and severely affects pedestrian safety. The restoration of parkways and public sidewalks, consistent with the recently established parkway standards, is an essential element toward improving the corridor. Parkway restoration should be diligently pursued, undertaken and implemented whenever possible.

*Recommendation: Diligently pursue parkway restoration whenever possible along Lincoln Avenue, including tree planting and sidewalk restoration.*

- **Improve Pedestrian Crosswalks**

Currently, many of the marked crosswalks across Lincoln Avenue do not provide the pedestrian with the shortest possible crossing distance for safety. Pedestrians are in the path of traffic for longer periods of time than necessary. To enhance the pedestrian environment and safety, conscious efforts should be undertaken to create highly visible marked crosswalks utilizing the shortest distance possible. The creation of intersection bump outs (described below) will further aid in reducing crosswalk distances, improving public safety and encouraging pedestrian use.

*Recommendation: Enhance crosswalk markings and designations, including signage.*

- **Create a Designated Parking Lane**

Currently, parking is generally permitted along the entire length of the corridor, although it is not well marked as such. Only in a few concentrated areas, such as in the vicinity of Air Room, is street parking now commonplace. Creation of designated parking lanes on both sides of Lincoln Avenue would help slow traffic and support active retail uses.

The width of the Lincoln Avenue street corridor currently allows for creation of this parking lane. Initially, such a parking lane could be achieved relatively inexpensively through painted striping and signage. A more long term, lasting and beneficial

improvement is to create curbed “bump outs” at intersections. Such bump-out areas essentially bring the sidewalk/pedestrian area of the public right-of-way out into the parking lane, approximately 8-9 feet toward the traffic lanes. Bump outs increase public safety and enhance the pedestrian environment. Specifically, bump outs at intersections: 1) protect and better define the beginning and end of the parking lane between intersections; and, 2) physically shorten the distance for pedestrians crossing Lincoln Avenue. In some areas, additional mid-block bump outs might be desirable to visually break up long parking lane stretches. Moreover, certain urban design features could be added to these new pedestrian spaces. Items such as decorative brick pavers, lighting, benches, bicycle racks, additional landscaping, newspaper vending machines, or even public art could be incorporated into these spaces.

***Recommendation: Create a designated, marked parking lane with pedestrian bump outs.***

### **• Reduce Traffic Speed**

High speed traffic is not conducive to either a pedestrian environment, or to residential or retail uses. Physical changes such as instituting a parking lane with bump outs, in and of itself, will likely reduce speed to some extent, both because driving lane widths will be perceived to be narrower and because of parking movement maneuvers slowing the speed of traffic.

Although no formal traffic study was commissioned as part of this work, traffic observations in the corridor suggest that much of existing Lincoln Avenue traffic, particularly during rush hour, is essentially “cut-through” traffic generated by Chicago residents seeking a quicker way to the Edens Expressway. This is suggested by two left turn lanes on Lincoln for westbound Touhy Avenue and two left turn lanes on Lincoln in Chicago for eastbound Peterson Avenue. Cut through traffic, particularly related to rush hour periods and home-to-work trips, typically consists of drivers intent on minimizing time. Slowing down Lincoln Avenue traffic in Lincolnwood would result in lengthened drive times for such drivers, deterring some traffic and making alternative routes more attractive.

By slowing down traffic, pedestrian safety is enhanced and greater visual observation by motorists is achieved. Businesses in the corridor will be more visible to the motorist and businesses will be better able to capitalize on impulse shopping and the customer base produced by the traffic volume. Therefore, slower traffic on Lincoln Avenue can help to contribute to the Village’s sales tax revenue.

The current posted traffic speed on Lincoln Avenue is 35 mph, although traffic appears to typically travel at speeds well in excess of the posted limit. Touhy Avenue west of Lincoln is posted at 30 mph, and a reduction in the posted speed limit on Lincoln Avenue to 30 mph should be seriously considered.

***Recommendation: Take action to reduce the speed of traffic in the corridor, including physical street design changes and a reduction in the posted speed limit.***

### • **Reduce and Minimize Parkway Curb Cuts**

Throughout the corridor, particularly as new developments are reviewed, driveway curb cuts in the Parkway should be kept at a minimum and apron width limited to only that needed for vehicle ingress and egress. Where possible and particularly for residential development called for in this plan, driveway curb cuts should be located not on Lincoln Avenue itself, but on adjoining side streets. This would further improve pedestrian safety as well as the aesthetic appeal of the streetscape.

*Recommendation: Reduce and minimize Parkway curb cuts along Lincoln and relocate as needed.*

### • **Create a Unified Corridor Streetscape Plan**

A specific streetscape plan should be created and implemented, specifying and delineating urban design elements that create an attractive, pedestrian scale and sense of place within the corridor. As envisioned, this streetscape plan would specify the style and identify the location of urban design and pedestrian friendly elements such as lighting, benches, bicycle racks, trash bins, and kiosks. This work could explore such other items as special signage, public art and sculptures, hanging flower containers and street banners. Use of brick pavers in sidewalks and crosswalks and specific designs for the recommended parking lane bump outs are other suggested elements that could be explored in development of this streetscape plan.

One critical component of the streetscape plan would be new streetlights for the entire corridor, to replace existing streetlights which are rapidly aging and appear to have exceeded their useful life. Our review suggests that new streetlights should not be installed in the medians as they are now, but along the sides of the street. However, this could be further investigated as the specific streetscape plan is developed. Creation of a specific streetscape plan would not only result in the identification of specific styles and designs for street furniture, sidewalks and light standards, but also would locate them in the corridor. The advantage to developing such a plan is twofold: it will help guide specific improvements in the public right-of-way at specific locations and it would allow the Village to seek Illinois Transportation Enhancement Program (ITEP) grants to undertake such improvements. We believe this is a critical component which should be budgeted within the next two fiscal years.

*Recommendation: Create and implement a unified streetscape plan for the corridor.*

### • **Consider Vacating Portions of Intersecting Streets**

Lincoln Avenue cuts diagonally through Lincolnwood, resulting in many unusual lot configurations along the corridor. The angle of Lincoln Avenue, set against the Village's basic grid pattern of streets, also has produced a number of intersections in the corridor which vary from the typical 90 degree street intersection angle, some involving more than one intersecting street. Because such intersections are not standard or typical, they pose public safety concerns, particularly to those not familiar with the area. This situation also results in long stretches of pavement and active traffic for the pedestrian to confront.

Our plan suggests the vacation of portions of some of these intersecting side streets to eliminate such unusual intersections. Such street vacations also have the potential benefits of reducing traffic into adjoining residential neighborhoods as well as enlarging and improving the shape of development parcels in the corridor. Any vacation of a specific street, however, must be thoroughly examined as to its potential effects. This plan has not undertaken such an investigation and therefore is only suggestive in this regard.

***Recommendation:*** *Side street vacations should be explored and examined at appropriate locations.*

### **• Create Public Space Focal Points**

A more long term but bold component of this plan, is to consider creating new public space focal points at three locations in the corridor: 1) Lincoln/Touhy/Kostner; 2) Pratt/Lincoln/Crawford; and 3) Lincoln Avenue at Proesel Park.

The first two locations are triangular shaped sites surrounded on all sides by public streets. Converting these sites into public spaces would result in open vistas at these important intersections, producing natural focal points. Along with redevelopment of adjoining properties, these new public spaces would help create a unique sense of place. Based largely on results of an Image Preference Survey of residents, our plan suggests that in these spaces, tower-like monuments be constructed of similar design, potentially utilizing prairie style elements, to help bring continuity to the corridor. This is however only an initial suggestion. As the community moves forward with implementing this element of the plan, other focal point ideas could emerge or be incorporated into such designs. These spaces for instance could commemorate significant local or national events or persons, celebrate the multicultural diversity of the community, or become spaces for the remembrance of the holocaust or of war dead.

A third recommended focal point along the Lincoln Avenue corridor is at Proesel Park. Currently the park, a major asset of the community extends a relatively long distance along Lincoln Avenue. Our observation is that the current design of the park provides neither a welcoming presence nor much activity along the Lincoln Avenue corridor. Our plan suggests highlighting this community asset by creating a focal point on Lincoln Avenue at Proesel Park, similar to those proposed on the triangle parcels. Specifically, this suggestion is to widen and expand the current Lincoln Avenue median at this location and install a similar tower-like icon, visually unifying the three sites. Creating such an expanded median area would require moving and arcing the Lincoln Avenue traffic lanes somewhat, producing an oval shape. This would be another feature which would slow Lincoln Avenue traffic. This proposal obviously would require IDOT review and approval and likely would require some land acquisition.

