



**VILLAGE OF LINCOLNWOOD
PRESIDENT AND BOARD OF TRUSTEES
COMMITTEE OF THE WHOLE MEETING
VILLAGE HALL COUNCIL CHAMBERS
6:00 P.M., OCTOBER 7, 2014**

AGENDA

- I) Call to Order**
- II) Roll Call**
- III) Minutes** – Committee of the Whole Meeting – September 16, 2014
- IV) Regular Business**
 - 1) Fiscal Year 2014/15 Budget Update (6:00 – 6:30 p.m.)
 - 2) Discussion Concerning the Proposed Process to Update the Comprehensive Plan (6:30 – 7:00 p.m.)
 - 3) Discussion Concerning the Proposed Devon Avenue Streetscape Plan (7:00 – 7:30 p.m.)
- V) Public Comment**
- VI) Adjournment**

DATE POSTED: October 3, 2014

**VILLAGE OF LINCOLNWOOD
PRESIDENT AND BOARD OF TRUSTEES
COMMITTEE OF THE WHOLE MEETING
VILLAGE HALL COUNCIL CHAMBERS,
SEPTEMBER 16, 2014**

DRAFT

Call to Order

President Turry called the Committee of the Whole meeting of the Lincolnwood Board of Trustees to order at 6:32PM, Tuesday, September 16, 2014 in the Council Chambers of the Municipal Complex, 6900 North Lincoln Avenue, Village of Lincolnwood, County of Cook and State of Illinois.

Roll Call

On roll call by Village Clerk Beryl Herman the following were:

PRESENT: President Turry, Trustees Cope, Klatzco, Leftakes, Patel,

ABSENT: Trustees Sprogis-Marohn, Elster

A quorum was present. Also present: Timothy Wiberg, Village Manager; Charles Meyer, Assistant to the Village Manager; Douglas Petroschius, Assistant Village Manager; Steven Elrod, Village Attorney; Amanda Williams, Management Analyst; Ashley Engelmann, Assistant to Public Works Director; Charles Greenstein, Village Treasurer.

Approval of Minutes

Minutes of the September 2, 2014 Committee of the Whole meeting were distributed in advance of the meeting and were examined. Trustee Patel moved to approve the minutes as presented, Trustee Cope seconded the motion. The motion passed by Voice Vote.

Adjournment to Executive Session

Trustee Leftakes moved to adjourn the Committee of the Whole meeting to Executive Session for the purpose of discussing land acquisition at 6:34P.M. The motion was seconded by Trustee Cope.

Upon a Roll Call vote the Results were:

AYES: Trustees Cope, Patel, Leftakes, Klatzco

NAYS: None

The motion passed

Reconvention

President Turry reconvened the Committee of the Whole meeting at 7:32PM.

Regular Business

1. Fiscal Year 2014/15 Budget Update

Due to time constraints, this item will be addressed at a future Committee of the Whole meeting.

Adjournment

At 7:33 Trustee Klatzco moved to adjourn the Committee of the Whole, seconded by Trustee Patel.

The motion passed with a Voice Vote.

Respectfully Submitted,

Beryl Herman
Village Clerk



MEMORANDUM

TO: President Turry and Members of the Village Board

FROM: Timothy C. Wiberg, Village Manager

DATE: October 3, 2014

SUBJECT: **October 7 Committee of the Whole Meeting**

As a reminder, the Committee of the Whole (COTW) meeting is scheduled for **6:00 p.m.** on Tuesday evening. Dinner will be available beginning at 5:15 in the Village Hall Board Conference Room. Please find below a summary of the items on the COTW agenda:

1) **Fiscal Year 2014/15 Budget Update (6:00 – 6:30 p.m.)**

Since we are a little over 25% through the current fiscal year, staff will discuss the [attached](#) report with the Board concerning the Village's finances.

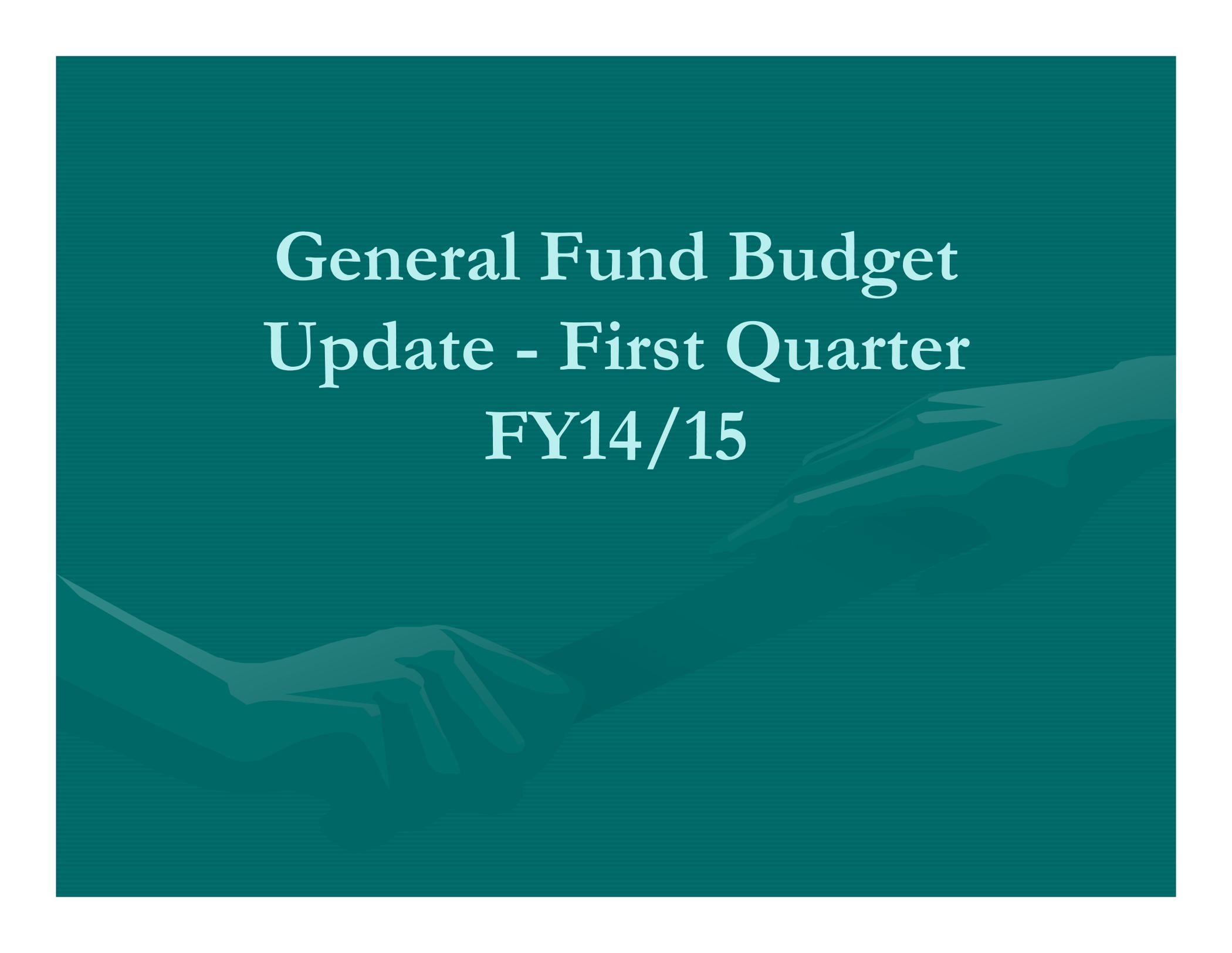
2) **Discussion Concerning the Proposed Process to Update the Comprehensive Plan (6:30 – 7:00 p.m.)**

The Village Board approved funds in the FY 2014/15 Budget to hire a consultant to assist the Village in updating the Comprehensive Plan. [Attached](#) is a memorandum from the Community Development Director summarizing a proposed process to review and update the Comprehensive Plan, as well as a draft Request for Proposals (RFP) document to secure the services of a planning consultant to assist in updating the Plan. Staff will be seeking Board direction on the proposed process and the RFP document.

3) **Discussion Concerning the Proposed Devon Avenue Streetscape Plan (7:00 – 7:30 p.m.)**

At the September 16 Village Board meeting, the Board tabled the recommendation from staff to award a contract to Christopher Burke Engineering to prepare a streetscape improvement plan for Devon Avenue, from McCormick to Lincoln Avenue. The Board requested additional information pertaining to options contained in the plan for work to be conducted on the south side of Devon which is in the City of Chicago, and the option for preparing plans to implement a "road diet" for Devon whereby the street would be reduced to one lane in each direction to allow for bicycle lanes, additional street parking, and other streetscape improvements. [Attached](#) is a memorandum from the Assistant to the Public Works Director summarizing the components of the proposed plan which will be discussed on Tuesday evening.

If you should have any questions concerning these matters, please feel free to contact me.



**General Fund Budget
Update - First Quarter
FY14/15**

Budget Update FY14/15

	Adopted Budget FY14/15	Projected Actual FY14/15	Variance
Revenues	\$19,736,489	\$19,636,489	\$(100,000)
Expenditures	19,540,242	19,879,242	339,000
Deficit	196,247	(242,753)	(439,000)
Transfers - Net	(196,247)	(196,247)	0
Net change to Fund Balance	\$ 0	\$(439,000)	\$(439,000)

GF Revenues FY14/15

- Sales tax - Projecting as budgeted
- Property taxes - Would decrease approx. \$100,000 due to delay of the purchase of the UP property.
- Building permit revenue – tracking as budgeted (Village has budgeted \$250,000 in permit revenue from Shoppes at Lincoln Pointe)
- Summer Camp Results - net income approx. \$20,000 over budget
- Pool Results - net income approx. \$30,000 over budget

GF Expenses FY14/15

- If purchase of UP property is delayed this year Village will have to pay approx. \$389,000 to SD #74 (not budgeted this year)
- Liability insurance has been decreasing. Village will save approx. \$50,000 from this year's budgeted amount

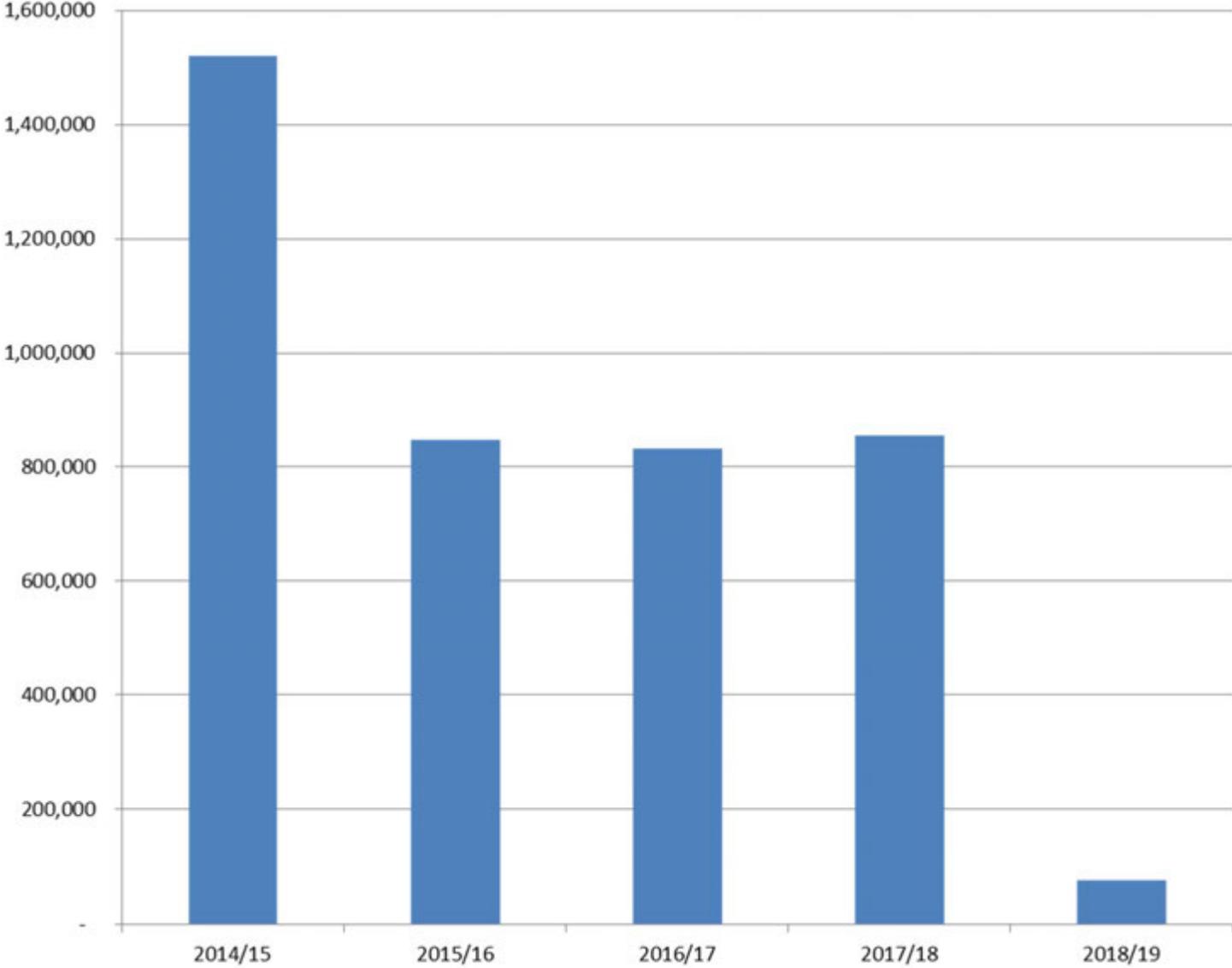
FY2015/16 Revenue Issues

- Projected 2014 Tax Levy Increase – 1.5% increase - \$79,634
(2013 levy – 1.7% increase - \$88,744)
- Revenue increase from pay-off of 2004 General Obligation Refunding Bond (\$710,882)
- Sustainability of recovery of current businesses in the Village and new business development
- U.S. economy is still in slow growth mode, global economy is still lagging

FY2015/16 Expenditure Issues

- IMRF Pension Rate Decrease (.2%)
(Current rate 10.91%)
- Police Pension Funding - minimal decrease from current year budget (\$39,328)
- GF capital expenditures – Replacement of Fire Engine
- Pay off remaining loans on the purchase of the Fire truck in 2010 (\$574,340)
- Purchase of UP property – delayed from FY2015

Debt Principal Re-payment Schedule



Memorandum

To: Timothy C. Wiberg
Village Manager

From: Timothy M. Clarke, AICP
Community Development Director

Date: September 29, 2014

**Subject New Comprehensive Plan
 Appointment of a Citizens Committee and RFP Release**

The Preparation, updating or revising of a Comprehensive Plan often occurs through a Plan Commission and State law requires the convening of a public hearing by the Plan Commission prior to adoption of a Comprehensive Plan. As you may recall, the Village Board has requested, and the Plan Commission is currently reviewing, that portion of the Comprehensive Plan concerning Lincoln Avenue.