***Recommendation:*** *Consider creating three public focal points at key corridor locations.*

## **II. Development Regulations and Guidelines**

In addition to improving the corridor through the appearance and use of public streets and spaces, the Task Force is recommending major changes to the Village's land use codes; the creation of new "form-based" regulations for new development; and, creation of design guidelines for new buildings. Below are the Task Force recommendations as they relate to private properties and their development in the Corridor.

### **• Modify Land Use Codes to Encourage New Uses**

Land use in the corridor is a critical component to the plan. Except for that portion of Lincoln Avenue north of the Purple Hotel site, the plan recommends significant changes to the Village's current land use policies and zoning code. These changes, over time, will enhance and encourage a pedestrian environment for the corridor. Currently, the corridor is one long, mostly undistinguished strip of business properties. The plan proposes creation of three specific business activity hubs at key intersections, with the intersection of Lincoln and Touhy Avenues identified for higher density and activity, in an effort toward creating a downtown business district for the Village (described further below).

Currently, most of Lincoln Avenue is zoned B1 Restricted Business District. This zone permits office and bank facility developments. Retail is currently allowed only through special use designation. To achieve the vision for Lincoln Avenue, a significant change in land use, over time, is warranted. This plan calls for major changes in the zoning code regarding permitted and special uses. Under this plan, retail uses which cater to both Lincolnwood residents and the larger community should be encouraged to locate in designated business hubs. In such hubs, retail uses should either be exclusive uses on the ground level or predominant uses on the ground level to help achieve a pedestrian and retail oriented nexus. Book and drug stores, food specialty shops, coffee shops, sit-down restaurants, ice cream shops, dry cleaners, furniture, clothing and boutique stores should be encouraged and permitted to operate in such hub districts, subject only to complying with the community's development standards, buffering requirements with residential properties and the Village's off street parking requirements. Because office or bank uses generally do not contribute to cross-shopping, walk-in traffic, or a pedestrian environment, such uses in designated business hubs should be kept to a minimum or prohibited altogether on the ground level.

Similarly, specifically auto-oriented or auto-related businesses should be either prohibited altogether or kept to a minimum throughout the corridor. Automobile service and repair shops, oil change centers, gasoline stations, auto part stores and car washes do not contribute to either a pedestrian or shopping environment and should either be prohibited altogether, or highly limited. Where such uses may be authorized because of significant contributions to the tax base, such as automobile dealerships, careful attention to outdoor vehicle display and its effect on the streetscape is in order. Similarly, future drive-thru operations should be highly regulated, limited or prohibited altogether.

The plan presented for Lincoln Avenue embraces the concept of mixed use development, a term not currently found in the Village's Zoning Code. The plan envisions large development parcels, such as the Purple Hotel site, as including multiple land uses within its development. Individual buildings or smaller development parcels should also have

vertical multiple uses, namely retail on the ground floor and residential or office uses on upper floors.

In addition to permitting housing as part of certain mixed use developments, the plan also calls for the introduction of housing as an infill land use between the designated business hubs. Allowing housing as an in-fill land use will help demarcate the edges of the recommended business hubs and eliminate the current condition of a long, continuous strip of business properties. Moreover, development of in-fill housing between business hubs would visually extend the Village's residential neighborhoods into the corridor, thereby accentuating the residential persona of the community. In such in-fill areas, housing as a ground floor use should be permitted. Where housing is permitted, it is envisioned that residential uses in the corridor would be comprised of either multi-level townhouses or condominium buildings.

***Recommendation: Modify the current land use code to allow for mixed use development and in-fill housing; and, to prohibit expansion of drive-thru operations and auto-oriented uses.***

#### **• Designate Business District Hubs**

Related to the above changes being recommended, the Village should incorporate changes into its current zoning code to allow for development of the following proposed business hubs.

##### **Lincoln-Touhy Business Hub**

This business hub, centered on the intersection of Lincoln and Touhy Avenues, would include the Purple Hotel, Bank of Lincolnwood and Lincolnwood Produce sites, as well as nearby adjoining properties. This business hub is envisioned as the downtown for the community. Building height in this area is recommended to be limited to 5 floors, exceeding the 3 floor limitation recommended for the remainder of the corridor. Ground floor retail including restaurants, some with alfresco dining, would be a predominant ground floor use, with upper floors of residential and/or office space. Uses which encourage the public to gather, as well as strong or unique retail anchors, a boutique hotel or family entertainment venues, should be incorporated into specific parcel development plans. In addition, sculptures, fountains and plazas with outdoor seating should be designed into parcel redevelopments in this hub. It is envisioned that this business hub would have the highest development intensity in the corridor and potentially would require either underground parking, parking decks or a combination of both. Any parking structure should be well designed with public safety in mind and located so as to be unobtrusive to the primary use activities. For building sides located along Touhy Avenue, we suggest an exception to the recommended corridor build-to line of 5 feet. For such building sides, we recommend a build-to line of 15 feet or greater from the public right of way to account for high traffic volume on Touhy Avenue and pedestrian safety.

##### **Lincoln-Pratt-Crawford Business Hub**

This business hub is near the geographic center of the Village and a crossroads for Village residents. Functionally, this business area is expected to serve the more

immediate area and provide shopping convenience for residents. This area is recommended to include mixed use buildings having ground level retail, including such uses as book stores, bistros, drug stores, coffee shops, restaurants, dry cleaners and the like, as well as small outdoor gathering spaces. Two floors of residential units above the ground floor are also suggested for this area. Off street parking should be located toward the rear of properties.

### **Devon-Lincoln Business Hub**

This business hub is an entryway into the community and a major gateway from Chicago. Uses in this business hub are recommended to be primarily retail uses on the ground level, with off street parking located toward the rear. Any development of 2<sup>nd</sup> or 3<sup>rd</sup> floors should be for office uses. Good building design, site planning and quality architecture, important throughout the corridor, is paramount in this gateway area for establishing the ambiance of the corridor. New developments in this hub should include strong anchors to provide identity and a sense of place.

*Recommendation: Modify the Village Zoning Code to designate and regulate uses and encourage development in newly established business district hubs.*

### **• Create Form-Based Development Regulations**

The form and physical shape of private development is just as important as the type of uses which are permitted. Much of the existing development found in the corridor today is strip center in character, meaning that buildings are set back on the property and off street parking is provided directly in front of the building, toward the street. This form of development results in a scenery of endless parking lots, cars and freestanding signs for the passerby. Such an environment challenges any sense of place, and contributes to an auto dominated rather than pedestrian oriented environment. This plan recommends a very different approach and far different form for new development along the corridor.

Prior to modern conveniences, urban form generally took the shape of buildings built close to the public right-of-way, enclosing the street with buildings along it. Shop windows were large to let in light and also to display their products to the passerby.

Our plan embraces the essence of this earlier development pattern and seeks to update this earlier form for the 21<sup>st</sup> century. Our plan embraces the relatively new planning principles of Neo Traditional Development and New Urbanism, which seek to balance the needs of auto use with people and which assert that the form of development is as important as the permitted use of the property.

We recommend that, in addition to land use changes, applicable sections of the zoning code be created to contain form-based provisions applicable to new development along Lincoln Avenue. The following form-based provisions are recommended for the corridor (additional recommendations are contained in the addendum to this report).

**Build-to Line:** instead of a traditional setback line, where buildings are required to be setback from the front property line by a minimum distance, we recommend institution of a “Build-to line” that would require new buildings to be built-to. We suggest that this

build-to line should be no greater than 5 feet from the public right of way line. This recommendation would result in new buildings located very near the public sidewalk. For residential-only buildings this build-to line should be no greater than 10 feet from the right of way line, with a 4 foot wrought iron style fence, located 5 feet from the right of way.

**Building Orientation:** Because Lincoln Avenue travels through the Village at an angle, the orientation and facades of buildings in the corridor are critical and should be parallel to the street. Only in exceptional situations should the orientation of buildings and facades be at an angle to the Lincoln Avenue right-of-way line.

**Building Height:** Except for the Lincoln/Touhy Avenue Hub, the maximum height in the corridor should be 3 floors or 38 feet. In the Lincoln/Touhy downtown hub, a maximum height of 5 floors or 65 feet is recommended. Any building which is permitted to exceed 3 stories in height is recommended to have a building façade setback of 10 feet for all floors above 3 stories.

**Building Corner Towers:** Buildings located at corners, particularly at major intersections, should incorporate a turret or tower feature in harmony with the building's architecture. This tower feature component of the building may exceed the maximum height indicated, so as to give the building a distinctive form and focal point.

**Façade Windows:** Building facades fronting onto Lincoln Avenue, or onto major adjoining public streets or which otherwise are highly visible from the public right-of-way, should incorporate windows in their facades, particularly at the street level. Walls facing the public right of way which lack windows should be prohibited.

**Recommendation:** *Create form based development regulations as outlined here and elsewhere in this report.*

#### • **Create Building Design Guidelines**

Our report and the work of our consultant have suggested certain building design guidelines for new buildings in the corridor. The Task Force believes further work and additional design details are appropriate to help guide developers and architects and to illustrate design elements acceptable to the Village. It is envisioned that this work would result in a separate user friendly, heavily illustrated architectural design guideline handbook. To undertake this work, the Task Force recommends the formation of an Ad Hoc Committee of Village design professionals to help prepare this document.

**Recommendation:** *Create an Ad Hoc Village Design Committee to develop a Building Design Guideline Handbook.*

#### • **Regulate Off Street Parking Locations**

For new developments, off street parking should be prohibited from the front of new buildings along Lincoln Avenue. Such parking should be located toward the rear of the property. Related to this, the Village's recently adopted requirements ensuring buffering and screening of business properties with adjoining residential properties should be continued and required in all new developments.

The location of off street parking areas and the recommendation to prohibit them from the front of new buildings along Lincoln Avenue is a critical component of this plan and this new standard should only be varied in the most unusual circumstances. For some large scale developments, parking decks may be in order and these should be placed either behind the buildings they serve or with a level of activity, such as retail, located on the ground floor of the parking structure.

This plan does not recommend relaxation of the Village's off street parking standards at this time. These standards should be reviewed to determine their current adequacy for various uses and to investigate the feasibility of shared parking arrangements among uses.

Where new uses will occupy existing buildings, existing front yard parking areas should be screened from view with landscaping or a short screen wall along the public sidewalk. Recently, the Village adopted new code standards requiring such screening treatments for off street parking lots. Strict adherence to these standards is strongly recommended with variations granted only in the most unusual cases.

For the public right-of-way, this plan recommends the creation of a designated parking lane. Although the Task Force does not recommend that off street parking requirements be specifically relaxed, variations for certain desired proposed uses should be considered depending upon the degree of available nearby on-street parking. In the reuse of existing buildings, since there is a strong interplay between providing parking lot screening and providing off street parking spaces, where on-street parking exists and is generally available in the designated Lincoln Avenue parking lane, required parking lot screening should take precedence over the required number of off street parking stalls.

***Recommendation: Prohibit new off-street parking lots in front of buildings along Lincoln Avenue. Continue to require landscape screening of existing parking areas.***

#### **• Improve Business Signage**

The Village updated its Sign Code in the late 1990's to incorporate new community sign standards. This code encourages (but does not require) the use of monument signs instead of freestanding pole signs. With the greater emphasis on creating a pedestrian scale along Lincoln Avenue, consideration should be given to lowering the maximum height of monument signs, possibly to 8 feet, and either creating stronger incentives to use monument signs or reducing the maximum height of freestanding signs. Another option would be to allow only monument signs and not freestanding signs along Lincoln Avenue.

***Recommendation: Explore stricter requirements for signs that would improve the pedestrian scale of the corridor.***

## **Plan Implementation**

To implement this plan, Village government must be focused at all levels and in all departments on achieving the enunciated goals and desired results of this plan. Each department and recommending body has a role in moving the Village forward toward achieving this plan. Below are the key elements of this plan and recommended implementation actions.

### **Streetscape Plan Development:**

We recommend that funds be budgeted in the Village's Capital Improvement Plan for preparation of a streetscape plan for Lincoln Avenue. High priority consideration should be given for plan preparation in the next fiscal year, since this plan will help the Village seek grant funds for necessary streetscape improvements and provide reference for improvements which should take place as part of private development. In the actual development of this plan, where appropriate, input from various recommending bodies, should be sought.

### **Land Use and Form-Based Code Changes:**

The land use changes and form based provisions recommended for Lincoln Avenue require changes to the Village's zoning code. Various options are available to achieve this result including overlay districts or separate zoning districts. We recommend that the consultant engaged to update the Village's Zoning Code also be charged with incorporating into this update, the recommendations of this report. Since this work may require further detailing and a variety of determinations, in concert with the Plan Commission, Task Force members have agreed to provide assistance in this work.

### **Village Departmental Actions**

To achieve this plan, implementation will require the ongoing and concerted work of the entire Village organization. Coordinated through the Village Administrator's Office, departments must continue their current work such as median landscaping. These departments also must incorporate new activities called for in this report, such as improving crosswalks, into routine or scheduled work or maintenance plans. More significant improvements, such as those identified in the recommended streetscape plan, will require a strong working relationship and coordination between Village staff and IDOT for implementation.

### **Village Recommending Bodies**

Citizen led recommending bodies to the Village Board have a very large role toward achieving and implementing this plan. Below are key Village Board recommending bodies having certain oversight, duties or expertise which can be utilized and employed to achieve the plan and vision for Lincoln Avenue.

### **Traffic Commission**

Consider and provide input on the creation of the designated parking lane; the posted speed limit and other measures to reduce travel speeds in the corridor. It is possible that parking management in the corridor will become desirous in the future and parking restrictions and regulations for the parking lane may become appropriate. Further, the

Traffic Commission should study and provide input and recommendations regarding appropriate side street vacations, after first reviewing this report and clearly understanding its goals.

**Plan Commission/Zoning Board of Appeals**

Consider and include in the updated revised zoning code, the recommended use changes, the designation of new business district hubs and the addition of new form-based development regulations. Study and recommend changes to off-street parking requirements in this update. In the role as Zoning Board of Appeals, consider case-by-case variations to these new standards, only where unusual circumstances warrant.

**Sculpture and Beautification Committees**

Provide input and suggestions in the development of the recommended streetscape plan as well as suggestions on the selection of the specific styles of street furniture, street lights and monuments for the public space focal points.

**Sign Appearance Review Board**

Consider and review the existing Sign Code and consider recommending changes to the sign code that would enhance the pedestrian scale and environment of the corridor called for in this report.

**Economic Development Commission**

Study and provide recommendations which could assist in funding of corridor improvements including the long range component of new public open space, as well as measures which could further encourage new private reinvestment and corporate donations in the corridor.

**Ad Hoc Building Design Guideline Committee**

Study and create an illustrated architectural design guideline booklet which would communicate the building design elements and expectations of the community.

## **APPENDIX**

# **LINCOLN AVENUE CORRIDOR URBAN DESIGN PLAN AND GUIDELINES**

Lincoln Avenue Corridor Study  
Lincolnwood, Illinois

CORRIDOR URBAN DESIGN PLAN  
& URBAN DESIGN GUIDELINES



Prepared by Land Vision, Inc.  
for  
Lincoln Avenue Task Force  
Final Report  
November 16, 2005

Lincoln Avenue Corridor Study  
**Corridor Urban Design Plan  
& Urban Design Guidelines**

**Table of Contents**

I. Introduction	3
II. Corridor Redevelopment Strategy	3
III. Lincoln Avenue Corridor Urban Design Plan	7
IV. Urban Design Guidelines	7
1. Guidelines for Public Domain	10
A. General Guidelines	
B. Section Specific Guidelines	
2. Guidelines for Private Domain	15
A. Guidelines for All Buildings	
B. Additional Guidelines for Residential Buildings	
C. Additional Guidelines for Commercial/Mixed-use Buildings	

**List of Figures**

Figure A: Recommended Corridor Redevelopment Strategy	5
Figure B1: Corridor Plan North	8
Figure B2: Corridor Plan South	9
Figures 1A & 1B Parkway materials and wheel-stops	12
Figures 2A & 2B: Limited parkway landscaping and wheel-stops	13
Figures 3A & 3B: Alternate to short walls	13
Figures 4A & 4B: No parkway landscaping and no parking lot	14
Figures 5A & 5B: Ideal conditions for landscaping on private property	14
Figure 6: Illustrative guidelines for Redevelopment of private properties	15
Figure 7: Plan view showing corridor design guidelines	16
Figure 8: Illustrative guidelines for buildings higher than three floors	17