You have advised that the Village Board has decided that instead of the Plan Commission, to use an ad hoc citizen body selected to specifically guide the preparation of a new Comprehensive Plan for the Village. While using such a citizen's body is permissible, and in fact was utilized by the Village in the preparation of the 2001 Village Comprehensive Plan, procedurally, adoption of a new Comprehensive Plan still requires convening of a public hearing (and recommendation) by the Plan Commission.

Creation of Citizens Committee

Appointment to a Citizens oversight Committee for the preparation of a Comprehensive Plan is a political decision to be made by the Village President and Village Trustees. One reason to form a Citizens Committee, rather than utilize the Plan Commission, is to ensure broad representation and a wide-range of backgrounds and perspectives, in order to fully represent the community. Since the Comprehensive Plan is intended to guide future decision making, particularly concerning matters related to land use, appointing community representatives who are forward-looking and who understand issues facing the community, particularly those concerning development and land use, will help to achieve a useful Comprehensive Plan that will help to guide the community for many years into the future. In this regard, staff would suggest a citizen body of seven forward-looking individuals for this oversight committee. Staff believes a body of seven members is optimal for this Committee and being an odd number ensures decisive votes.

Suggested Committee Composition

While any interested citizen can be appointed to this Committee, staff recommends a majority of members have an understanding of Village government procedures and selecting individuals from existing Village bodies helps to ensure this understanding. To aid the Village President and Village Board, staff forwards the following suggestions for consideration and discussion.

Plan Commission Representation

Of any Village recommending body, the Plan Commission is most involved in land use matters. Moreover the Comprehensive Plan that is developed by the citizens committee will need to be considered by the Plan Commission at a public hearing. Therefore, continuity between the Citizen's body and the Plan Commission is especially important. Because of the importance of the Plan Commission to the work of the Citizens Committee, staff suggests up to two Plan Commission members be considered for appointment.

Village Board Representation

Since the Village Board has ultimate authority over land use policy and will ultimately adopt the Plan and also because the Plan is intended to guide current and future Village Board members, staff suggests up to two Village Board members be appointed to this Citizen Oversight Committee.

Economic Development Commission Representation

Members of the Economic Development Commission bring a good understanding of development issues confronting the community. This perspective is invaluable to crafting a useful Comprehensive Plan and staff suggests the appointment of at least one EDC member to this Committee.

Staff believes representation from the above boards to be especially important to the work of crafting a new Comprehensive Plan. Below are additional Village recommending bodies that could provide additional expertise and valuable perspectives to the work of this citizen committee.

Park Board Representation

In recent years, parks and recreation have become more visible with several improvements occurring, underway or on the drawing board, such as the planned network of bike trails. To bring this perspective to the table in the preparation of the Comprehensive Plan, staff suggests a member of the Parks Board be considered for this Comprehensive Plan Committee.

Traffic Commission Representation

Parking and traffic issues are likely to become more pronounced in future years and bringing this perspective to the Comprehensive Plan Committee would be helpful. A representative of the Traffic Commission could bring this perspective to this work.

Zoning Board of Appeals Representation

Although the ZBA typically considers only hardship cases for persons seeking variations, these matters all relate to the Zoning Code, an important land use document. A member of the Zoning Board therefore could be a good candidate for the Comprehensive Plan Committee

Other Appointment Suggestions

In addition to the above suggestions, other suggested considerations for this Citizen Committee would be any citizen at-large, with no current ties to a recommending body, who has expressed interest in the long term health of the community. Other considerations could be representatives from other Lincolnwood organizations, such as the Chamber of Commerce.

Proposed Committee Meeting Date and Schedule

In choosing members of the oversight committee, availability and a commitment to attend Committee Meetings is essential. In reviewing the Village calendar, it appears that the best evening to convene this oversight committee is the 2nd Thursday of the month at 7PM. Staff anticipates the work of the Committee to be for approximately one year. Staff recommends that members selected for this Committee indicate prior to their appointment, their general availability and commitment to attend meetings on the evenings of the 2nd Thursday of the month.

Consultant Engagement

The work of this citizen committee will be assisted by an experienced planning consultant. As discussed by the Village Board during this year’s budget workshop, the budgeted amount for this initiative is \$50,000, with \$25,000 budgeted this year and another \$25,000 to be budgeted next year. Attached is the proposed Request For Proposals (RFP) to solicit this consultant.

Anticipated Timeline

Based on Village Board direction to proceed on October 7, 2014, below is the anticipated schedule for consultant selection and the convening of the Committee. Under this schedule, the anticipated first meeting of the Comprehensive Plan Committee is Thursday January 8, 2015 at 7PM.

Tuesday October 7	Village Board Consideration
Friday October 10	RFP released, issued to area planning firms
Friday November 7	Deadline for proposal submittal
Monday November 10 to	Selection/consultant interviews
Wednesday December 3	
Tuesday December 16	Village Board approves consultant Citizen appointments made (no later than this meeting)
December 17-December 31	Consultant contract executed
Thursday January 8 7PM	Citizen Committee first meets to begin work

Under this timeline, appointments to the Comprehensive Plan Committee can occur at any of the remaining Village Board meetings this year, but would need to be made no later than the Village Board's December 16, 2014 meeting.

Attachments

1. RFP



Village of Lincolnwood

Request For Proposals Preparation of a New Comprehensive Plan

Submittal Deadline

4PM Friday November 7, 2014

**Submit Response Marked "New Comprehensive Plan Proposal" to
Timothy M. Clarke, AICP
Community Development Director
Village of Lincolnwood
6900 North Lincoln Avenue
Lincolnwood, Illinois 60712**

Synopsis

The Village of Lincolnwood, an attractive north suburban inner ring community of Chicago, is requesting proposals from experienced consulting firms to assist the Village in preparing a new Comprehensive Plan for the Village. A 7-member citizen committee representing various perspectives of the Village is expected to be appointed to work with the chosen firm on preparation of this plan. **Responses to this RFP are due no later than 4PM on Friday November 7, 2014.**

Introduction and Background

Established in 1911, Lincolnwood is situated approximately 10 miles north of downtown Chicago and three miles west of Lake Michigan. It is a first tier suburb comprising approximately 2.7 landlocked square miles immediately adjacent to Chicago's north boundary. The Village's 12,590 residents (2010 census) are ethnically and racially diverse and the Village has a large senior population. The Village became Home Rule in 1998.

The Village prides itself on its attractive residential neighborhoods with upscale housing. More than 75% of the Village's housing stock is detached single family homes with approximately 55% of housing units built prior to 1960. Teardowns have been a phenomenon in the Village with nearly 15% of the Village's housing stock having been built since 1990, typically by replacing an older home.

Although viewed by many residents as a bedroom suburb, Lincolnwood is a significant employment center. Total employment in the Village in 2011 was 9,898 with 8,173 jobs classified as non-manufacturing. While total employment in the Village has fallen from a high of 12,817 in 1992, the number of businesses located in the Village continues to grow, from 662 businesses reported in 1992 to 770 businesses in the Village in 2011. The Village's large nonresidential tax base allows for some of the lowest property tax rates to be found in North Cook County.

Many of the current land use issues confronting the Village result from functional obsolescence of initial development, various redevelopment pressures and the interplay between the Village's residential and business areas, especially where these uses adjoin or are in close proximity.

The Village's current Zoning Code was adopted in 2008, replacing a Code that dated to 1946 with a comprehensive amendment that occurred in 1962. The Village's first Comprehensive Plan, 16 pages in length, was not adopted until 1992 and was subsequently amended by a new Plan in 2001. The 2001 Comprehensive Plan was further amended in 2006 to incorporate the 2005 Lincoln Avenue Corridor Plan.

Scope of Services

To help guide future land use decision-making, the Village desires a forward-looking Comprehensive Plan. As such, the Village requires an innovative, creative and practical plan for addressing the Village's existing development patterns and anticipated redevelopment trends. In addition to land use expertise, it is expected that the selected consultant will have sufficiently broad experience to integrate various related disciplines including transportation and open space planning.

In developing this plan, the selected firm will work directly with a 7-member citizen committee appointed specifically for this work. This committee is anticipated to represent various recommending bodies and perspectives of the Village. It is anticipated that meetings of this Committee will take place as necessary at 7PM on the 2nd Thursdays of the month. The Community Development Department has been assigned to staff this committee and to oversee the work of the consultant.

The final Plan document should include two elements in reproducible electronic form: 1) the Comprehensive Land Use Map; and, 2) a Narrative Plan report which identifies Goals, Objectives and Policies. In concert with the Committee, latitude will be given the selected firm in the organization of the work program and plan document. At minimum however the following sections shall be included in the narrative:

- ◆ Introduction and Purpose of Plan
- ◆ Community Overview and Profile
- ◆ Summary of Goals, Objectives and Policies
- ◆ Description of Land Use Plan
- ◆ Specific Sections on:
 - Neighborhoods
 - Housing
 - Recreation and Open Space
 - Natural Resources
 - Transportation and Parking
 - Business, Commercial and Industrial Areas
 - Urban Design and Beautification
 - Potential Redevelopment Areas
 - Economic Development
- ◆ Action Agenda for Plan Implementation

Comprehensive Planning Issues

The following topics are likely to be raised, discussed or determined pertinent in preparation of the Village's Comprehensive Plan.

- *Neighborhood Preservation and Housing*, including teardowns and ways to maintain and enhance residential character and quality neighborhoods
- *Manufacturing Areas*, including long term viability, viability of retail reuse or potential alternate uses
- *Business Corridors*, (such as portions of Lincoln, Devon and Touhy Avenues), including, parking adequacy, urban design, buffering adjacent residential areas, and ways to encourage attractive retail reinvestment
- *Transportation and Traffic* concerns and issues, including traffic congestion and traffic calming measures and alternate transportation means such as potential local bus service

- *Density and Land Use Intensity*, especially concerning new development
- *Challenged, Vacant or Underperforming Business Properties*, and methods to induce attractive redevelopment
- *Combined Sewer Infrastructure* and its inadequacy during heavy rain events

(Note, the above list is not intended to be an exhaustive or all inclusive list)

Available Material

The Village has undertaken a variety of studies and reports that may be helpful in the preparation of the Comprehensive Plan. All of these studies and reports are available on the Village’s website, www.lincolnwoodil.org and may be accessed under the “Village Government” tab on the Home Page. The Village Zoning Code and Subdivision regulations, as well as other Village regulations, can also be found in the Code of Ordinances also located under the “Village Government” tab on the Home Page.

The Village website also contains related demographic reports pertaining to the Village. These Demographic Reports can be found under the “Our Community” tab on the Home Page. Also through this tab, the Village’s GIS Mapping System can be accessed.

Anticipated Completion Date and Costs

Completion of this assignment and preparation of the Comprehensive Plan is expected within one year. The Village Board has authorized a total maximum cost for this initiative of \$50,000.

Submittal Requirements

Interested firms should submit four hard copies as well as an electronic version of the following items:

1) Qualifications of the Firm

A Statement of Qualifications, including vita or resume of personnel proposed for assignment to this engagement; the name of the designated primary person for this work; a list of previous clients; and a description of how the firm will assure quality of staff skills and work product.

In addition, a summary of relevant experience especially as it relates to comprehensive plan preparation in inner ring suburban locations and experience with citizen bodies. *(Submittal of one sample copy of a completed Comprehensive Plan for a similar community is suggested and welcomed)*

2) Approach to Engagement and Proposed Work Program

Provide a narrative describing and providing an outline of the proposed work program including description of approach to this assignment; the proposed steps or actions to be taken in the preparation of the Comprehensive Plan; the proposed means of engaging and soliciting community input; and, the anticipated timeline for work components and plan completion.

In addition, an estimate of the number of meetings envisioned with the Committee, the expectations regarding the Committee's role in plan development and tasks expected of staff, acknowledging staff's limited resources and personnel. Specifically identify the work program receivables.

3) Cost

An engagement proposal with an all-inclusive, not to exceed cost estimate for completion of the assignment and the work program proposed (such figure to include all estimated out of pocket costs); the hourly rate and the number of work hours proposed for each staff level; and a listing by category of all anticipated out of pocket expenses.

In addition, hourly rates for additional work or services that may be deemed necessary at a later date.

Submittal Deadline

Proposals containing the above information must be submitted **no later than 4PM Friday November 7, 2014**. Submittals should be clearly marked "*New Comprehensive Plan Proposal*" and submitted to:

**Timothy M. Clarke, AICP
Community Development Director
Village of Lincolnwood
6900 Lincoln Avenue
Lincolnwood Illinois 60712**

Selection Process

Responses to this RFP will be reviewed by Village staff and on review selected firms may be invited to an interview. Any staff recommendation regarding consultant engagement is subject to approval of the Village Board. It is anticipated that the Village Board will approve the consultant at its December 16, 2014 meeting and the Committee and consultant will convene and begin work on Thursday January 8, 2015.

Terms

There is no expressed or implied obligation of the Village to reimburse firms for any costs incurred in preparing proposals in response to this request. The Village reserves the right to reject any and all proposals and to modify the scope of services. The Village further reserves the right to retain all proposals submitted and to use any idea in a proposal regardless of whether that proposal is selected.

The successful firm will be required to enter into an agreement acceptable to the Village. All work under an agreement with the Village shall become the property of the Village of Lincolnwood. During the period of engagement, the firm shall provide monthly detailed invoices to the Village for actual hours worked, not to exceed the maximum all-inclusive quoted price. During the engagement ten percent may be withheld by the Village to assure completion of the work and acceptable delivery of the final report and map. The

selected firm shall not replace staff assigned to this engagement and shall not subcontract to another firm without the expressed written consent of the Village

The successful firm shall comply with all applicable laws, regulations and rules promulgated by any federal, state, county and/or other governmental unit or regulatory body now in effect, or which may be in effect, during the performance of the work. Submission to the Village of a response to this RFP indicates acceptance by the firm with the conditions and terms contained in this RFP.



MEMORANDUM

TO: Timothy C. Wiberg, Village Manager

FROM: Ashley Engelmann, Assistant to the Public Works Director

DATE: September 26, 2014

SUBJECT: Devon Avenue Streetscape Proposal

Background:

On June 24, 2014 the Devon-Lincoln TIF District (“TIF District”) was established. During the process of establishing the TIF District in September of 2013 the Village and the City of Chicago engaged the Urban Land Institute (ULI) through the Chicago Metropolitan Agency for Planning (CMAP) to provide recommendations for redevelopment of the six-block span from Lincoln Avenue to McCormick Boulevard on Devon Avenue within the TIF District. As part of this report several recommendations were made regarding revitalization of the area. Specifically, the report recommends implementing streetscape enhancements. On October 28, 2013 the report was presented and subsequently endorsed by the Village Board.