**List of Images**

Image 1: Example of building parallel to street	18
Image 2: Examples of buildings angles to street	19
Image 3: Example of Corner Towers in buildings	19
Image 4: Example of attractive street landscaping	20
Image 5: Example of buildings with blank walls along the street side	20
Image 6: Example of acceptable fence along street	21
Image 7: Example of a residential building with well-articulated entries	21
Image 8: Example of residential units fronting on rear driveways	22
Image 9: Example of successful pocket park	22
Image 10: Example of additional building setback	23

# **Lincoln Avenue Corridor Study Corridor Urban Design Plan & Urban Design Guidelines**

Lincolnwood, Illinois

## **I. INTRODUCTION**

The Village of Lincolnwood (the Village) and its Lincoln Avenue task Force (LATF) identified the Lincoln Avenue corridor as an opportunity for redevelopment. As the main street through the community, this strip could be transformed into a vibrant community centerpiece with a real sense of place. Land Vision, Incorporated (LVI) was retained in August 2004 to conduct the Lincoln Avenue Corridor Study that would assist the LATF in preparing an urban design plan and urban design guidelines to transform the corridor. Specifically, the LATF expected the Lincoln Avenue Corridor Study to focus on the following:

1. Identify activity centers and infill development opportunities in the corridor
2. Identify means of creating a unified image of the corridor
3. Apply principles of New Urbanism to identify appropriate building setbacks, heights and densities for new development
4. Identify areas for attractive gathering places
5. Recommend land uses and urban design guidelines to help achieve the corridor vision that can become the basis for updating the existing Village Comprehensive Plan and Zoning Ordinance
6. Provide development recommendations and guidelines for the hotel site at Lincoln and Touhy Avenues

To help accomplish these objectives, LVI invited input from Village citizens through an Image Preference Survey (IPS). Ninety-five citizens provided input, and the result of the survey showed strong preference for:

1. More traditional looking buildings than buildings with modern architecture
2. Two-to-four story mixed-use buildings with well-articulated architecture
3. Buildings with inviting, pedestrian-friendly, well-landscaped and safe sidewalks in front
4. Private developments with outdoor gathering places, such as sidewalk cafes, and pedestrian enclaves
5. Buildings located closer to residential areas with more complementary architecture
6. Unobtrusive signs, and
7. Parking areas that are well landscaped.

With the goals and objectives set by the Village and the design preferences identified by the Village citizens, LVI first prepared a Corridor Redevelopment Strategy, and overall Corridor Plan, and finally, a detailed set of Urban Design Guidelines that would govern the corridor redevelopment and enhance appearance of the existing properties.

LVI found that both the Village goals and residents' preferences coincided with the tenets of New Urbanism. New Urbanism is a modern planning movement that seeks to re-create a sense of place and foster walkable, human-scaled communities. It advocates for central downtowns, quality public spaces, diverse streetscapes, and location of stores, services, parks and schools within easy walking distance of homes.

LVI recommends that new buildings – regardless of their use or type – be placed at the front of the property, close to the street, with all parking located behind buildings. This is

principal tenet of New Urbanism; disallowing parking in front of buildings improves the street for pedestrians and enhances streetscape appearances.

Urban design guidelines will further direct the “form” of buildings: how they look, how they relate to the street and to one another, and how they, along with the streetscape, form a cohesive and well-designed urban environment. Such urban design guidelines, called for by New Urbanism, can be outlined in Village land use and zoning codes in what are called form-based regulations.

LVI strongly recommends that, as a follow-up to the adoption of the Corridor Plan and Urban Design Guidelines, the Village adopt a form-based code for the corridor. This will ensure that the vision for the Lincoln Avenue corridor is implemented consistently into the future.

## **II. CORRIDOR REDEVELOPMENT STRATEGY**

As many other strip commercial corridors in the Chicago region, Lincoln Avenue has evolved into a corridor consisting primarily of retail and commercial uses. The current Village regulations permit banks to locate in the corridor more easily than retail uses such that the corridor now has a preponderance of banks. Most of the properties fronting onto Lincoln Avenue are shallow, and many of the existing retail uses and businesses in the corridor are at best nominal. In conjunction with a lack of proper maintenance on some of the corridor properties and a lack of adequate parking on other, the LATF determined that the existing form of development in the corridor is outmoded and fails to exploit the corridor’s full potential.

Given these concerns, LATF identified specific areas of the corridor that could be considered as future hubs of activity with new uses replacing the old and consisting of either mixed-uses or single uses. As opposed to a continuous set of uses spread uniformly along the corridor, LATF determined that creating one main hub and other smaller hubs in the corridor would encourage the development of a potential downtown in Lincolnwood that could be used for creating key gathering places to attract the Village residents and give the village a new identity. LATF also indicated that the site with the “Purple” hotel at the northwest corner of Lincoln Avenue and Touhy Avenue should be given consideration for creating the “downtown” hub in the Village.

In response, LVI recommended a Corridor Redevelopment Strategy that built on the key ideas of the LATF. As shown in Figure A, some of the major recommendations LVI made include:

1. To create a downtown hub in the Village, consider redevelopment of the “Purple” hotel site, the Bank of Lincolnwood site, and the site for Lincolnwood Produce. While some of these recommendations – especially the Bank of Lincolnwood site -- may be implemented in the long-run (over a period of next ten years and beyond), LVI recommended that in order to create a critical mass for the new developments to function as a downtown, the Village would have to allow the redevelopment of more than just the “Purple” hotel site. The range of mixed uses for these sites will include residential, retail and office. The “Purple” hotel site, in addition, may also include a boutique hotel, should the market conditions favor such use to be feasible.



2. LVI also recommended redeveloping the existing Tuffy site with a plaza and a focal-point monument in order to enhance the visual appearance of the major point of entry in to the Village from north. It would also serve as a key marker for the new downtown Lincolnwood
3. Directly in front of Proesel Park, along the park's frontage with Lincoln Avenue, LVI recommended installing another focal-point monument in the middle of the road with new pavements channeling around the monument in an oval form. From a transportation standpoint, this was recommended to calm the traffic along Lincoln Avenue, and as an urban design strategy this would create another key identifier that in conjunction with the monument at the Tuffy site would be the start of forming a cohesive image of the corridor. (See later discussion on Urban Design Guidelines for how these monuments should be treated architecturally).
4. At the site of existing Mobil gas station, located at the triangular site bounded by Lincoln Avenue, Crawford Avenue and Pratt Avenue, LVI recommended the installation of another focal monument, as part of a new mixed-use hub at this location. Surrounded by three major roads, the existing gas station creates adverse traffic conditions in the corridor that would be best ameliorated by its replacement with a public plaza (with the monument). From an urban design standpoint, since the Lincolnwood Public Library is located on Pratt Avenue, the triangular plaza in its front will provide an appropriate foreground for the civic building.
5. For the properties around the new triangular plaza along Lincoln Avenue, LVI recommended retail and residential uses, such that along with the existing public library, this area will become another mixed-use hub in the corridor. While the existing Grossinger Toyota (soon to become Grossinger Volvo) will remain an auto-oriented use at this hub, the rest of the uses will emphasize pedestrian access along with specific outdoor gathering places as part of the redevelopment projects.
6. For the next hub at the intersection of Devon Street and Lincoln Avenue, with the "Bunny Hutch" site providing a key redevelopment opportunity, LATF recommended that the new uses should primarily be commercial. This hub will include the site east of Lincoln Avenue that will be redeveloped into a new commercial use. This commercial hub at the southern end of the corridor will need to design consistency between individual buildings to create a strong sense of entry into the corridor.
7. For the intermediate redevelopment opportunities in the corridor, LVI recommended the primary use to be medium density residential. The key sites proposed for such residential redevelopment include the industrial area southwest of Lincoln Avenue and the railroad and the area east of Lincoln Avenue.
8. In conjunction with many of the above redevelopment projects, LVI recommended closing a number of street segments as they intersect with Lincoln Avenue. These are highlighted on the Corridor Plan for Lincoln Avenue that LVI developed after discussing with the LATF the specific types of medium-density residential and mixed-used projects that would be most appropriate for the corridor.

### **III. LINCOLN AVENUE CORRIDOR URBAN DESIGN PLAN**

As the Corridor Redevelopment Strategy was finalized in discussion with the LATF, LVI prepared a detailed Corridor Urban Design Plan (Corridor Plan) to show how the individual properties or groups of properties may be redeveloped to consistent with the overall strategy. This Corridor Plan is depicted in Figure B (with larger versions of this plan available in poster form).

As Figure B shows, LVI has shown detailed building footprints and parking areas or parking garages that would be necessary to serve a redevelopment project. Many of the recommended projects will require consolidation of individual parcels and street closings. It will be critical for the Village to allow redevelopment of projects with all required parcels consolidated and not permit their individual, piecemeal development. Another major thrust of the recommendations is to “line” the front of the properties with buildings, .i.e. place all new buildings at a set maximum distance from the property line (see Urban Design Guidelines for more discussion). The Village should require all redevelopment projects to have such “liner” buildings with parking for the projects concealed behind the buildings.

While LVI prepared the individual site plans for redevelopment to demonstrate that the recommended projects will be feasible following the principles of New Urbanism, LVI and LATF expressly state that none of the redevelopment projects have to follow the footprints of buildings exactly as drawn by LVI. The Corridor Plan is CONCEPTUAL in nature and it is the intent of LATF to allow developers and builders who will undertake redevelopment in the corridor, a full measure of flexibility provided their projects follow the urban design guidelines. As and when the Village adopts a new form-based code that is based on the urban design guidelines recommended by LVI (see below), the project redevelopers will need to conform to the new ordinance regulations of the Village.

#### **IV. URBAN DESIGN GUIDELINES**

As stated by the LATF, the future Lincoln Avenue Corridor must have an overall image that is not haphazard but more uniform and cohesive. A mile-long corridor with an array of uses developed over many years has an image today that is disparate, outmoded (in visual terms as well, in addition to economic) and dysfunctional. The challenge of achieving the goal of cohesive redevelopment out of such conditions can be met by adopting the urban design guidelines recommended by LVI.

In order to meet the full scope of a new urban design for the corridor that also includes the citizens’ image preferences, LVI recommended urban design guidelines that cover not only the domain of the public street, but also the domain of private properties and how the buildings in the corridor relate to the street and to one another.

The public domain guidelines apply to properties that will not be redeveloped for the foreseeable future. These guidelines include the “interface” between the public and private domains, covering the area of private property directly adjacent to the right-of-way. The private domain guidelines apply to a property or a group of properties that is set for redevelopment. These guidelines will cover the portions of redevelopment project that include sidewalks, street furniture, planters and landscaping.

For the corridor as a whole, LVI also recommended that there be on-street parking – whether associated with existing uses or redevelopment projects. While the corridor already has on-street parking, this parking will need to be integrated into a new sidewalk system with “bump-outs” from the existing sidewalk line.

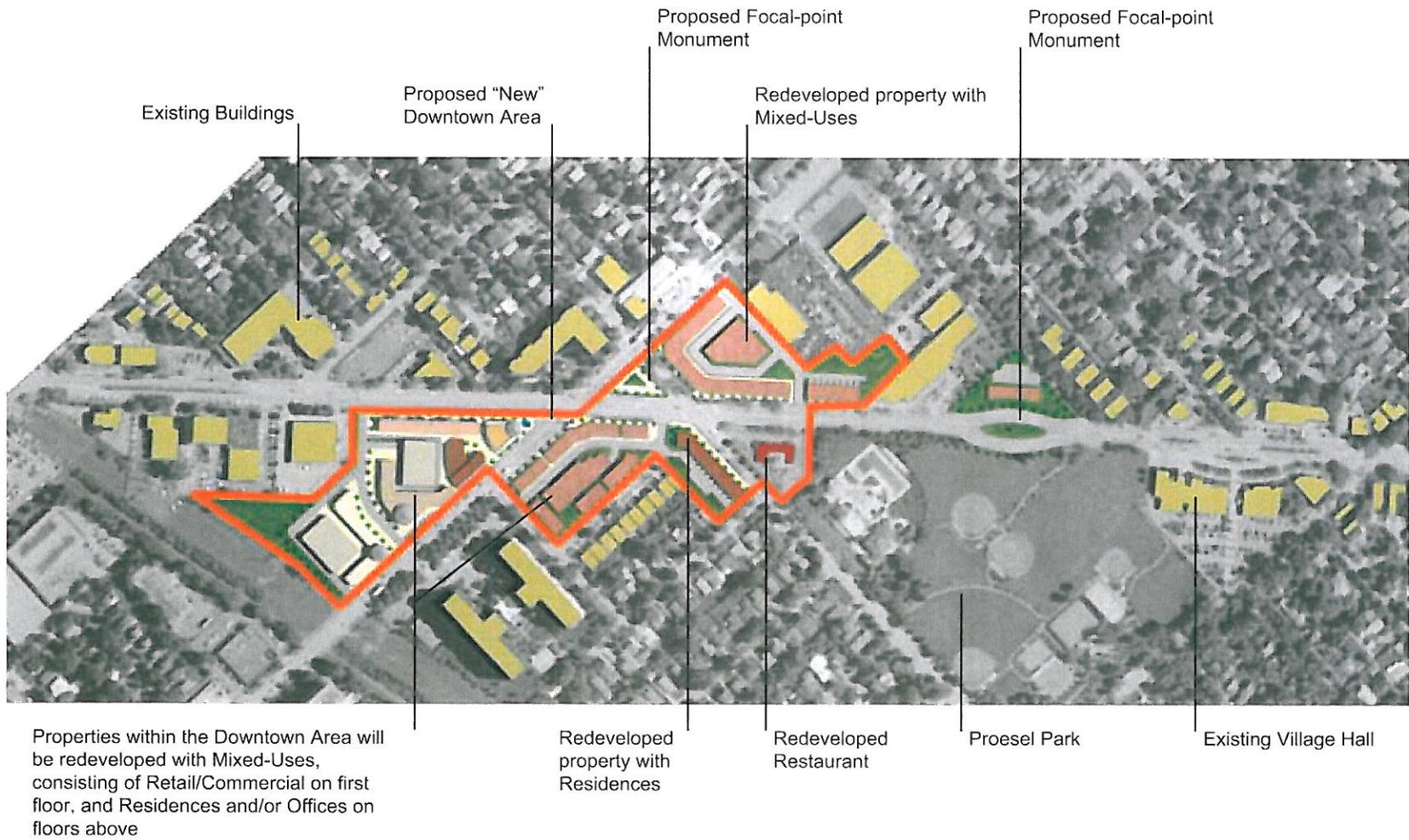


Figure B1: Illustrative Corridor Plan: North  
 (Note: All site plans in this exhibit are shown for illustration only. Developers seeking to redevelop properties will follow the urban design guidelines )



Figure B2: Illustrative Corridor Plan: South  
(Note: All site plans in this exhibit are shown for illustration only. Developers seeking to redevelop properties will follow the urban design guidelines)

The Village will need to prioritize the implementation of the urban design guidelines for the public domain based on the severity of existing conditions and availability of funding. If redevelopment is anticipated, then the urban design guidelines for the private domain (or the form-based code regulations) will be implemented at the time when new project is constructed. Regardless of the timing of when public or private improvements are implemented, in the long run, following the urban design guidelines (or form-based code regulations) will help the corridor achieve the desired cohesive image.

## 1. GUIDELINES FOR PUBLIC DOMAIN

*Covering the area within the R.O.W., including these streetscape elements: Streets; Medians; Street curbs; Parkways; Sidewalks; Street signs; Private signs; Street lights; Median lights and landscaping*

Taking into account the above streetscape elements, LVI identified several existing conditions that are typical of different sections of the corridor. For these corridor sections, LVI recommends a number of design guidelines that are illustrated in plan views and cross-sections below. LVI also recommends certain general guidelines that can be applied to any section in the corridor. The Village staff will need to determine the best fit between the existing conditions and the recommended guidelines to select the most appropriate streetscape improvements necessary for implementation in a given section. The recommended guidelines are not meant to supersede the existing, more restrictive zoning regulations that meet the design intent of the guidelines. What follows first is a description of the general urban design guidelines and then the more section-specific guidelines. The guidelines are described in no specific order of preference.