Upon establishment of the TIF District staff convened to discuss implementing the streetscape recommendation. Staff requested that the Village’s Engineering Firm Christopher B. Burke Engineering, Ltd. (CBBEL) develop concept plans for the Devon-Lincoln TIF District.

Scope of Work:

On September 16, 2014 at the regularly scheduled Village Board meeting staff presented a proposal from CBBEL to develop concept plans. The Board tabled the item and requested that staff return at a Committee of the Whole to further review the alternatives discussed within the proposal.

A summary of the proposal may be found below:

- Develop typical cross sections for four primary alternatives for the Devon Avenue Corridor:
 - Alternative 1-Parkway enhancements on the north side only
 - Alternative 2-Parkway enhancements on the north and south side
 - Alternative 3-Road diet (lane reduction or rechannelization) for bike lane and landscaped medians
 - Alternative 4-Road diet for bike lane and widened parkways
- Conduct site visits
- Topographic survey
- Stakeholder outreach
- Develop 4 streetscape concepts
- Crosswalk enhancements
- Cost estimate and schedule
- Present concepts to the Village Board

The fee to complete the aforementioned work is \$47,500. During the September 16 Village Board meeting, the Board requested additional information regarding alternative 3 and 4. Specifically, for alternative 3 whether a road diet would be ideal for Devon Avenue and for alternative 4 whether the City of Chicago would be interested in or plans to implement similar improvements.

Alternative 3-Road Diet for Bike Lane and Landscaped Medians

This alternative would evaluate the use of a road diet on Devon Avenue. A road diet is a traffic calming technique where travel lanes are reduced to provide a reduction in the width of the road in order to improve safety and provide additional room for other modes of travel such as bike lanes and pedestrians and the addition of street parking. The alternative would include assessing parkway enhancements along with landscape medians on Devon Avenue. Constructing a center median on Devon Avenue would require the removal of one through lane in each direction. The current average daily traffic (ADT) count for Devon Avenue is within the Illinois Department of Transportation's allowable ADT count for consideration of a road diet.

Alternative 4-Road Diet for Bike Lane and Widened Parkways

This alternative would evaluate both the north and south side of Devon Avenue for widened parkways as well as bike lanes. This alternative would require coordination with the City of Chicago. Staff contacted Deputy Chicago Department of Transportation Commissioner Luann Hamilton regarding Chicago's potential interest in such a project, at this time staff is awaiting a response from her office.

Should alternatives 3 and 4 be removed from the scope of work the not-to-exceed cost would be reduced by \$4,875 for a total cost of \$42,625.

Project Schedule:

CBBEL will develop four typical cross sections for Devon Avenue. Upon review with the Village Board a cross section will be selected for further development into a concept plan. Once the concept plan is approved CBBEL will prepare a proposal to develop civil engineering construction drawings and specifications for bid. Staff envisions construction of the project in FY2015/2016.

Village Board Direction:

Staff is seeking direction from the Village Board regarding moving forward with CBBEL's proposal as presented.

Attachments:

1. Proposal from Christopher B. Burke Engineering



CHRISTOPHER B. BURKE ENGINEERING, LTD.

9575 West Higgins Road Suite 600 Rosemont, Illinois 60018 TEL (847) 823-0500 FAX (847) 823-0520

August 22, 2014

REVISED September 25, 2014

Village of Lincolnwood
7001 N. Lawndale Avenue
Lincolnwood, IL 60712

Attention: Ms. Ashley R. Engelmann
Assistant to the Public Works Director

Subject: Proposal for Professional Engineering Services
Devon Avenue Streetscape – Concept Planning
Village of Lincolnwood

Dear Ashley:

As requested, Christopher B. Burke Engineering, Ltd. (CBBEL) is providing this proposal for professional engineering services related to the Village of Lincolnwood's Devon Avenue Streetscape Project. Included in this proposal are our Understanding of the Assignment, Scope of Services and Estimate of Fee.

UNDERSTANDING OF THE ASSIGNMENT

CBBEL understands that the Village of Lincolnwood would like to develop concepts for streetscape improvements for the portion of Devon Avenue between Lincoln Avenue and McCormick Boulevard. CBBEL also understands that the Village of Lincolnwood has Tax Increment Financing (TIF) dollars available to fund improvements for the portion within the Village limits however the Village is interested in developing a landscape median option and would like to look at the effects of a road diet. The development of these elements will affect both the north and south sides of the roadway and since the southern half of Devon Avenue is within the City of Chicago we will want to reach out to the Alderman of that ward and the Chicago Department of Transportation (CDOT) for their input on these features.

CBBEL envisions developing typical cross sections for 4 primary alternatives for this portion of Devon Avenue. Once an alternative is selected, a concept plan shall be prepared.

➤ Alternative 1 – Parkway Enhancements along north side only

This is the simplest scenario. It will look at only the north portion of Devon Avenue. This concept will show pedestrian pavement improvements, roadway / pedestrian lighting, ADA

route improvements, site furnishings and general enhancements to the aesthetics of these 6 blocks.

➤ Alternative 2 – Parkway Enhancements along north and south sides

This alternative will look at improving both the north and south parkways of Devon Avenue. This concept will show pedestrian pavement improvements, roadway / pedestrian lighting, ADA route improvements, site furnishings and general enhancements to the aesthetics of these 6 blocks.

➤ Alternative 3 – Road Diet for bike lane and landscaped medians

This alternative includes the parkway enhancement elements plus looks at the development of landscape median(s) in the center of Devon Avenue. The development of the median will require the removal of one of the through lanes in each direction which will necessitate a Road Diet for this portion of Devon Avenue. The current ADT for this portion of Devon is 14,300 which is at the upper end for IDOT allowable Road Diets. Since the dividing line between the Village of Lincolnwood and the City of Chicago is the center of Devon Avenue, cooperation and approval will be sought from both the adjoining Alderman's office and CDOT. This scenario does not disturb the south curb line or any of their existing lighting.

➤ Alternative 4 – Road Diet for bike lane and widened parkways

The last alternative will look at a 'Complete Streets' option where both the north and south sides of the corridor are enhanced. In this option it will be important to coordinate the proposed appearance and infrastructure improvements with the City of Chicago. The Alderman, CDOT and the various utility divisions for the City of Chicago will be involved in the planning process.

Once the board approves the concept design, CBBEL will provide a proposal for Phase 2 / Design Engineering for developing final plans, specifications and soliciting bids for construction.

SCOPE OF SERVICES

Task 1 – Site Visits (Initial Meeting and Walkthrough): CBBEL Staff will meet with representative of the Village to understand their goals and desires for the project area. Members of our project team will accompany VILLAGE personnel on a site visit of the project area (N. Lincoln Avenue – North McCormick Boulevard). The team will walk the entire project length including portions of the side streets and determine the exact project limits and identify any unique features which may affect the streetscape design. CBBEL will prepare meeting minutes and a photograph log for use in preparing the design. As the project design continues, Team members will return to the site to collect additional data as issues are identified.

Task 2 – Topographic Survey: CBBEL will perform a topographic survey within the Concept Project Limits (Lincoln to McCormick) to understand design and utility parameters. The survey will extend to the alleyway down side streets. The survey will be used as an accurate base for developing the various concepts and for final design engineering.

Task 3 – Stakeholder Outreach: When we begin to explore the alternatives, we will contact the local Alderman's office, CDOT, and the various City of Chicago Utility departments to identify potential conflicts, goals and desires for this portion of the project.

Task 4 – Streetscape Concept Typical Section: Using the information gathered in the above tasks, CBBEL will create a Concept Streetscape Typical Cross Section for the project area, illustrating alternative one.

Task 5 – Streetscape Concept Plan: Once the preferred alternate is selected by Village Staff, CBBEL will prepare a concept Streetscape plan of the preferred alternative. The plan will consist of a plan view and typical illustrations of the proposed appearance.

Task 6 – Crosswalk Enhancements: CBBEL will investigate and provide various crosswalk enhancement alternatives for the corridor.

Task 7 – Cost Estimate and Schedule: An estimate will be developed for alternative one along with an accompanying implementation timeline.

Task 8 – Staff Meetings and Village Board Presentation: CBBEL will attend 2 working staff meetings to discuss the project scope and solicit input from Village Staff and one meeting with the Economic Development Commission. The final product will be a presentation of the concept design to the Village Board.

Task 9 – Streetscape Concept Typical Sections for Alternatives 2-4: Using the information gathered in the above tasks, CBBEL will create Concept Streetscape Typical Cross Section for the project area, illustrating alternatives 2-4.

Task 10 – Cost Estimate and Schedule for Alternatives 2-4: An estimate will be developed for alternatives 2-4 along with an accompanying implementation timeline.

ESTIMATE OF FEE

TASK	FEE
Task 1 – Site Visit	\$ 1,000
Task 2 – Full Topographic Survey	\$ 16,500
Task 3 – Stakeholder Outreach	\$ 3,000
Task 4 – Streetscape Concept Typical Section	\$ 500
Task 5 – Streetscape Concept Plan	\$ 10,000
Task 6 – Crosswalk Enhancements	\$ 3,000

Task 7 – Cost Estimate and Schedule	\$ 1,125
Task 8 – Staff Meetings and Village Board Presentation	\$ 5,000
	<hr/> Direct Costs \$ 2,500
	<hr/> SUBTOTAL \$42,625

OPTIONAL TASKS

Task 9 – Streetscape Concept Typical Sections for Alternatives 2-4	\$ 1,500
Task 10 – Cost Estimate and Schedule for Alternatives 2-4	\$ 3,375
	<hr/> TOTAL \$47,500

We will bill you at the hourly rates specified in our previously accepted Master Agreement. Direct costs for blueprints, photocopying, mailing, overnight delivery, messenger services and report compilation are not included in the fee estimate. Please note that any requested meetings or additional services are not included in the preceding fee estimate and will be billed at the hourly rates.

Please sign and return one copy of this agreement as an indication of acceptance and notice to proceed. Please feel free to contact us anytime.

Sincerely,



Christopher B. Burke, PhD, PE, D.WRE, Dist.M.ASCE
President

THIS PROPOSAL ACCEPTED FOR THE VILLAGE OF LINCOLNWOOD:

BY: _____

TITLE: _____

DATE: _____



**VILLAGE OF LINCOLNWOOD
PRESIDENT AND BOARD OF TRUSTEES
REGULAR MEETING
VILLAGE HALL COUNCIL CHAMBERS
7:30 P.M., OCTOBER 7, 2014**

I. Call to Order

II. Pledge to the Flag

III. Roll Call

IV. Approval of Minutes

1. Board Meeting Minutes – September 16, 2014

V. Warrant Approval

VI. Village President's Report

1. Swearing in of Police Officers Justin Wolfe and Michael Pignato

VII. Consent Agenda (If any one wishes to speak to any matter on the Consent Agenda, a Speaker's Request Form must be completed, presented to the Village Clerk, and the matter will be removed from the Consent Agenda and added to Regular Business.)

1. Approval of a Resolution Authorizing the Village Manager to Execute a Renewal Agreement with the Lincolnwood Town Center, LLC, Inc. to Regulate Parking and Traffic within the Parking Lots (Appears on the Consent Agenda because it is a Routine Function of Government)
2. Approval of an Ordinance Granting a Zoning Map Amendment to Rezone 6755 North Cicero Avenue to the B-2 General Business Zoning District (Appears on the Consent Agenda because it was Approved at a Previous Village Board Meeting)
3. Approval of a Recommendation by the Zoning Board of Appeals to Approve a Variation from Section 4.11 of the Zoning Code to Permit a One-Story Addition to Encroach Into the Required Side Yard Setback for the Property at 7131 North Keating Avenue (Appears on the Consent Agenda because it was Approved Unanimously by a Recommending Body)

VIII. Regular Business

4. Consideration of a Recommendation by the Zoning Board of Appeals to Approve a Variation from Section 4.11 of the Zoning Code to Permit a One-Story Addition to Encroach Into the Required Side Yard Setback for the Property at 5080 West North Shore Avenue
5. Consideration of a Recommendation by the Plan Commission Concerning a Text Amendment to the Comprehensive Plan Concerning Residential Development within the Lincoln Avenue Corridor

6. Consideration of an Ordinance Waiving the Competitive Bidding Process and Accepting a Proposal from Christopher B. Burke Engineering, Ltd. for the Provision of Civil Engineering Services to Develop a Streetscape Concept Plan for the Devon-Lincoln Tax Increment Financing District in the Not-to-Exceed Amount of \$47,500

IX. Manager's Report

X. Board, Commission, and Committee Reports

XI. Village Clerk's Report

XII. Trustee Reports

XIII. Public Forum

XIV. Executive Session

An Executive Session is Requested to Discuss Potential Litigation

XV. Adjournment

DATE POSTED: October 3, 2014

All Village Board meetings are broadcast live to residents on Comcast Cable Channel 6, AT&T U-VERSE Channel 99, and online at Lincolnwood.tv at 7:30 p.m. Rebroadcasts of Village Board meetings can be viewed one week following the live broadcast at 1:00 p.m. and 7:30 p.m. on cable television or online at Lincolnwood.tv.

**VILLAGE OF LINCOLNWOOD
PRESIDENT AND BOARD OF TRUSTEES
REGULAR MEETING
VILLAGE HALL COUNCIL CHAMBERS
SEPTEMBER 16, 2014**

Draft

Call to Order

President Turry called the Regular Meeting of the Lincolnwood Board of Trustees to order at 7:35 P.M., Tuesday, September 16, 2014, in the Council Chambers of the Municipal Complex, 6900 North Lincoln Avenue, Village of Lincolnwood, County of Cook, and State of Illinois.

Pledge to the Flag

The Corporate Authorities and all persons in attendance recited the Pledge of Allegiance to the flag of our country.

Roll Call

On roll call by Village Clerk Beryl Herman the following were:

PRESENT: President Turry, Trustees Patel, Cope, Klatzco, Leftakes

ABSENT: Trustees Sprogis-Marohn, Elster

A quorum was present. Also present: Timothy Wiberg, Village Manager; Charles Meyer, Assistant to the Village Manager; Aaron Cook, Community Development Manager; Steven Elrod, Village Attorney; Amanda Williams, Management Analyst; Charles Greenstein, Village Treasurer; Ashley Engelmann, Assistant to the Public Works Director; Manuel Castaneda, Director of Public Works.

Approval of Minutes

The minutes of the September 2, 2014 regular Village Board meeting had been distributed in advance and were examined. Trustee Klatzco moved to approve the presented minutes. The motion was seconded by Trustee Patel. Trustee Leftakes abstained. The motion passed with a Voice Vote.

Warrant Approval

Trustee Klatzco moved to approve Warrants in the amount of \$539,077.95. Trustee Patel seconded the motion.