### A. General Guidelines

1. **Parkway Trees:** All parkways within the Lincoln Avenue corridor with available space for landscaping should have trees planted at a minimum center-to-center distance of 25 feet (or, as per the existing Village ordinance)
2. **Median Landscaping:** Within the medians along the Lincoln Avenue Corridor, there should be adequate landscaping consisting of shrubs and other materials that are well-maintained
3. **Landscape Materials:** Selection of landscape materials to be used in the parkways, medians or sidewalks should be governed by their hardiness to road salt and with the goal of achieving consistency between one median landscaping to the next
4. **Sidewalk Materials:** Sidewalks in the corridor should have two different materials to better delineate the area used for pedestrians – especially when new sidewalks are built in conjunction with the redevelopment of properties. There should be consistency in the use of sidewalk materials from one property to another
5. **Pedestrian Buffers:** All pedestrian portions of sidewalks should be buffered from the street by landscape planters and/or row of trees
6. **Street Lights:** Selection of new light standards for the corridor should be based on how well they relate to the pedestrians without overwhelming them by their size, height or level of illumination. Examples from other communities such as Elmhurst where IDOT agreed to install special light fixtures in conjunction with a major roadway widening project, suggest that it should be possible for the Village to work with IDOT to have special light fixtures installed in the corridor. The light fixtures with special pedestrian oriented lighting should be located on both sides of the street and not in the median.

7. **Street Signs:** Street signs in the corridor should adopt a distinct new design that makes them stand out as compared to the regular street signs in the rest of the Village to create a special image of the corridor
8. **Street Furniture:** Street furniture, such as benches, information kiosks, telephone kiosks, waste receptacles and bicycle stands, should be provided along the municipal park where it meets the Lincoln Avenue corridor, mixed-use hubs and other areas of the corridor as later deemed appropriate. The design of street furniture in one area should match the street furniture design in another area to further reinforce the visual cohesiveness of the corridor
9. **Business Signs:** For free-standing signs advertising businesses located on the same property, their location, size, height and design should have a level of consistency that lends cohesiveness to the appearance of the corridor
10. **Focal-point Monuments:** The Corridor Plan recommends three focal-point monuments in the corridor: the first at the current Tuffy site (at Touhy and Lincoln Avenues); the second within the proposed bi-furcated roadway pavements of a “traffic oval” in front of Proesel Park, and the third at the current Mobil gas station site (at Crawford and Lincoln Avenue). The design of these monuments should be consistent in order to create a unified set of visual landmarks in the corridor. Of the three monuments the most significant in height and architectural treatment should be the tower in front of the Proesel Park. Because of its close proximity to the Village Hall that’s built in Prairie Style architecture, the Village may consider requiring the design of this monument to be in Prairie style as well. In addition to visually tie these major identifiers together, the other two monuments may also be constructed in Prairie style architecture.

## **B. Section-specific Guidelines**

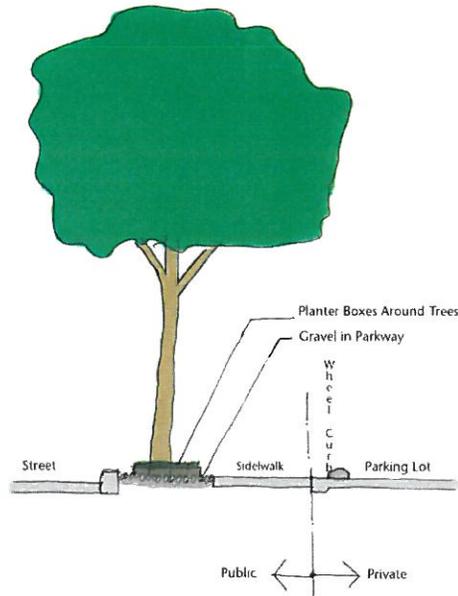
The use of these guidelines will vary from one section of the corridor to another. They are presented as illustrations that show the existing conditions needing improvement and the recommended guidelines to achieve better urban design. The guidelines also cover the interface where the private property abuts the right of way. This interface affects the visual perception of the corridor as much as the quality of the streetscape elements within the right-of-way. Until the time the buildings on a property remain the same – even if the uses in the buildings change – focusing on this interface between the public and private domains will be important. When properties redevelop, new buildings in the corridor will be located close to the street along with new sidewalks in front of the new buildings. This will create a new set of conditions that will be governed by the guidelines for Private Domain. Until change occurs on the private properties, the following design guidelines show how to treat the public domain and the edge of the private properties.

### **Section-specific Guidelines for Public Domain**

**NOTE:** *These guidelines apply only to properties with existing buildings. For new or redevelopment projects, see Urban Design Guidelines for Private Domain*

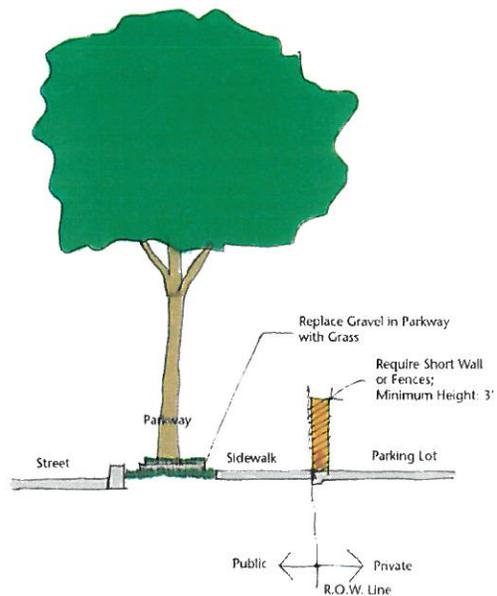
- Existing parkway has materials other than grass, and only wheel-stops exist between the sidewalk and adjoining parking lot

Figure 1: EXISTING CONDITIONS



- Appearance of gravel in the parkway is visually unappealing and inconsistent with the rest of the properties in the corridor with grass in parkway
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists. This makes the corridor more unattractive

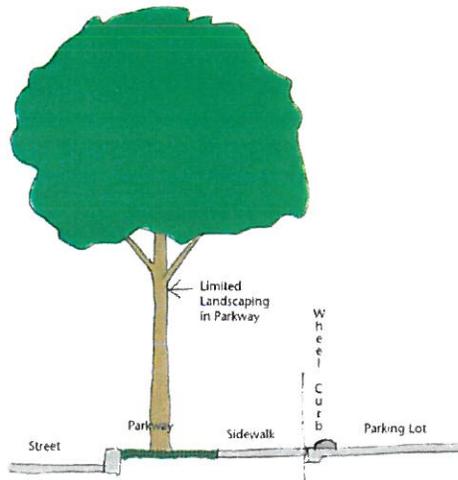
Figure 1A: RECOMMENDED GUIDELINES



- Replace gravel with grass in the parkway
- Require a short wall of no less than 3' to be installed at the property line to conceal the parked cars. Materials and color of wall should primarily match the materials and color of other walls along the corridor

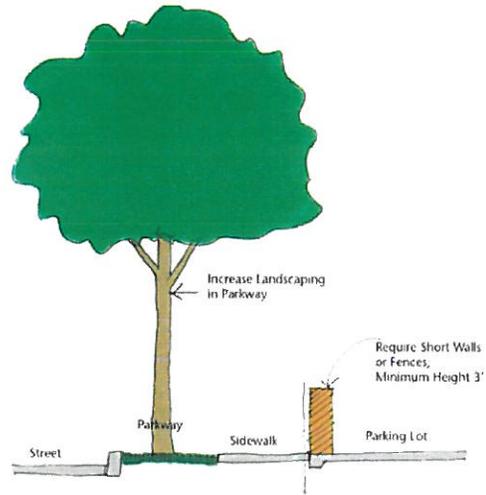
- Existing parkway has limited number of trees and like the first condition has no significant barrier between the sidewalk and the adjoining parking lot

Figure 2: EXISTING CONDITIONS



- Limited number of trees in the parkway fails to define the corridor edge effectively
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists makes the corridor more unattractive

Figure 2A: RECOMMENDED GUIDELINES



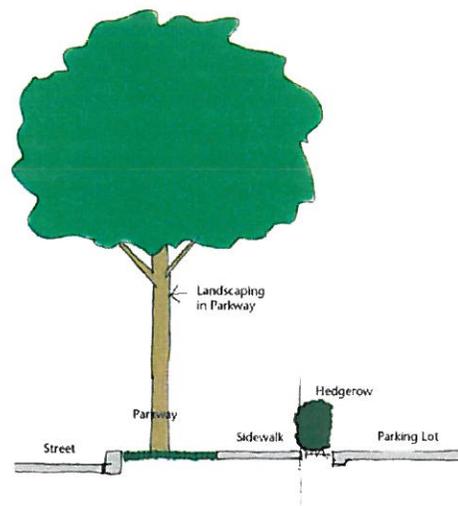
- Increase the number of trees in the parkway to be no more than 25' apart
- As in Recommendation 1, require short walls at the edge of the private property