Upon a Roll Call vote by the Village Clerk the results were:

AYES: Trustees Klatzco, Patel, Leftakes, Cope

NAYS: None

The motion passed

Village President's Report

- Dr. Michael Wahl, Director of the Illinois Poison Center announced the selection of Lincolnwood Fire Chief Michael Hansen for the Lifesaver of the Year Award. Dr. Wahl spoke of the work done by Chief Hansen, for the good of the community and for the Village's success in the campaign to raise funds. Chief Hansen spoke offering thanks for the Award. President Turry congratulated the Chief.
- President Turry announced that the Blood Drive will be held on Thursday, September 18 from 1PM to 7pm in the Village Hall Council Chambers. President Turry expressed the hope that residents and staff will donate blood to save lives. He also stated that they are looking for 2 volunteers to run the blood drive, please contact the Village if you are interested.
- Happy New Year to those of the Jewish faith who will be celebrating Rosh Hashanah, the Jewish New Year 5775.

Consent Agenda

President Turry announced the following items on the Consent Agenda.

1. Approval of an Ordinance Authorizing the Village Manager to Execute the Local Agency Agreement and Engineering Agreement Required by the Illinois Department of Transportation for Participation in the Congestion Mitigation Air Quality Grant Program for the Construction of the Proposed Bike/Pedestrian Path on the ComEd Right-of-Way

Trustee Patel requested removal of Item #2 from the Consent Agenda, to be placed as Item 4 under Regular Business

2. Approval of an Ordinance Waiving the Competitive Bidding Process and Accepting a Proposal from Christopher B. Burke Engineering, Ltd. for the Provision of Civil Engineering Services to Develop a Streetscape Concept Plan for the Devon-Lincoln Tax Increment Financing District in the Not-to-Exceed Amount of \$47,500

Trustee Patel moved to approve the Consent Agenda as amended. Trustee Klatzco seconded the motion.

Upon a Roll Call vote the results were:

AYES: Trustees Patel, Klatzco, Leftakes, Cope

NAYS: None

The motion passed

Regular Business

3. Consideration of a Recommendation by the Zoning Board of Appeals to Approve a Variation from Section 4.11 to Permit a One-Story Addition to Encroach Into the Required Side Yard Setback for the Property at 5080 West North Shore Avenue

This item was presented by Mr. Cook.

Nature of Request

Homeowner desires to construct a 348 square foot addition to existing home at 5080 West North Shore Avenue (N E Corner of North Shore and Navajo)

Variations Requested

*Side Yard Setback Variation

- Side yard setback requirement is 8 feet 1 3/16th inches
- Proposed setback is 6 feet 3 5/8th inches
- 1 foot 9 9/16th inch variation requested

*Variation to Exceed Maximum Lot Coverage

- Initially sought by Applicant
- Request withdrawn during Public Hearing

Site plans, photos, elevation drawings were presented for viewing.

ZBA Public Hearing

*Public Hearing

- Held on August 20, 2014

*Petitioner Testimony received from

- Mr. Tom Dallas, Property Owner
- Mr. Fred Chapekis, Attorney for Property Owner
- Mr. George Prosiliakos, Project Architect

*Public Testimony Received

- David and Silvia Hollander, 6715 North Navajo Ave
 - Opposed due to addition blocking light into their home
 - Concern over nearby landscape on their property dying

ZBA Deliberations

*Petitioner withdrew maximum lot coverage variation request

- Stated option to remove patio in rear yard to allow for proposed addition

*Side Yard Setback Variation

- Petitioner indicated request is to construct an accessible bedroom/bathroom on first floor of existing home
- Since 2008 Village has considered and approved 10 Side Yard Variation requests
 - All to allow proposed additions to match existing condition
 - Proposed variation differs since proposed addition extends into existing side yard
- Fall 2013 Village considered and approved request for an addition at 6643 Tower Circle Drive for the purpose of installing a residential elevator
 - Petitioner identified accessibility and mobility issues as reason for elevator and variation

ZBA Recommendation

*By 4 – 3 Vote, ZBA recommends approval of Side Yard Setback Variation

- Dissenting votes cast due to no demonstrated hardship
 - Commissioners indicated addition could be constructed in manner to address accessibility and mobility issues and comply with Zoning Code
 -

George Prosiliakos, architect for the petitioner, addressed the Board and responded to questions.

Discussion ensued. There were concerns expressed.

Trustee Leftakes moved to postpone this discussion until the Village Board meeting of October 7, 2014, when the full Board will be in attendance. Trustee Patel seconded the motion. The motion passed with a Voice Vote.

4. *This item was moved from the Consent Agenda for the purpose of discussion at the request of Trustee Patel.*

Approval of an Ordinance Waiving the Competitive Bidding Process and Accepting a Proposal from Christopher B. Burke Engineering Ltd for the Provision of Civil Engineering Services to Develop a Streetscape Concept Plan for the Devon-Lincoln Tax Increment Financing District in the Not-to-Exceed Amount of \$47,000.

Discussion ensued regarding valid options for the area and dealings with Chicago. Questions were posed with clarification by Mr. Wiberg who recommends revision of the Ordinance rather than approval.

Trustee Patel moved to refer this item to a future Committee of the Whole meeting, seconded by Trustee Cope.

The motion passed with a Voice Vote

Manager's Report

Mr. Wiberg spoke of the following items

- Pratt and Central – The work is proceeding nicely. Final project completion should be by the end of November 2014
- Crawford Avenue – Thanks to the residents for their patience, there have been few complaints. Crawford from Devon to Touhy will be closed on Saturday, September 20 from 7:30 AM to 3:30PM

Board and Commissions Report

Trustee Cope spoke of the September 11 Memorial Service. Congratulations to our fire and police departments on a job well done. He stated that the ceremony was excellent. He commended the Mayor for his presence at the event despite the physical challenges resulting from his accident. Mr. Cope complimented all participants including the District 74 students who participated.

Village Clerk's Report

Forms for request of absentee ballots are available from the Clerk's office. Email or call and we will mail one to you. Early voting from the November election will be October 20 to November 2. Remember there will be no early voting in Lincolnwood. The closest locations are Skokie Village Hall and Skokie Courthouse.

Trustees Report

None

Public Forum

None

Adjournment

At 8:40p.m. Trustee Patel moved to adjourn the Regular meeting of the Village Board, seconded by Trustee Leftakes.

The motion passed with a Voice Vote.

Respectfully Submitted,

Beryl Herman
Village Clerk

TO: President and the Board of Trustees

FROM: Timothy C. Wiberg, Village Manager

SUBJECT: Warrant Approval

DATE: October 3, 2014

The following are the totals for the List of Bills being presented at the October 7th Village Board meeting.

10/7/2014	299,003.32
10/7/2014	138,143.64
10/7/2014	97,436.35
10/7/2014	58,027.74
10/7/2014	24,059.24
10/7/2014	34,640.62
Total	<hr/> \$ 651,310.91

Accounts Payable

To Be Paid Proof List

User: jmazzeffi
 Printed: 09/29/2014 - 2:03PM
 Batch: 00100.10.2014



Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number					Description	Reference			
Amazon AMAZON									
106207584087	9/8/2014	124.99	0.00	10/07/2014	Office supplies - Docking station			False	0
101-250-511-6530 Equipment - data processing									
	106207584087 Total:	124.99							
18541259917	8/29/2014	27.94	0.00	10/07/2014	Office supplies - Floor mat			False	0
101-200-511-5700 Office supplies									
	18541259917 Total:	27.94							
255952729852	8/29/2014	372.19	0.00	10/07/2014	Office supplies - Adjustable desk top			False	0
101-200-511-5700 Office supplies									
	255952729852 Total:	372.19							
300590912785	9/8/2014	147.89	0.00	10/07/2014	Office supplies - docking station			False	0
101-250-511-6530 Equipment - data processing									
	300590912785 Total:	147.89							
300593657211	9/8/2014	46.99	0.00	10/07/2014	Office supplies - Monitor stand			False	0
101-200-511-5700 Office supplies									
	300593657211 Total:	46.99							
39562566749	8/11/2014	34.18	0.00	10/07/2014	Office supplies - Toner			False	0
101-200-511-5700 Office supplies									
	39562566749 Total:	34.18							
67784430858	8/19/2014	17.84	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
101-210-511-5700 Office supplies 67784430858	8/19/2014	14.43	0.00	10/07/2014	Office supplies - Adding machine tape			False	0
101-200-511-5700 Office supplies					Office supplies - Batteries and tester				
		<hr/>							
67784430858 Total:		32.27							
677886246427	8/19/2014	52.45	0.00	10/07/2014				False	0
101-200-511-5700 Office supplies					Office supplies - Monitor stand				
		<hr/>							
677886246427 Total:		52.45							
		<hr/>							
AMAZON Total:		838.90							
		<hr/>							
Amazon Total:		838.90							
American Express AMEREXP									
31083091814	9/18/2014	18.00	0.00	10/07/2014				False	0
101-350-512-5560 Printing & copying services					Fed Ex - Map lamination				
31083091814	9/18/2014	-0.90	0.00	10/07/2014				False	0
101-350-512-5560 Printing & copying services					Credit				
31083091814	9/18/2014	51.99	0.00	10/07/2014				False	0
101-350-512-5660 EMS supplies					Hand sanitizer				
31083091814	9/18/2014	24.95	0.00	10/07/2014				False	0
101-350-512-5620 Books & publications					Subscription				
31083091814	9/18/2014	33.45	0.00	10/07/2014				False	0
101-350-512-5660 EMS supplies					EMP - C Collr bag				
31083091814	9/18/2014	-19.44	0.00	10/07/2014				False	0
101-350-512-5660 EMS supplies					Credit				
		<hr/>							
31083091814 Total:		108.05							
31158081914	8/19/2014	109.89	0.00	10/07/2014				False	0
101-200-511-5599 Other contractual					Lifelock membership				
		<hr/>							
31158081914 Total:		109.89							
31158082214	8/22/2014	209.73	0.00	10/07/2014				False	0
101-200-511-5799 Other materials & supplies					Supplies - Employee appreciation event				

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	31158082214 Total:	209.73							
31158082714	8/27/2014	132.00	0.00	10/07/2014				False	0
101-200-511-5799	Other materials & supplies				SmartEvents- Entertainment for Employee appreciation eve				
31158082714	8/27/2014	100.00	0.00	10/07/2014				False	0
101-200-511-5799	Other materials & supplies				Gift cards for Employee appreciation event				
	31158082714 Total:	232.00							
31158090514	9/5/2014	167.85	0.00	10/07/2014				False	0
101-200-511-5830	Lodging				IPELRA lodging				
31158090514	9/5/2014	375.00	0.00	10/07/2014				False	0
101-200-511-5810	Conference & meeting registrat				IPELRA Conference				
	31158090514 Total:	542.85							
32008081114	8/11/2014	30.63	0.00	10/07/2014				False	0
101-200-511-5840	Meals				Dinner - HRC Meeting				
32008081114	8/11/2014	23.00	0.00	10/07/2014				False	0
101-200-511-5840	Meals				Dinner - HRC Meeting				
	32008081114 Total:	53.63							
32008082014	8/20/2014	42.90	0.00	10/07/2014				False	0
101-100-511-5799	Other materials & supplies				Get Well flowers for employee				
	32008082014 Total:	42.90							
32008082714	8/27/2014	82.51	0.00	10/07/2014				False	0
101-200-511-5840	Meals				Lunch prior to Stormwater meeting				
32008082714	8/27/2014	31.00	0.00	10/07/2014				False	0
101-200-511-5820	Local mileage, parking & tolls				Parking for MWRD Stormwater meeting				
	32008082714 Total:	113.51							
32008090414	9/4/2014	72.90	0.00	10/07/2014				False	0
101-100-511-5799	Other materials & supplies				Get Well flowers for Mayor				
	32008090414 Total:	72.90							
36140081214	8/12/2014	126.00	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
205-530-515-5270	Purchased program services				Rainbow Falls - camp trip				
	36140081214 Total:	126.00							
36140081414	8/14/2014	338.72	0.00	10/07/2014				False	0
205-504-515-5730	Program supplies				Promotions for Family Fun Fest				
	36140081414 Total:	338.72							
36140081514	8/15/2014	9.00	0.00	10/07/2014				False	0
205-530-515-5590	Training				Supplies for camp				
	36140081514 Total:	9.00							
36140081814	8/18/2014	199.00	0.00	10/07/2014				False	0
205-504-515-5730	Program supplies				New canopy				
36140081814	8/18/2014	468.00	0.00	10/07/2014				False	0
205-570-515-5270	Purchased program services				Show for seniors				
	36140081814 Total:	667.00							
36140081914	8/19/2014	107.00	0.00	10/07/2014				False	0
205-530-515-5270	Purchased program services				White sox camp trip				
	36140081914 Total:	107.00							
36140082014	8/20/2014	56.67	0.00	10/07/2014				False	0
205-571-515-5730	Program supplies				Ricoh				
36140082014	8/20/2014	65.17	0.00	10/07/2014				False	0
205-571-515-5730	Program supplies				Ricoh				
36140082014	8/20/2014	65.17	0.00	10/07/2014				False	0
205-571-515-5730	Program supplies				Ricoh				
36140082014	8/20/2014	5.96	0.00	10/07/2014				False	0
205-571-515-5535	Facility rental				Lowes				
	36140082014 Total:	192.97							
36140082114	8/21/2014	12.50	0.00	10/07/2014				False	0
205-500-515-5810	Conference & meeting registrat				Southwest upgrade				
36140082114	8/21/2014	12.50	0.00	10/07/2014				False	0
205-500-515-5810	Conference & meeting registrat				Southwest upgrade				
36140082114	8/21/2014	264.20	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
205-500-515-5810 Conference & meeting registrat				Conference trip					
36140082114	8/21/2014	390.00	0.00	10/07/2014				False	0
205-570-515-5270 Purchased program services				Trip for seniors					
36140082114	8/21/2014	8.44	0.00	10/07/2014				False	0
205-571-515-5535 Facility rental				Paint from Lowes					
36140082114	8/21/2014	536.00	0.00	10/07/2014				False	0
205-530-515-5270 Purchased program services				Camp trip					
	36140082114 Total:	1,223.64							
36140082714	8/27/2014	590.72	0.00	10/07/2014				False	0
205-504-515-5645 Concessions & food				Pumpkins for Boo bash					
	36140082714 Total:	590.72							
36140082814	8/28/2014	236.61	0.00	10/07/2014				False	0
205-504-515-5730 Program supplies				Items for Family Fun Fest					
36140082814	8/28/2014	70.08	0.00	10/07/2014				False	0
205-504-515-5730 Program supplies				Items for Family Fun Fest					
	36140082814 Total:	306.69							
36140083014	8/30/2014	125.97	0.00	10/07/2014				False	0
205-570-515-5270 Purchased program services				Arlington park deposit					
	36140083014 Total:	125.97							
36140090214	9/2/2014	46.05	0.00	10/07/2014				False	0
205-500-515-5725 Credit card charges				Plug n play					
36140090214	9/2/2014	150.00	0.00	10/07/2014				False	0
205-570-515-5270 Purchased program services				Transportation for seniors - To Art Institute					
36140090214	9/2/2014	150.00	0.00	10/07/2014				False	0
205-570-515-5270 Purchased program services				Transportation for seniors - Return from Art Institute					
	36140090214 Total:	346.05							
36140091714	9/17/2014	-14.56	0.00	10/07/2014				False	0
205-500-515-5820 Local mileage, parking & tolls				Credit - Tax refund					
36140091714	9/17/2014	60.00	0.00	10/07/2014				False	0
205-500-515-5820 Local mileage, parking & tolls				National Recreation and Parks renewal					
36140091714	9/17/2014	150.00	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
205-530-515-5270	Purchased program services				Camp trip - Tall Ship				
	36140091714 Total:	195.44							
	AMEREXP Total:	5,714.66							
	American Express Total:	5,714.66							
AT&T									
AT&T									
773R07163608	8/28/2014	77.39	0.00	10/07/2014				False	0
215-000-512-5580	Telephone				Telephone services - E911				
	773R07163608 Total:	77.39							
847734584008	9/7/2014	1,203.45	0.00	10/07/2014				False	0
215-000-512-5580	Telephone				Telephone services - E911				
	847734584008 Total:	1,203.45							
847734584308	9/7/2014	352.32	0.00	10/07/2014				False	0
215-000-512-5580	Telephone				Telephone services - E911				
	847734584308 Total:	352.32							
	AT&T Total:	1,633.16							
	AT&T Total:	1,633.16							
City of Chicago Dept of Water									
CTYOFCHI									
430883-430883	9/9/2014	100,588.80	0.00	10/07/2014				False	0
660-620-519-5790	Water purchases				Water - 7/11/14-8/11/14				
	430883-430883 Total:	100,588.80							
430884-430884	9/9/2014	87,965.60	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
660-620-519-5790	Water purchases			Water - 7/11/14-8/11/14					
	430884-430884 Total:	87,965.60							
	CTYOFCHI Total:	188,554.40							
	City of Chicago Dept of W	188,554.40							
Coca-Cola Bottling Company									
COCACOLA									
300019720	8/19/2014	150.00	0.00	10/07/2014				False	0
	101-210-511-5700 Office supplies			Annual rental fee for all Coca Cola machines					
	300019720 Total:	150.00							
338470511	9/8/2014	120.96	0.00	10/07/2014				False	0
	101-210-511-5700 Office supplies			Pop for Village Hall pop machine					
	338470511 Total:	120.96							
	COCACOLA Total:	270.96							
	Coca-Cola Bottling Compa	270.96							
Comcast Cable									
COMCA									
COM09112014	9/11/2014	97.85	0.00	10/07/2014				False	0
	660-610-519-5580 Telephone			Inernet service for Pump House - September					
COM09112014	9/11/2014	347.90	0.00	10/07/2014				False	0
	101-250-511-5580 Telephone			Inernet service for Village Hall - September					
	COM09112014 Total:	445.75							
	COMCA Total:	445.75							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Comcast Cable Total:	445.75							
Comcast Cable COMCAST COM090314	9/3/2014	70.00	0.00	10/07/2014				False	0
	660-610-519-5580 Telephone			Internet Service for Public Works - September					
	COM090314 Total:	70.00							
	COMCAST Total:	70.00							
	Comcast Cable Total:	70.00							
Commonwealth Edison COMED 0008132018	9/9/2014	909.34	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Electric - Kostner/Morse - 8/7-9/8					
	0008132018 Total:	909.34							
0104767008	9/10/2014	2,864.43	0.00	10/07/2014				False	0
	660-620-519-5785 Utilities - public way			Electric - Pump Station - 8/8-9/9					
	0104767008 Total:	2,864.43							
0592075011	8/27/2014	2,133.31	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Master Account/Street Lighting - 8/7-9/8					
	0592075011 Total:	2,133.31							
0933017059	9/9/2014	543.49	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Electric - 6754 Cicero - 8/7-9/8					
	0933017059 Total:	543.49							
142709057	9/9/2014	94.31	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Metered Street Lights - Karlov & Pratt					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	142709057 Total:	94.31							
1700394002	9/9/2014	80.50	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Electric - ES Crawford - 8/7-9/8					
	1700394002 Total:	80.50							
1784010001	9/9/2014	722.25	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Electric - Shelter - 8/7-9/8					
	1784010001 Total:	722.25							
1784059008	9/9/2014	1,836.20	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Electric - NS Morse - 8/7-9/8					
	1784059008 Total:	1,836.20							
1784346006	9/9/2014	381.21	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Electric - SS Touhy - 8/7-9/8					
	1784346006 Total:	381.21							
1784521009	9/8/2014	19.54	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Electric - SS Touhy - 8/7-9/8					
	1784521009 Total:	19.54							
2028043041	9/8/2014	4,053.77	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Master Account/Street Lighting - 8/7-9/5					
	2028043041 Total:	4,053.77							
2187009072	9/9/2014	395.07	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Electric - 7000 Mc Cormick - 8/7-9/8					
	2187009072 Total:	395.07							
2631087013	9/9/2014	97.51	0.00	10/07/2014				False	0
	101-440-513-5785 Utilities - public way			Electric - 6851 Central Park - 8/7-9/8					
	2631087013 Total:	97.51							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
2649157097	9/9/2014	154.39	0.00	10/07/2014				False	0
101-440-513-5785	Utilities - public way			Electric - 3550 Pratt - 8/5-9/8					
	2649157097 Total:	154.39							
3219170058	8/6/2014	67.42	0.00	10/07/2014				False	0
101-440-513-5785	Utilities - public way			Metered Street Lights - 4990 Pratt					
	3219170058 Total:	67.42							
3462712002	9/8/2014	116.43	0.00	10/07/2014				False	0
660-620-519-5785	Utilities - public way			Electric - Water Tower - 8/7-9/8					
	3462712002 Total:	116.43							
4147167024	9/10/2014	2,602.14	0.00	10/07/2014				False	0
101-440-513-5785	Utilities - public way			Electric - 7055 Kostner - 8/8-9/9					
	4147167024 Total:	2,602.14							
4413156059	9/10/2014	115.67	0.00	10/07/2014				False	0
101-440-513-5785	Utilities - public way			Electric - 7300 Cicero - 8/8-9/9					
	4413156059 Total:	115.67							
4791110064	9/10/2014	267.63	0.00	10/07/2014				False	0
101-440-513-5785	Utilities - public way			Electric - 3928 Touhy - 8/8-9/9					
	4791110064 Total:	267.63							
57221-35010	9/9/2014	17.07	0.00	10/07/2014				False	0
101-440-513-5785	Utilities - public way			Master Account/Street Lighting - 8/7-9/8					
	57221-35010 Total:	17.07							
	COMED Total:	17,471.68							
	Commonwealth Edison To	17,471.68							