Alternate to Short Walls (as shown in Recommendations '1' and '2')

Figure 3: EXISTING CONDITIONS

- Limited number of trees in the parkway fails to define the corridor edge effectively
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists. This makes the corridor more unattractive

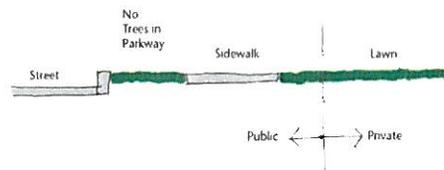
Figure 3A: RECOMMENDED GUIDELINES



- Increase the number of trees in the parkway to be no more than 25' apart
- Instead of short walls, require hedgerows at the edge of the private property in a way. This will not affect the overall parking supplied at the property. Hedgerows should be a minimum 3' tall

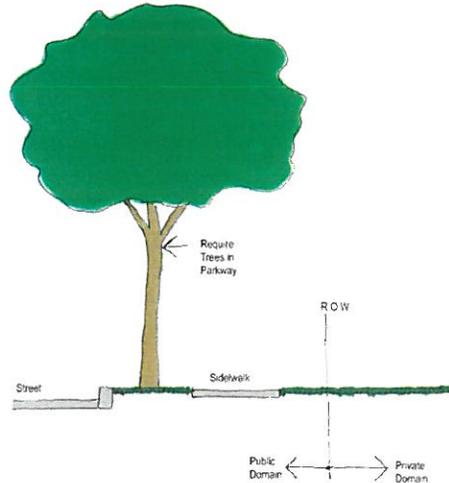
4. Existing parkway has no (or few) trees and the adjoining property has no parking lot

Figure 4: EXISTING CONDITIONS



1. Without the trees, even when the adjoining property has lawn in its front yard and no parking lot, the street lacks a defining edge.

Figure 4A: RECOMMENDED GUIDELINES



1. Require trees to be planted within the R.O.W. where there are no trees, OR when there are few trees,
2. Require trees in the parkway no less than 25' apart

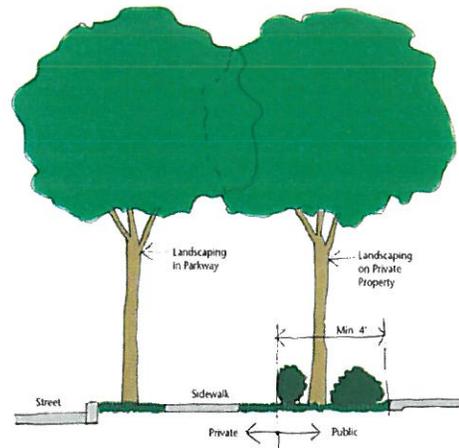
5. Ideal Condition for Landscaping on Private Property

Figure 5: Existing Conditions

When any of the following conditions exist on a property, i.e. if:

1. There are materials within the parkway that are not grass
2. There are limited number of trees in the parkway
3. There are no trees in the parkway at all, and when there are no physical elements separating the adjoining parking lots from the sidewalk

Figure 5A: Recommended Guidelines



1. Require parkway trees
2. On private property, require a landscape buffer no less than 4 feet wide for trees and shrubs. The double row of trees – one in the parkway, the other on private property – should be staggered.

(The Village of Lincolnwood already has examples of this type of streetscape along Devon Avenue near the intersection with Crawford Avenue).

## 2. GUIDELINES FOR PRIVATE DOMAIN

*Covering the private property, including: Placement of buildings; Height of Buildings; Relation of building to street/sidewalk; Relation of building to buildings along one side of the street; Relation of buildings to buildings across the street; Treatment of building entryways; Relation of entryways of one building to the next; Treatment of building bulk; Treatment of special gathering places outside the building*

The design guidelines for private domain are described in general terms and not on section-specific or site-specific basis. They are differentiated by categories of use and are based on the design preferences of the Village citizens and LATF members. The three categories of private domain guidelines are:

- A. All Buildings
- B. Residential Buildings
- C. Commercial and Mixed-use Buildings

### A. Design Guidelines for All Buildings

1. ***Build-to Line for Placement of Commercial and Mixed-Use Buildings, and Residential Buildings\**** : The new commercial and mixed-use buildings in the Lincoln Avenue corridor should be located close to the front of property, at 5 feet from the right-of-way line, with parking concealed from view from Lincoln Avenue and located behind buildings. Residential buildings that do not provide fences in the front yard should also be located at 5 feet from the ROW line. (See Figure 6)

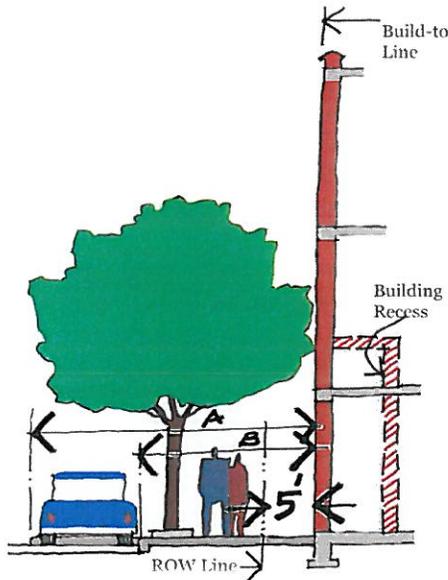


Figure 6: Illustrative Guidelines for Redevelopment of Properties

Maximum Distance from R.O.W to "Build-to" Line:	5 feet
'A': Maximum Distance from Exterior Wall to Street Curb, Including Parking Bay:	23-24 feet
'B': Maximum Distance from Exterior Wall to Street Curb, Excluding Parking Bay:	15 feet

\* Applies to only Residential Buildings without fences in the front yard. For residential buildings with fences in the front yard, see guidelines for residential buildings

2. **Building Relationship to Street.** The relationship of new buildings (all commercial and mixed-use and some residential\*) to the street and other street amenities is illustrated in Figure 7. The current right-of-way of Lincoln Avenue is 100 feet and the road pavement is 80 feet. The guidelines call for creating “bump-outs” from the existing sidewalk line of up to 8 feet or 9 feet to enclose parking bays within the sidewalk system. Recommended trees in the corridor will be installed at 25’ center-to-center. Buildings will be located at the “Build-to” line 5 feet from the Lincoln Avenue right-of-way. Touhy Avenue has the same right-of-way and pavement widths as Lincoln Avenue. The proposed guidelines will also apply for redevelopment of properties along Touhy Avenue within the recommended mixed-use/downtown hub.

(\*Only for residential buildings without fences in the front yard)

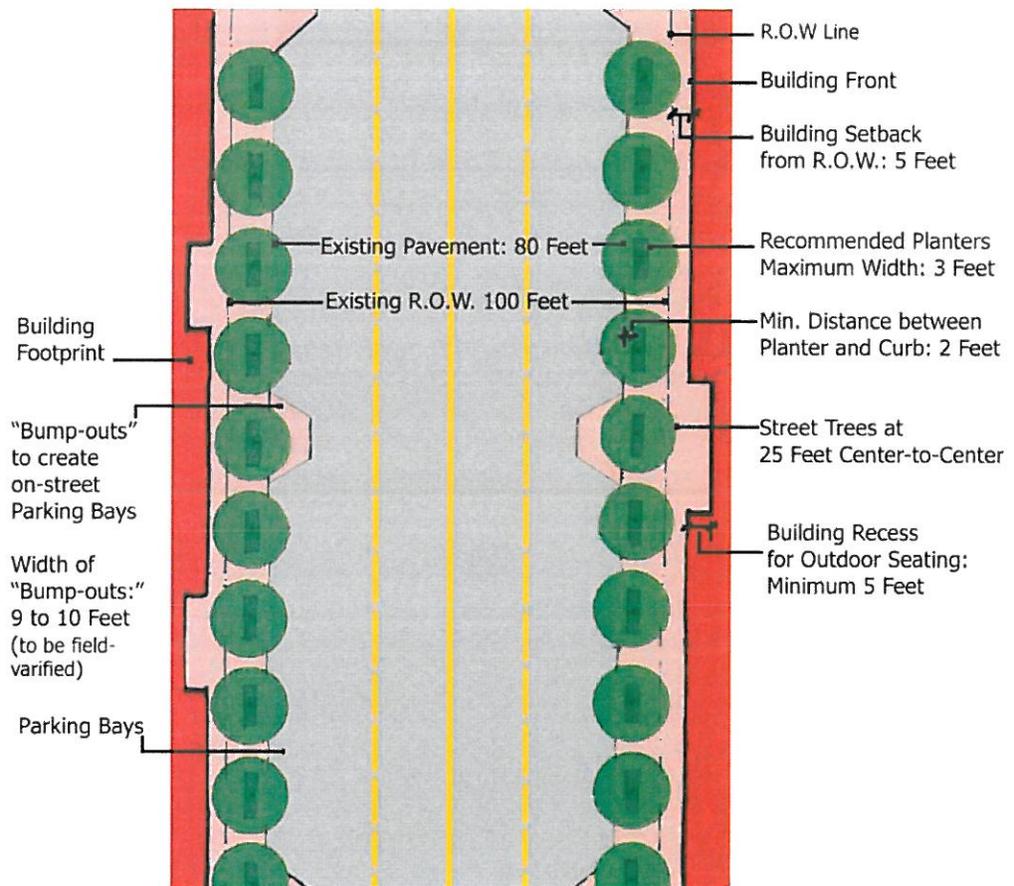


Figure 7: Lincoln Avenue Corridor Redevelopment Guidelines

(Note: These guidelines also apply to Touhy Avenue Redevelopment, in proximity to Lincoln Avenue)

3. **Building Heights:** No building in Lincoln Avenue Corridor should have more than three floors, except in the recommended mixed-use/downtown hub where the buildings may be five stories tall. As shown in Figure 8, portions of building higher than three stories, should be recessed back a minimum of 10 feet.

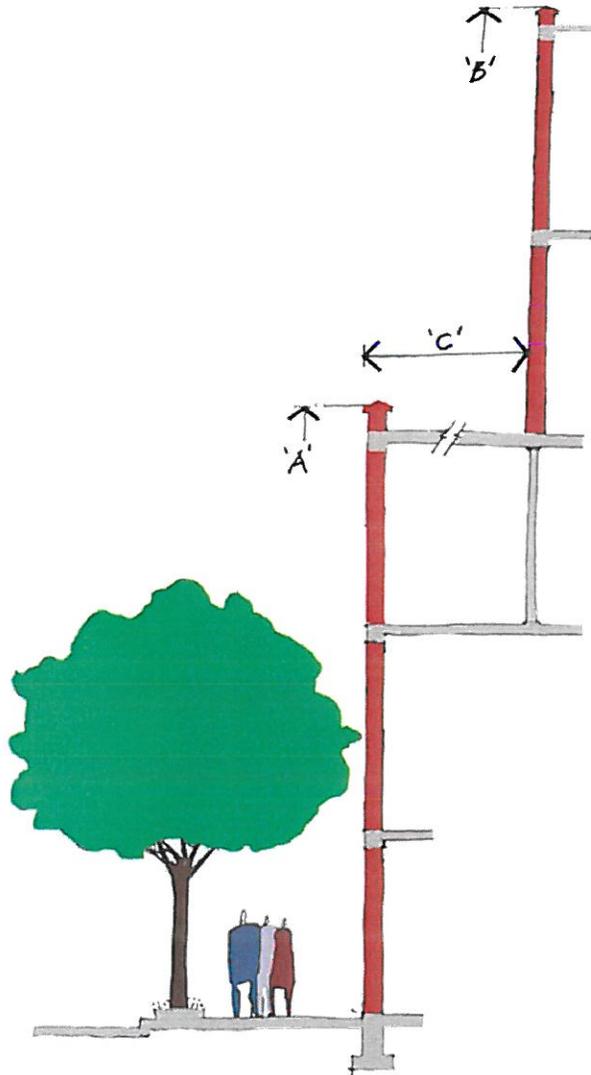


Figure 8: Illustrative Guidelines for Buildings Higher than Three Floors

- |  |                  |
|--|------------------|
| 'A': Maximum Allowable Height Immediately Adjacent to Sidewalks: | 3 Floors/38 feet |
| (Measured to the top of eave from the top of the sidewalk)       |                  |
| 'B': Maximum Allowable Height for Buildings:                     | 5 Floors/65 feet |
| (Measured to the top of the eave from the top of the sidewalk)   |                  |
| 'C': Minimum Setback for Floors Above Three Floors:              | 10 feet          |

4. **Building Orientation and Elevation:** New buildings in the corridor should be located parallel to the street frontage. The mass of buildings should be broken up in a rhythmic fashion – well articulated -- so as not to create plain building facades. Balconies and other such elements may be used to create variety and interest and yet remain integral to the overall design of the building (See Image 1)



Image 1: An example of building placed parallel to Lincoln Avenue, with a well-articulated building elevation

5. **Angled Building Facades to Streets:** On an exceptional basis, provided there is a strong reason to do so, the Village may approve buildings with angled façade toward Lincoln Avenue. Image 1 is an example of angled buildings that may be considered acceptable on exceptional basis.



Image 2: An example of angled buildings facing a major street that may be acceptable

6. **Building Corner Towers:** Buildings located at a major intersection should have a tower feature at the corner that may emphasize key entrance to the building. These

corner towers should be in harmony with the building's architecture and will be allowed to exceed the maximum permitted building height so as to create the distinct appearance. (See Image 3 for illustration)



Image 3: An example of "Corner Tower" integral to the overall design of building

7. **On-street Parking:** For new buildings facing Lincoln Avenue there should be on-street parking as part of a sidewalk system that includes "bump-outs" from the existing sidewalk line. All required parking for a use should be provided behind the buildings. (See Figure 7 for parking "bump-outs.")
8. **Building to Building Relation Across or Along Lincoln Avenue:** Buildings located next to each other along the corridor, and buildings located across the street, should have a level of design consistency through either the use of similar materials and colors or distinct architectural elements such as cornices, dormers, balconies, brackets and pendants. It is not necessary for buildings to have the same architectural style to achieve design consistency, although it can help.
9. **Impacts on Adjoining Residential Uses:** Whenever the rear parking impacts the uses on adjoining properties, specific landscape buffers, fences or short walls, or a combination thereof, should be installed by the new building owners
10. **"Dead" Walls Along Streets:** While attractive landscaping can enhance the quality of streetscape, buildings with "dead" or blank walls along the sidewalks should be avoided (or minimized). Residential or other buildings with units above a terrace and parking below that create blank walls along the street should be avoided. (See Images 4 & 5 for comparison.)



Image 4: Example of attractive landscaping that enhances streetscape



Image 5 shows the same streetscape (as in Image 4) with buildings that create blank walls along a sidewalk. This is an example of building relation to street that should be discouraged.

## B. Additional Design Guidelines for Residential Buildings

1. **Placement of Residential Buildings with Fences in Front Yard:** Residential buildings with fences in front yard should not be placed more than 10 feet from the right-of-way line. In such cases, the fence should be placed at 5 feet from the right-of-way line, with a hedgerow behind the fence toward the building side. The fences in the front yards should be wrought iron that are no more than 4' high. (See Image 6)



Image 6: An acceptable example of fence along residential buildings facing Lincoln Avenue

2. **Pedestrian Access to Residential Buildings:** Residential buildings should have direct access to units from the front street side, with strong entries to individual units emphasized by distinct architectural elements such as porticos. (See Image 7)



Image 7: Residential building along street with strong statement for entries to units

3. ***Vehicular Access to Residential Buildings from Lincoln Avenue:*** No residential building should be allowed to have garages facing Lincoln Avenue or driveways with direct vehicular access from Lincoln Avenue.
4. ***Garages in Residential Buildings:*** For residential buildings, the fronts of units at the rear of the property should not face the garages of the units in the building in front. (See Image 8 that is an example of what should not be allowed.)



Image 8 shows residential units facing garages. This should not be allowed in the corridor.

5. ***Pocket Parks in Residential Projects:*** For new residential projects on properties with shallow depth, pocket parks between buildings should be provided to create visual interest and on-site recreational opportunities. (See Image 9)



Image 9: Pocket parks for properties with shallow depth

### C. Additional Design Guidelines for Commercial and Mixed-Use Buildings

1. **Outdoor Seating:** For commercial buildings that allow outdoor seating for their patrons, the portion of buildings that houses cafes, restaurants and other such uses should be set back 5' from the normal build-to line. (See Image 10)



Image 10: Additional building set back from build-to line to allow outdoor eating and other activities

2. **Minimum Height for Building Setbacks for Outdoor Seating:** For the outdoor seating areas created by the additional setback from the build-to line, the minimum height for "recesses" should be 10 feet. (See Figures 6 & 7 for building "recesses")