Fedex

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
FEDEX									
2-776-17411	9/10/2014	23.30	0.00	10/07/2014				False	0
101-230-511-5399				Other professional services	Shipping - IFOP				
	2-776-17411 Total:	23.30							
804144716648	8/28/2014	20.15	0.00	10/07/2014				False	0
101-210-511-5720				Postage	Shipping - Legal				
	804144716648 Total:	20.15							
	FEDEX Total:	43.45							
	Fedex Total:	43.45							
FIA Card Services									
FIACARD									
0923081314	8/13/2014	92.13	0.00	10/07/2014				False	0
205-530-515-5730				Program supplies	Morrison Medical - ice packs				
	0923081314 Total:	92.13							
3695081314	8/13/2014	91.10	0.00	10/07/2014				False	0
205-500-515-5590				Training	Food for staff meeting				
	3695081314 Total:	91.10							
3695081414	8/14/2014	4.00	0.00	10/07/2014				False	0
205-530-515-5730				Program supplies	Camp supplies				
3695081414	8/14/2014	46.39	0.00	10/07/2014				False	0
205-530-515-5730				Program supplies	Food for seniors				
3695081414	8/14/2014	35.94	0.00	10/07/2014				False	0
205-530-515-5730				Program supplies	Camp supplies				
	3695081414 Total:	86.33							
3695081814	8/18/2014	22.85	0.00	10/07/2014				False	0
205-520-515-5799				Other materials & supplies	Food for Club Kid training				
3695081814	8/18/2014	27.46	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
205-520-515-5799	Other materials & supplies				Food for Club Kid training				
	3695081814 Total:	50.31							
7393082714	8/27/2014	595.42	0.00	10/07/2014				False	0
101-200-511-5799	Other materials & supplies				Supplies - Employee Appreciation event				
	7393082714 Total:	595.42							
7588071314	7/13/2014	149.00	0.00	10/07/2014				False	0
205-530-515-5270	Purchased program services				Chicago White Sox - camp trip				
	7588071314 Total:	149.00							
7588080714	8/7/2014	14.23	0.00	10/07/2014				False	0
205-530-515-5730	Program supplies				Walgreens - Camp supplies				
	7588080714 Total:	14.23							
7588083014	8/30/2014	7.89	0.00	10/07/2014				False	0
205-571-515-5730	Program supplies				Amazon - Community Center Supplies				
7588083014	8/30/2014	3.98	0.00	10/07/2014				False	0
205-571-515-5730	Program supplies				Amazon - Community Center Supplies				
	7588083014 Total:	11.87							
7588090514	9/5/2014	6.97	0.00	10/07/2014				False	0
205-571-515-5730	Program supplies				Amazon - Community Center Supplies				
	7588090514 Total:	6.97							
7588090814	9/8/2014	364.19	0.00	10/07/2014				False	0
101-300-512-5700	Office supplies				Office supplies				
7588090814	9/8/2014	99.99	0.00	10/07/2014				False	0
101-300-512-5700	Office supplies				Dry erase board				
7588090814	9/8/2014	401.90	0.00	10/07/2014				False	0
101-300-512-5730	Program supplies				Frame for LPD photo				
7588090814	9/8/2014	695.00	0.00	10/07/2014				False	0
101-300-512-5730	Program supplies				Printer for BEAST - property inventory				
7588090814	9/8/2014	21.98	0.00	10/07/2014				False	0
101-300-512-5730	Program supplies				Refreshments for training class				
7588090814	9/8/2014	36.40	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
101-300-512-5730	Program supplies				Lunch with Fire & Police Comm.				
	7588090814 Total:	1,619.46							
8334082014	8/20/2014	40.00	0.00	10/07/2014				False	0
101-210-511-5850	Purchased transportation				I pass auto replenish				
	8334082014 Total:	40.00							
	FIACARD Total:	2,756.82							
	FIA Card Services Total:	2,756.82							
GFOA									
GFOACAP									
090814	9/8/2014	435.00	0.00	10/07/2014				False	0
101-210-511-5730	Program supplies				GFOA excellence in financial reporting award appl fee				
	090814 Total:	435.00							
	GFOACAP Total:	435.00							
	GFOA Total:	435.00							
Groot Recycling & Waste Services									
GROOT									
10385441	8/29/2014	1,914.99	0.00	10/07/2014				False	0
101-440-514-5230	Garbage & recycling				22280-002/Public Works				
	10385441 Total:	1,914.99							
10385525	9/1/2014	53,678.78	0.00	10/07/2014				False	0
101-440-514-5230	Garbage & recycling				1229-001/Community pick up				
	10385525 Total:	53,678.78							
10385526	9/1/2014	683.47	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
101-440-514-5230	Garbage & recycling			1230-001/School District 74					
	10385526 Total:	683.47							
10413201	8/22/2014	2,432.09	0.00	10/07/2014				False	0
101-440-514-5230	Garbage & recycling			22280-001/Public Works					
	10413201 Total:	2,432.09							
10428539	9/1/2014	2,967.23	0.00	10/07/2014				False	0
101-440-514-5230	Garbage & recycling			1231-001/Multi family pick up					
	10428539 Total:	2,967.23							
	GROOT Total:	61,676.56							
	Groot Recycling & Waste S	61,676.56							
Lowe's Business Acc/GECE									
LOWES									
000361	8/22/2014	5.65	0.00	10/07/2014				False	0
205-571-515-5535	Facility rental			Community Center - paint supplies					
	000361 Total:	5.65							
000371	8/25/2014	12.59	0.00	10/07/2014				False	0
205-571-515-5535	Facility rental			Community Center - paint supplies					
	000371 Total:	12.59							
000373	8/27/2014	12.32	0.00	10/07/2014				False	0
205-571-515-5535	Facility rental			Community Center - grill supplies					
	000373 Total:	12.32							
000377	8/27/2014	16.93	0.00	10/07/2014				False	0
205-571-515-5535	Facility rental			Community Center - grill supplies					
	000377 Total:	16.93							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
000499	8/25/2014	140.60	0.00	10/07/2014				False	0
205-571-515-5535				Community Center - paint supplies					
	000499 Total:	140.60							
07156	9/9/2014	18.88	0.00	10/07/2014				False	0
101-350-512-5675				Bolts, washers, buckets, power strip					
07156	9/9/2014	68.83	0.00	10/07/2014				False	0
101-350-512-5799				Bolts, washers, buckets, power strip					
	07156 Total:	87.71							
1279	9/10/2014	183.25	0.00	10/07/2014				False	0
205-560-515-5630				Cleaning supplies for pool					
	1279 Total:	183.25							
1285529	9/9/2014	16.13	0.00	10/07/2014				False	0
101-300-512-5730				Outlet for digital reader for evidence technicians					
	1285529 Total:	16.13							
2193	9/10/2014	52.38	0.00	10/07/2014				False	0
205-560-515-5405				Duct tape for pool					
	2193 Total:	52.38							
2453	9/12/2014	8.15	0.00	10/07/2014				False	0
205-560-515-5630				Pipe fitting for pool					
	2453 Total:	8.15							
	LOWES Total:	535.71							
	Lowe's Business Acc/GEC	535.71							
Nicor Gas NICOR 1436840000	9/15/2014	259.33	0.00	10/07/2014				False	0
205-430-515-5780				Community Center					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	1436840000 Total:	259.33							
21-46-84-00003	8/18/2014	221.15	0.00	10/07/2014				False	0
	205-560-515-5780 Utilities - government buildin				Pool - 7/11-8/13				
	21-46-84-00003 Total:	221.15							
21-84-84-00004	9/9/2014	67.18	0.00	10/07/2014				False	0
	660-620-519-5780 Utilities - government buildin				Pump Station - 8/8-9/8				
	21-84-84-00004 Total:	67.18							
3017240000	9/12/2014	177.92	0.00	10/07/2014				False	0
	101-420-511-5780 Utilities - government buildin				Public Services				
	3017240000 Total:	177.92							
5202340000	9/11/2014	261.52	0.00	10/07/2014				False	0
	101-420-511-5780 Utilities - government buildin				Village Hall				
	5202340000 Total:	261.52							
6202340000	9/11/2014	1,091.94	0.00	10/07/2014				False	0
	101-420-511-5780 Utilities - government buildin				Public Safety				
	6202340000 Total:	1,091.94							
70-61-47-04487	9/10/2014	108.37	0.00	10/07/2014				False	0
	205-560-515-5780 Utilities - government buildin				7055 Kostner - 8/8-9/10				
	70-61-47-04487 Total:	108.37							
	NICOR Total:	2,187.41							
	Nicor Gas Total:	2,187.41							
Palatine Oil, Co, Inc. PALAT 7045782	9/3/2014	279.93	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
101-350-512-5670 Fuel				Fuel usage					
7045782	9/3/2014	162.61	0.00	10/07/2014				False	0
101-400-511-5670 Fuel				Fuel usage					
7045782	9/3/2014	153.68	0.00	10/07/2014				False	0
101-400-511-5670 Fuel				101410					
7045782	9/3/2014	691.74	0.00	10/07/2014				False	0
101-440-513-5670 Fuel				101410					
7045782	9/3/2014	530.78	0.00	10/07/2014				False	0
205-430-515-5670 Fuel				101410					
7045782	9/3/2014	727.11	0.00	10/07/2014				False	0
660-620-519-5670 Fuel				101410					
7045782	9/3/2014	122.62	0.00	10/07/2014				False	0
101-420-511-5670 Fuel				Fuel usage					
7045782	9/3/2014	3,565.44	0.00	10/07/2014				False	0
101-300-512-5670 Fuel				Fuel usage					
	7045782 Total:	6,233.91							
7045783	9/3/2014	1,106.36	0.00	10/07/2014				False	0
101-350-512-5670 Fuel				Fuel usage					
7045783	9/3/2014	1,817.28	0.00	10/07/2014				False	0
101-440-513-5670 Fuel				Fuel usage					
7045783	9/3/2014	133.92	0.00	10/07/2014				False	0
205-430-515-5670 Fuel				Fuel usage					
7045783	9/3/2014	541.66	0.00	10/07/2014				False	0
660-620-519-5670 Fuel				Fuel usage					
	7045783 Total:	3,599.22							
7046074	9/12/2014	525.58	0.00	10/07/2014				False	0
101-350-512-5670 Fuel				Fuel usage					
7046074	9/12/2014	642.04	0.00	10/07/2014				False	0
101-440-513-5670 Fuel				Fuel usage					
7046074	9/12/2014	245.40	0.00	10/07/2014				False	0
205-430-515-5670 Fuel				Fuel usage					
7046074	9/12/2014	181.10	0.00	10/07/2014				False	0
660-620-519-5670 Fuel				Fuel usage					
	7046074 Total:	1,594.12							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
7046075	9/12/2014	75.90	0.00	10/07/2014				False	0
101-420-511-5670 Fuel				Fuel usage					
7046075	9/12/2014	1,300.98	0.00	10/07/2014				False	0
101-300-512-5670 Fuel				Fuel usage					
7046075	9/12/2014	75.64	0.00	10/07/2014				False	0
101-350-512-5670 Fuel				Fuel usage					
7046075	9/12/2014	74.37	0.00	10/07/2014				False	0
101-400-511-5670 Fuel				Fuel usage					
7046075	9/12/2014	260.30	0.00	10/07/2014				False	0
101-440-513-5670 Fuel				Fuel usage					
7046075	9/12/2014	278.63	0.00	10/07/2014				False	0
205-430-515-5670 Fuel				Fuel usage					
7046075	9/12/2014	419.99	0.00	10/07/2014				False	0
660-620-519-5670 Fuel				Fuel usage					
	7046075 Total:	2,485.81							
	PALAT Total:	13,913.06							
	Palatine Oil, Co, Inc. Total	13,913.06							
Sam's Club SAMSCL 91014	9/10/2014	126.20	0.00	10/07/2014				False	0
205-520-515-5645 Concessions & food				Snacks - Club Kid					
	91014 Total:	126.20							
	SAMSCL Total:	126.20							
	Sam's Club Total:	126.20							
Sun-Times Media/Pioneer Press PIONEPRS 252584	8/31/2014	150.00	0.00	10/07/2014				False	0
205-504-515-5510 Advertising				1/2 page ad - promote Touch a Truck					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	252584 Total:	150.00							
791614	6/12/2014	150.00	0.00	10/07/2014				False	0
205-504-515-5510	Advertising			1/2 page ad in Summer Fun Guide					
	791614 Total:	150.00							
	PIONEPRS Total:	300.00							
	Sun-Times Media/Pioneer	300.00							
The Fun Ones									
THEFUN									
41234	8/20/2014	415.00	0.00	10/07/2014				False	0
205-504-515-5270	Purchased program services			Family Fun Fest 2014 - Moonwalk					
	41234 Total:	415.00							
	THEFUN Total:	415.00							
	The Fun Ones Total:	415.00							
United States Postal Service									
USPOSTAL									
PB090814	9/8/2014	10.44	0.00	10/07/2014				False	0
101-210-511-5720	Postage			Pitney Bowes postage					
PB090814	9/8/2014	5.54	0.00	10/07/2014				False	0
101-210-511-5720	Postage			Pitney Bowes postage					
PB090814	9/8/2014	28.80	0.00	10/07/2014				False	0
101-210-511-5720	Postage			Pitney Bowes postage					
PB090814	9/8/2014	45.12	0.00	10/07/2014				False	0
101-210-511-5720	Postage			Pitney Bowes postage					
PB090814	9/8/2014	11.94	0.00	10/07/2014				False	0
101-210-511-5720	Postage			Pitney Bowes postage					
PB090814	9/8/2014	60.21	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
205-500-515-5720 Postage PB090814	9/8/2014	55.68	0.00	10/07/2014	Pitney Bowes postage			False	0
205-500-515-5720 Postage PB090814	9/8/2014	9.85	0.00	10/07/2014	Pitney Bowes postage			False	0
101-210-511-5720 Postage PB090814	9/8/2014	153.12	0.00	10/07/2014	Pitney Bowes postage			False	0
101-210-511-5720 Postage PB090814	9/8/2014	427.68	0.00	10/07/2014	Pitney Bowes postage			False	0
660-610-519-5720 Postage					Pitney Bowes postage				
	PB090814 Total:	808.38							
	USPOSTAL Total:	808.38							
	United States Postal Servic	808.38							
UPS UPS 48TT5364	9/6/2014	14.61	0.00	10/07/2014				False	0
101-350-512-5720 Postage					Delivery fee				
	48TT5364 Total:	14.61							
	UPS Total:	14.61							
	UPS Total:	14.61							
Village of Lincolnwood VOL PB091514	9/15/2014	40.41	0.00	10/07/2014				False	0
205-530-515-5730 Program supplies PB091514	9/15/2014	226.31	0.00	10/07/2014	Petty Cash Re-imbursement			False	0
101-200-511-5799 Other materials & supplies PB091514	9/15/2014	175.00	0.00	10/07/2014	Petty Cash Re-imbursement			False	0
205-500-515-5840 Meals PB091514	9/15/2014	25.05	0.00	10/07/2014	Petty Cash Re-imbursement			False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
101-240-517-5700 Office supplies				Petty Cash Re-imburement					
PB091514	9/15/2014	17.39	0.00	10/07/2014				False	0
205-530-515-5590 Training				Petty Cash Re-imburement					
PB091514	9/15/2014	31.50	0.00	10/07/2014				False	0
101-200-511-5840 Meals				Petty Cash Re-imburement					
PB091514	9/15/2014	88.00	0.00	10/07/2014				False	0
205-563-515-5649 Supplies				Petty Cash Re-imburement					
PB091514	9/15/2014	22.00	0.00	10/07/2014				False	0
101-240-517-5830 Lodging				Petty Cash Re-imburement					
PB091514	9/15/2014	21.98	0.00	10/07/2014				False	0
101-240-517-5799 Other materials & supplies				Petty Cash Re-imburement					
PB091514	9/15/2014	43.97	0.00	10/07/2014				False	0
101-210-511-5840 Meals				Petty Cash Re-imburement					
PB091514	9/15/2014	100.00	0.00	10/07/2014				False	0
205-500-515-5820 Local mileage, parking & tolls				Petty Cash Re-imburement					
	PB091514 Total:	791.61							
	VOL Total:	791.61							
	Village of Lincolnwood To	791.61							
	Report Total:	299,003.32							

Accounts Payable

To Be Paid Proof List

User: jmazzeffi
 Printed: 09/29/2014 - 2:03PM
 Batch: 00101.10.2014



Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description	Reference				
Advocate Occupational Health									
ADVOCA									
539417	8/4/2014	330.00	0.00	10/07/2014				False	0
101-200-511-5599 Other contractual				Drug screening					
	539417 Total:	330.00							
541439	8/26/2014	86.00	0.00	10/07/2014				False	0
101-200-511-5599 Other contractual				Drug screening					
	541439 Total:	86.00							
	ADVOCA Total:	416.00							
	Advocate Occupational He	416.00							
Airgas									
AIRGAS									
9921381938	8/31/2014	158.80	0.00	10/07/2014				False	0
101-350-512-5660 EMS supplies				Oxygen cylinders for ambulances					
	9921381938 Total:	158.80							
	AIRGAS Total:	158.80							
	Airgas Total:	158.80							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
Alvira, Diana ALVIRA 090914	9/9/2014	150.00	0.00	10/07/2014				False	0
205-000-210-2430 Parks and Recs Control Deposi				Refund - Computer Explorers					
090914 Total:		150.00							
ALVIRA Total:		150.00							
Alvira, Diana Total:		150.00							
ARRP Trucking & Hauling Inc ARRP 7482	8/29/2014	1,285.00	0.00	10/07/2014				False	0
660-620-519-5599 Other contractual				5 loads dirt hauled out					
7482 Total:		1,285.00							
ARRP Total:		1,285.00							
ARRP Trucking & Hauling		1,285.00							
Avichal, Smita AVICHAL 091214	9/12/2014	475.00	0.00	10/07/2014				False	0
205-000-210-2430 Parks and Recs Control Deposi				Refund - Community Center Rental					
091214 Total:		475.00							
AVICHAL Total:		475.00							
Avichal, Smita Total:		475.00							

Business Only Broadband

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
BUSONLY									
52018	10/1/2014	250.00	0.00	10/07/2014				False	0
101-250-511-5580 Telephone				Back up connection-Internet access - October					
	52018 Total:	250.00							
	BUSONLY Total:	250.00							
	Business Only Broadband	250.00							
CDW Government									
CDWGOV									
PG16469	9/5/2014	1,395.00	0.00	10/07/2014				False	0
101-250-511-5640 Computer supplies				New UPS for Village Hall Server room					
	PG16469 Total:	1,395.00							
PG17589	9/5/2014	16.14	0.00	10/07/2014				False	0
101-200-511-5799 Other materials & supplies				CAT 5 cable for Council Chambers					
	PG17589 Total:	16.14							
PG18241	9/5/2014	162.92	0.00	10/07/2014				False	0
101-250-511-6530 Equipment - data processing				UPS for Key FOB System					
	PG18241 Total:	162.92							
	CDWGOV Total:	1,574.06							
	CDW Government Total:	1,574.06							
Chicago Communications, LLC									
CHGOCOMM									
263899	9/3/2014	24.03	0.00	10/07/2014				False	0
101-400-511-5410 R&M - communications equipm				C.C.S Maintenance - October 2014					
263899	9/3/2014	27.06	0.00	10/07/2014				False	0
101-410-511-5410 R&M - communications equipm				C.C.S Maintenance - October 2014					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
263899	9/3/2014	34.59	0.00	10/07/2014				False	0
205-430-515-5410 R&M - communications equipm				C.C.S Maintenance - October 2014					
263899	9/3/2014	39.59	0.00	10/07/2014				False	0
660-620-519-5410 R&M - communications equipm				C.C.S Maintenance - October 2014					
263899	9/3/2014	50.08	0.00	10/07/2014				False	0
101-440-513-5410 R&M - communications equipm				C.C.S Maintenance - October 2014					
263899 Total:		175.35							
CHGOCOMM Total:		175.35							
Chicago Communications,		175.35							
Christopher Burke Engineering									
CHRISTB									
118012	9/9/2014	4,500.00	0.00	10/07/2014				False	0
101-290-511-5920 Administration Engineer Costs				Retainer - August					
118012	9/9/2014	4,500.00	0.00	10/07/2014				False	0
660-620-519-5399 Other professional services				Retainer - August					
118012 Total:		9,000.00							
118013	9/9/2014	4,747.11	0.00	10/07/2014				False	0
660-620-519-5320 Consulting				Review of Stormwater Modeling Program					
118013 Total:		4,747.11							
118024	9/9/2014	1,450.00	0.00	10/07/2014				False	0
101-290-511-5922 Building Engineering Costs				Business District Legal Description					
118024 Total:		1,450.00							
CHRISTB Total:		15,197.11							
Christopher Burke Enginee		15,197.11							

Clark Baird Smith, LLP

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
CLARKBAI									
4931	9/4/2014	157.50	0.00	10/07/2014				False	0
101-230-511-5399	Other professional services			Legal services for personnel matters					
	4931 Total:	157.50							
	CLARKBAI Total:	157.50							
	Clark Baird Smith, LLP To	157.50							
Currie Motors									
CURRIEMO									
187490	9/5/2014	35,450.00	0.00	10/07/2014				False	0
205-430-561-6599	Equipment - other			Purchase 2015 Ford F350					
	187490 Total:	35,450.00							
187491	9/5/2014	35,937.00	0.00	10/07/2014				False	0
660-620-562-6580	Equipment - vehicles			Purchase 2015 Ford F350					
	187491 Total:	35,937.00							
	CURRIEMO Total:	71,387.00							
	Currie Motors Total:	71,387.00							
Daly, Kevin									
DALYK									
090814	9/8/2014	150.00	0.00	10/07/2014				False	0
205-000-210-2430	Parks and Recs Control Deposi			Refund - Computer Explorers					
	090814 Total:	150.00							
	DALYK Total:	150.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Daly, Kevin Total:	150.00							
Dick, Carrie									
DICKC									
091114	9/11/2014	60.00	0.00	10/07/2014				False	0
205-000-210-2430	Parks and Recs Control Deposi			Refund - Trot Training					
	091114 Total:	60.00							
	DICKC Total:	60.00							
	Dick, Carrie Total:	60.00							
D'Original Juzz Dance Group									
DORIGINA									
932014	9/3/2014	738.50	0.00	10/07/2014				False	0
205-504-515-5270	Purchased program services			8/1 Drop In					
932014	9/3/2014	602.00	0.00	10/07/2014				False	0
205-504-515-5270	Purchased program services			8/8 Drop In					
932014	9/3/2014	441.00	0.00	10/07/2014				False	0
205-504-515-5270	Purchased program services			8/15 Drop In					
932014	9/3/2014	336.00	0.00	10/07/2014				False	0
205-504-515-5270	Purchased program services			8/22 Drop In					
932014	9/3/2014	532.00	0.00	10/07/2014				False	0
205-504-515-5270	Purchased program services			8/29 Drop In					
	932014 Total:	2,649.50							
	DORIGINA Total:	2,649.50							
	D'Original Juzz Dance Gro	2,649.50							

Essential Equipment Solutions
ESSEN

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
3001	9/4/2014	180.22	0.00	10/07/2014				False	0
101-350-512-5665	Firefighting supplies			Hi - Vision helmet					
	3001 Total:	180.22							
	ESSEN Total:	180.22							
	Essential Equipment Soluti	180.22							
Eterno, Attorney at Law, David									
ETERNO									
11436	9/4/2014	975.00	0.00	10/07/2014				False	0
101-230-511-5399	Other professional services			Adjudication Hearing Officer - August					
	11436 Total:	975.00							
	ETERNO Total:	975.00							
	Eterno, Attorney at Law, D	975.00							
Fast Signs									
FASTSIGN									
80-41301	9/10/2014	50.00	0.00	10/07/2014				False	0
205-430-515-6350	Park Construction & Improvem			Park signs					
	80-41301 Total:	50.00							
	FASTSIGN Total:	50.00							
	Fast Signs Total:	50.00							
Galls Incorporated									
GALLS									
002364778	8/28/2014	62.00	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
101-300-512-5070	Uniform allowance			Misc uniform items					
	002364778 Total:	62.00							
002368671	8/29/2014	78.93	0.00	10/07/2014				False	0
101-300-512-5070	Uniform allowance			Misc uniform items					
	002368671 Total:	78.93							
	GALLS Total:	140.93							
	Galls Incorporated Total:	140.93							
Garcia, Edgar									
GARCIAE									
091114	9/11/2014	62.00	0.00	10/07/2014				False	0
205-000-210-2430	Parks and Recs Control Deposi			Refund - Kids Track					
	091114 Total:	62.00							
	GARCIAE Total:	62.00							
	Garcia, Edgar Total:	62.00							
Gewalt Hamilton Associates Inc									
GEWALT									
4700.004-5	9/3/2014	2,048.00	0.00	10/07/2014				False	0
660-620-519-5320	Consulting			LWD Purple Hotel storm water DSN					
	4700.004-5 Total:	2,048.00							
4700.006-3	9/3/2014	6,732.00	0.00	10/07/2014				False	0
213-000-561-5340	Engineering			LWD Crawford Ave Water Main observation					
	4700.006-3 Total:	6,732.00							
9232.355-13	9/5/2014	615.50	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
213-000-561-5340 Engineering					Pratt Street Light Construction				
	9232.355-13 Total:	615.50							
9232.376-3	9/5/2014	180.00	0.00	10/07/2014				False	0
213-000-561-5340 Engineering					Crawford Street Light Construction				
	9232.376-3 Total:	180.00							
9232.410-2	9/5/2014	11,397.64	0.00	10/07/2014				False	0
212-000-511-5320 Consulting					Pratt and Central Construction				
	9232.410-2 Total:	11,397.64							
	GEWALT Total:	20,973.14							
	Gewalt Hamilton Associate	20,973.14							
Global Emergency Products Inc GLOBALEN AG41329	9/8/2014	862.72	0.00	10/07/2014				False	0
101-350-512-5740 Repair parts					Fuel tank, straps, fittings				
	AG41329 Total:	862.72							
	GLOBALEN Total:	862.72							
	Global Emergency Product	862.72							
Grainger GRAINGER 9522343004	8/20/2014	542.88	0.00	10/07/2014				False	0
101-420-511-5405 R&M - buildings					Light bulbs for Proesel Park				
	9522343004 Total:	542.88							
9532886315	9/3/2014	73.80	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
101-420-511-5405 R&M - buildings				Toilet paper dispenser for PD					
	9532886315 Total:	73.80							
9832886323	9/3/2014	355.01	0.00	10/07/2014				False	0
101-420-511-5405 R&M - buildings				Ballast for PD					
	9832886323 Total:	355.01							
	GRAINGER Total:	971.69							
	Grainger Total:	971.69							
Hagg Press									
HAGG									
56620	9/8/2014	1,617.15	0.00	10/07/2014				False	0
101-100-511-5565 Village Newsletter				Connections newsletter - Sept/Oct 2014					
	56620 Total:	1,617.15							
	HAGG Total:	1,617.15							
	Hagg Press Total:	1,617.15							
HD Supply Waterworks, LTD.									
HDSUPPLY									
C803422	8/29/2014	469.62	0.00	10/07/2014				False	0
660-620-519-5745 Small tools				Meter box frame					
	C803422 Total:	469.62							
C869903	8/29/2014	660.00	0.00	10/07/2014				False	0
660-620-519-5745 Small tools				Meter box frame					
	C869903 Total:	660.00							
C869975	8/29/2014	789.80	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
660-620-519-5745	Small tools				Meter valves				
	C869975 Total:	789.80							
	HDSUPPLY Total:	1,919.42							
	HD Supply Waterworks, LT	1,919.42							
Hoving Pit Stop									
HOVING									
93374	9/4/2014	90.00	0.00	10/07/2014				False	0
205-430-515-5730	Program supplies				Portable toilet at Centennial - August				
	93374 Total:	90.00							
	HOVING Total:	90.00							
	Hoving Pit Stop Total:	90.00							
Impact Networking, LLC									
IMPACT									
401321	8/14/2014	19.50	0.00	10/07/2014				False	0
205-500-515-5440	R&M - office equipment				Toner - Parks Dept - Shipping				
	401321 Total:	19.50							
407682	9/4/2014	109.62	0.00	10/07/2014				False	0
660-610-519-5340	Maintenance Agreement Expen				Copier - PW - September				
407682	9/4/2014	109.62	0.00	10/07/2014				False	0
205-500-515-5440	R&M - office equipment				Copier - Parks - September				
407682	9/4/2014	109.62	0.00	10/07/2014				False	0
101-000-210-2650	Contractor Permits Payable				Copier - Fire - September				
407682	9/4/2014	109.62	0.00	10/07/2014				False	0
101-210-511-5440	R&M - office equipment				Copier - Police - September				
407682	9/4/2014	109.61	0.00	10/07/2014				False	0
101-210-511-5440	R&M - office equipment				Copier - Admin - September				

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
407682	9/4/2014	109.62	0.00	10/07/2014				False	0
660-610-519-5340	Maintenance Agreement Expen			Copier - Finance - September					
	407682 Total:	657.71							
	IMPACT Total:	677.21							
	Impact Networking, LLC T	677.21							
Kostopoulos, Antonia									
KOSTOP									
090814	9/8/2014	150.00	0.00	10/07/2014				False	0
205-000-210-2430	Parks and Recs Control Deposi			Refund - Computer Explorers					
	090814 Total:	150.00							
090914	9/9/2014	62.00	0.00	10/07/2014				False	0
205-000-210-2430	Parks and Recs Control Deposi			Refund - Kids Track					
	090914 Total:	62.00							
	KOSTOP Total:	212.00							
	Kostopoulos, Antonia Tota	212.00							
Lee Auto Parts									
LEEAUTOP									
442-286200	9/5/2014	153.19	0.00	10/07/2014				False	0
101-300-512-5480	R&M - vehicles			Alternator, mini lamp for PD					
	442-286200 Total:	153.19							
442-286226	9/5/2014	68.88	0.00	10/07/2014				False	0
101-300-512-5480	R&M - vehicles			Fuel tank, gas tank for PD					
	442-286226 Total:	68.88							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	LEEAUTOP Total:	222.07							
	Lee Auto Parts Total:	222.07							
Lund Industries									
LUNDIND									
80273	8/29/2014	125.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Install Docking stations in Squads					
	80273 Total:	125.00							
80274	8/29/2014	125.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Install Docking stations in Squads					
	80274 Total:	125.00							
80275	8/29/2014	39.90	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Custom extension cables					
80275	8/29/2014	75.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Replace bad GPS antenna					
	80275 Total:	114.90							
	LUNDIND Total:	364.90							
	Lund Industries Total:	364.90							
Lurvey Landscape Supply									
LURVEY									
T1-10052545	5/8/2014	95.60	0.00	10/07/2014				False	0
	101-440-513-5680 Landscaping supplies			Grass					
	T1-10052545 Total:	95.60							
T1-10054635	5/16/2014	1,279.00	0.00	10/07/2014				False	0
	101-420-511-5680 Landscaping supplies			Plants for Village Hall					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	T1-10054635 Total:	1,279.00							
T1-10074658	5/16/2014	-117.60	0.00	10/07/2014				False	0
101-420-511-5680	Landscaping supplies			Plants for Village Hall					
	T1-10074658 Total:	-117.60							
T1-10074659	8/5/2014	630.00	0.00	10/07/2014				False	0
101-420-511-5680	Landscaping supplies			Bushes for Village Hall					
	T1-10074659 Total:	630.00							
T1-10075705	9/4/2014	119.50	0.00	10/07/2014				False	0
101-440-513-5680	Landscaping supplies			Grass					
	T1-10075705 Total:	119.50							
T1-10079554	9/4/2014	23.90	0.00	10/07/2014				False	0
101-440-513-5680	Landscaping supplies			Grass					
	T1-10079554 Total:	23.90							
	LURVEY Total:	2,030.40							
	Lurvey Landscape Supply	2,030.40							
Menini Cartage Inc									
MENICRT									
43787	9/3/2014	1,401.79	0.00	10/07/2014				False	0
660-620-519-5760	Street materials - Aggregate			Gravel to fill trenches due to water main breaks					
	43787 Total:	1,401.79							
	MENICRT Total:	1,401.79							
	Menini Cartage Inc Total:	1,401.79							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
MGP, Inc.									
MGPINC									
2306	8/31/2014	871.75	0.00	10/07/2014				False	0
101-250-511-5599 Other contractual				GISC Staffing - August					
2306	8/31/2014	871.75	0.00	10/07/2014				False	0
101-000-210-2650 Contractor Permits Payable				GISC Staffing - August					
2306	8/31/2014	1,746.50	0.00	10/07/2014				False	0
660-620-519-5599 Other contractual				GISC Staffing - August					
	2306 Total:	3,490.00							
	MGPINC Total:	3,490.00							
	MGP, Inc. Total:	3,490.00							
Morton Grove Automotive West									
MORTONG									
54385	9/8/2014	85.00	0.00	10/07/2014				False	0
101-440-513-5480 R&M - vehicles				Rebuild alternator for chipper					
	54385 Total:	85.00							
	MORTONG Total:	85.00							
	Morton Grove Automotive	85.00							
North Shore Uniform									
NSHRUFM									
14-1237	6/13/2014	375.35	0.00	10/07/2014				False	0
101-410-511-5070 Uniform allowance				Clothing allowance					
	14-1237 Total:	375.35							
	NSHRUFM Total:	375.35							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	North Shore Uniform Total	375.35							
Northern Illinois Police Alarm System NTILPALS									
9757	8/28/2014	15.00	0.00	10/07/2014				False	0
101-300-512-5580 Telephone				Language Line - Spanish					
	9757 Total:	15.00							
	NTILPALS Total:	15.00							
	Northern Illinois Police Al	15.00							
Ontaneda, Patricia ONTANEDA									
091114	9/11/2014	124.00	0.00	10/07/2014				False	0
205-000-210-2430 Parks and Recs Control Deposi				Refund - Classes					
	091114 Total:	124.00							
	ONTANEDA Total:	124.00							
	Ontaneda, Patricia Total:	124.00							
Orange Crush LLC ORANGCRH									
465900	8/21/2014	379.90	0.00	10/07/2014				False	0
213-000-561-5490 R&M Road Repairs				Mod Surface					
	465900 Total:	379.90							
466031	8/22/2014	182.50	0.00	10/07/2014				False	0
213-000-561-5490 R&M Road Repairs				Mod Surface					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	466031 Total:	182.50							
466127	8/25/2014	169.26	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod Surface					
	466127 Total:	169.26							
466354	8/26/2014	235.48	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod Surface					
	466354 Total:	235.48							
466642	8/28/2014	313.78	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod Surface					
	466642 Total:	313.78							
467146	9/4/2014	657.85	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod Surface					
	467146 Total:	657.85							
467269	9/4/2014	663.16	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod Surface					
	467269 Total:	663.16							
	ORANGCRH Total:	2,601.93							
	Orange Crush LLC Total:	2,601.93							
Ozkaymak, Ella									
OZKAYMAK									
090814	9/8/2014	210.00	0.00	10/07/2014				False	0
	205-000-210-2430 Parks and Recs Control Deposi			Refund - Tennis					
	090814 Total:	210.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	OZKAYMAK Total:	210.00							
	Ozkaymak, Ella Total:	210.00							
Pitney Bowes Inc. PITBOWIN 665773	9/11/2014	254.37	0.00	10/07/2014				False	0
	101-210-511-5700 Office supplies			2 ink cartridges					
	665773 Total:	254.37							
	PITBOWIN Total:	254.37							
	Pitney Bowes Inc. Total:	254.37							
Rizzotti, Brigita RIZZOTTI 090814	9/8/2014	150.00	0.00	10/07/2014				False	0
	205-000-210-2430 Parks and Recs Control Deposi			Refund - Computer Explorer					
	090814 Total:	150.00							
	RIZZOTTI Total:	150.00							
	Rizzotti, Brigita Total:	150.00							
Robbins, Salomon & Patt, LTD RS&PLTD 179791	9/10/2014	1,162.50	0.00	10/07/2014				False	0
	101-230-511-5399 Other professional services			Municipal Prosecution/Traffic - August					
	179791 Total:	1,162.50							
179792	9/10/2014	795.00	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
101-230-511-5399	Other professional services				Adjudicative Hearings - August				
	179792 Total:	795.00							
	RS&PLTD Total:	1,957.50							
	Robbins, Salomon & Patt,	1,957.50							
Schejbal, Jim SCHEJBAL									
090914	9/9/2014	62.00	0.00	10/07/2014				False	0
205-000-210-2430	Parks and Recs Control Deposi				Refund - Kids Track				
	090914 Total:	62.00							
	SCHEJBAL Total:	62.00							
	Schejbal, Jim Total:	62.00							
Sikaras, Demetra SIKARASD									
090814	9/8/2014	124.00	0.00	10/07/2014				False	0
205-000-210-2430	Parks and Recs Control Deposi				Refund - Track & Field				
	090814 Total:	124.00							
	SIKARASD Total:	124.00							
	Sikaras, Demetra Total:	124.00							
TransUnion Risk and Alternative TRANSUN									
556811-0814	8/31/2014	28.50	0.00	10/07/2014				False	0
101-300-512-5399	Other professional services				System for Background research and information on people				

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	556811-0814 Total:	28.50							
	TRANSUN Total:	28.50							
	TransUnion Risk and Alter	28.50							
Trizetto Provider Solutions									
TRIZETTO									
7108091400	9/1/2014	149.36	0.00	10/07/2014				False	0
	101-000-410-4315 Ambulance & EMS fees			Claims transaction fee for ambulance invoices					
	7108091400 Total:	149.36							
	TRIZETTO Total:	149.36							
	Trizetto Provider Solutions	149.36							
U-Line									
ULINE									
61330741	9/3/2014	36.31	0.00	10/07/2014				False	0
	101-350-512-5660 EMS supplies			Hand sanitizer dispenser					
61330741	9/3/2014	-19.44	0.00	10/07/2014				False	0
	101-350-512-5660 EMS supplies			Credit					
	61330741 Total:	16.87							
	ULINE Total:	16.87							
	U-Line Total:	16.87							
VCG Uniform									
VCGUNIFO									
11260	8/29/2014	46.00	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
101-300-512-5070	Uniform allowance			Uniform allowance					
	11260 Total:	46.00							
	VCGUNIFO Total:	46.00							
	VCG Uniform Total:	46.00							
Vollmar Clay Products Company									
VOLLMER									
163624	8/6/2014	238.50	0.00	10/07/2014				False	0
101-440-513-5730	Program supplies			Valve vault for streets					
	163624 Total:	238.50							
163833	8/27/2014	881.50	0.00	10/07/2014				False	0
101-440-513-5730	Program supplies			Bends, elbow, PVC for Streets					
	163833 Total:	881.50							
	VOLLMER Total:	1,120.00							
	Vollmar Clay Products Com	1,120.00							
Woodward Printing Services									
WOODWARD									
42829	8/31/2014	180.00	0.00	10/07/2014				False	0
205-500-515-5720	Postage			Additional postage for Fall brochure					
	42829 Total:	180.00							
	WOODWARD Total:	180.00							
	Woodward Printing Service	180.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
Yoshimura, Natalie									
YOSHIMUR									
	9/15/2014	105.00	0.00	10/07/2014				False	0
205-000-210-2430 Parks and Recs Control Deposi				Refund - Pee Wee Tennis					
	Total:	105.00							
	YOSHIMUR Total:	105.00							
	Yoshimura, Natalie Total:	105.00							
Zep Manufacturing Company									
ZEPMANUF									
9001168946	8/28/2014	70.93	0.00	10/07/2014				False	0
660-620-519-5675 Lubricants & fluids				Zep 50					
9001168946	8/28/2014	70.93	0.00	10/07/2014				False	0
101-440-513-5675 Lubricants & fluids				Zep 50					
9001168946	8/28/2014	70.94	0.00	10/07/2014				False	0
205-430-515-5675 Lubricants & fluids				Zep 50					
	9001168946 Total:	212.80							
	ZEPMANUF Total:	212.80							
	Zep Manufacturing Compa	212.80							
	Report Total:	138,143.64							

Accounts Payable

To Be Paid Proof List

User: jmazzeffi
 Printed: 09/29/2014 - 2:04PM
 Batch: 00102.10.2014



Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description	Reference				
303 Taxi									
303									
20878	9/9/2014	106.00	0.00	10/07/2014				False	0
205-570-515-5280 Subsidized taxi program				Taxi Coupons - August					
20878 Total:		106.00							
303 Total:		106.00							
303 Taxi Total:		106.00							
American Traffic Solutions									
ATS									
INV00016690	8/31/2014	4,610.00	0.00	10/07/2014				False	0
101-300-512-5599 Other contractual				Monthly Contractual Fee - August 2014					
INV00016690 Total:		4,610.00							
ATS Total:		4,610.00							
American Traffic Solutions		4,610.00							
Averus									
AVERIS									
775451	9/16/2014	30.00	0.00	10/07/2014				False	0
101-350-512-5499 R&M - other				Baffles for suppression system					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	775451 Total:	30.00							
	AVERIS Total:	30.00							
	Averus Total:	30.00							
Ayers, Karen AYERS 091214	9/12/2014	80.00	0.00	10/07/2014				False	0
	205-000-210-2430 Parks and Recs Control Deposi			Refund - Nerf Explosion					
	091214 Total:	80.00							
	AYERS Total:	80.00							
	Ayers, Karen Total:	80.00							
Batorski, Christina BATORSKI 091214	9/12/2014	128.00	0.00	10/07/2014				False	0
	205-000-210-2430 Parks and Recs Control Deposi			Refund - Ripped					
	091214 Total:	128.00							
	BATORSKI Total:	128.00							
	Batorski, Christina Total:	128.00							
Blackboard Connect, Inc. BLACKBOA 33343081314	8/13/2014	8,528.00	0.00	10/07/2014				False	0
	215-000-512-5410 R&M - communications equipm			Annual Service Fee - Nov 10, 2014 thru Nov 9, 2015					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	33343081314 Total:	8,528.00							
	BLACKBOA Total:	8,528.00							
	Blackboard Connect, Inc. T	8,528.00							
Call One CALLONE									
101-7823-0001	9/15/2014	2,406.87	0.00	10/07/2014				False	0
101-210-511-5580 Telephone				Telephone Services - Aug - Admin/Police					
	101-7823-0001 Total:	2,406.87							
101-7823-0002	9/15/2014	322.80	0.00	10/07/2014				False	0
660-610-519-5580 Telephone				Telephone Services - Aug - Standpipe SCADA					
	101-7823-0002 Total:	322.80							
101-7823-0003	9/15/2014	189.78	0.00	10/07/2014				False	0
101-210-511-5580 Telephone				Telephone Services - Aug - Police Radio Circuit					
	101-7823-0003 Total:	189.78							
101-7823-0004	9/15/2014	47.17	0.00	10/07/2014				False	0
205-560-515-5580 Telephone				Telephone Services - Aug - Aquatic Center Backup					
	101-7823-0004 Total:	47.17							
101-7823-0005	9/15/2014	268.19	0.00	10/07/2014				False	0
660-610-519-5580 Telephone				Telephone Services - Aug - Public Works PRI					
	101-7823-0005 Total:	268.19							
101-7823-0007	9/15/2014	377.53	0.00	10/07/2014				False	0
660-610-519-5580 Telephone				Telephone Services - Aug - Public Works point to point					
	101-7823-0007 Total:	377.53							
101-7823-0008	9/15/2014	27.70	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
660-610-519-5580 Telephone				Telephone Services - Aug - Pump House					
101-7823-0008 Total:		27.70							
101-7823-0009	9/15/2014	115.00	0.00	10/07/2014				False	0
660-610-519-5580 Telephone				Telephone Services - Aug - Red Center					
101-7823-0009	9/15/2014	138.03	0.00	10/07/2014				False	0
101-210-511-5580 Telephone				Telephone Services - Aug - Red Center					
101-7823-0009 Total:		253.03							
101-7823-0010	9/15/2014	179.67	0.00	10/07/2014				False	0
101-210-511-5580 Telephone				Telephone Services - Aug - Municipal Center					
101-7823-0010	9/15/2014	192.43	0.00	10/07/2014				False	0
101-210-511-5580 Telephone				Telephone Services - Aug - Municipal Center					
101-7823-0010 Total:		372.10							
CALLONE Total:		4,265.17							
Call One Total:		4,265.17							
Canon Solutions America									
CANN									
2429828	9/1/2014	36.30	0.00	10/07/2014				False	0
101-210-511-5440 R&M - office equipment				Maintenance Copier -PD - September 2014					
2429828	9/1/2014	33.35	0.00	10/07/2014				False	0
101-210-511-5440 R&M - office equipment				Maintenance Copier usage -PD - August 2014					
2429828 Total:		69.65							
CANN Total:		69.65							
Canon Solutions America T		69.65							

Canon Solutions America, Inc
CANONSOL

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
988306888	9/13/2014	270.78	0.00	10/07/2014				False	0
101-210-511-5440 R&M - office equipment				Maintenance for plotter - Sept					
	988306888 Total:	270.78							
	CANONSOL Total:	270.78							
	Canon Solutions America,	270.78							
CDW Government CDWGOV PB49630	8/27/2014	424.75	0.00	10/07/2014				False	0
215-000-512-5640 Computer supplies				Computer supplies					
	PB49630 Total:	424.75							
	CDWGOV Total:	424.75							
	CDW Government Total:	424.75							
Chicago Communications, LLC CHGOCOMM 263909	9/3/2014	774.40	0.00	10/07/2014				False	0
101-300-512-5410 R&M - communications equipm				Portable radio maintenance - October					
	263909 Total:	774.40							
	CHGOCOMM Total:	774.40							
	Chicago Communications,	774.40							
ClientFirst Consulting Group, LLC CLIENTFI 4512	8/31/2014	977.50	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
101-250-511-6530	Equipment - data processing				Back up system implementation				
	4512 Total:	977.50							
4513	8/31/2014	210.00	0.00	10/07/2014				False	0
101-250-561-6530	Equipment - data processing				Public Works to Village Hall fiber				
	4513 Total:	210.00							
4514	8/31/2014	127.50	0.00	10/07/2014				False	0
205-560-515-5640	Computer supplies				Pool set up				
	4514 Total:	127.50							
4515	8/31/2014	318.75	0.00	10/07/2014				False	0
660-610-519-5330	Data processing				Public Works projects				
	4515 Total:	318.75							
4516	8/31/2014	658.75	0.00	10/07/2014				False	0
660-610-519-5330	Data processing				Springbrook IT assistance				
	4516 Total:	658.75							
4517	8/31/2014	297.50	0.00	10/07/2014				False	0
101-250-511-6530	Equipment - data processing				File server replacement				
	4517 Total:	297.50							
4518	8/31/2014	42.50	0.00	10/07/2014				False	0
101-250-511-6530	Equipment - data processing				Laptop replacement				
	4518 Total:	42.50							
4526	8/31/2014	3,255.00	0.00	10/07/2014				False	0
660-620-519-5599	Other contractual				Link balancer				
	4526 Total:	3,255.00							
4527	8/31/2014	6,351.25	0.00	10/07/2014				False	0
101-250-511-5320	Consulting				IT support				

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	4527 Total:	6,351.25							
4528	8/31/2014	1,927.50	0.00	10/07/2014				False	0
101-250-511-5320	Consulting			IT Strategic planning					
	4528 Total:	1,927.50							
	CLIENTFI Total:	14,166.25							
	ClientFirst Consulting Gro	14,166.25							
Flood, Nancy									
FLOOD									
091714	9/17/2014	25.00	0.00	10/07/2014				False	0
205-000-210-2430	Parks and Recs Control Deposi			Refund - Club Kid					
	091714 Total:	25.00							
	FLOOD Total:	25.00							
	Flood, Nancy Total:	25.00							
Ghezzi, Carol									
GHEZZIC									
091614	9/16/2014	91.80	0.00	10/07/2014				False	0
205-000-210-2430	Parks and Recs Control Deposi			Refund - Peace Yoga					
	091614 Total:	91.80							
	GHEZZIC Total:	91.80							
	Ghezzi, Carol Total:	91.80							

Greenlayer

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
GREENLAY									
3875	9/11/2014	10,296.00	0.00	10/07/2014			0000000536	False	1
205-509-515-5730 Program supplies				Turkey Trot T - shirts					
3875	9/11/2014	1,788.00	0.00	10/07/2014			0000000536	False	2
205-509-515-5730 Program supplies				Cotton T - shirt					
	3875 Total:	12,084.00							
	GREENLAY Total:	12,084.00							
	Greenlayer Total:	12,084.00							
Ha, Grace									
HA									
091714	9/17/2014	46.00	0.00	10/07/2014				False	0
205-000-210-2430 Parks and Recs Control Deposi				Refund - Zumba					
	091714 Total:	46.00							
	HA Total:	46.00							
	Ha, Grace Total:	46.00							
Halogen									
HALOGEN									
459106	9/9/2014	2,966.00	0.00	10/07/2014				False	0
205-560-515-5630 Chemicals - swimming pool				Anti freeze for pool					
	459106 Total:	2,966.00							
459107	9/9/2014	241.61	0.00	10/07/2014				False	0
205-560-515-5499 R&M - other				Pump parts for pool					
	459107 Total:	241.61							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	HALOGEN Total:	3,207.61							
	Halogen Total:	3,207.61							
Health Endeavors HEALTHEN 3843	9/10/2014	735.00	0.00	10/07/2014				False	0
101-200-511-5599 Other contractual				Pre-employment screenings - Police Officer					
	3843 Total:	735.00							
	HEALTHEN Total:	735.00							
	Health Endeavors Total:	735.00							
Holland & Knight LLP HOLLAND 09182014	9/18/2014	11,347.00	0.00	10/07/2014				False	0
101-230-511-5350 Legal - retainer				August retainer					
	09182014 Total:	11,347.00							
3093275	9/15/2014	385.00	0.00	10/07/2014				False	0
101-230-511-5370 Legal - review				August 2014 legal - Firearms regulations					
	3093275 Total:	385.00							
3093276	9/15/2014	875.00	0.00	10/07/2014				False	0
217-000-517-5399 Other professional services				August 2014 legal - Nrrthst Indus. TIF District					
	3093276 Total:	875.00							
3093278	9/15/2014	1,382.50	0.00	10/07/2014				False	0
101-230-511-5370 Legal - review				August 2014 legal - Purple Hotel Dev.					
	3093278 Total:	1,382.50							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
3093280	9/15/2014	1,127.50	0.00	10/07/2014				False	0
101-230-511-5370	Legal - review			August 2014 legal - Misc Priv. Dockets					
	3093280 Total:	1,127.50							
3093281	9/15/2014	600.00	0.00	10/07/2014				False	0
101-230-511-5370	Legal - review			August 2014 legal - Jaffa Bagels					
	3093281 Total:	600.00							
3093283	9/15/2014	577.50	0.00	10/07/2014				False	0
101-230-511-5370	Legal - review			August 2014 legal - Brickyard Bank					
	3093283 Total:	577.50							
3093285	9/15/2014	275.00	0.00	10/07/2014				False	0
101-230-511-5370	Legal - review			August 2014 legal - Kow Kow					
	3093285 Total:	275.00							
3093288	9/15/2014	63.00	0.00	10/07/2014				False	0
101-230-511-5370	Legal - review			August 2014 legal - Telecom. Ord					
	3093288 Total:	63.00							
3093293	9/15/2014	1,080.00	0.00	10/07/2014				False	0
101-230-511-5360	Legal - litigation			August 2014 legal - Litigation					
	3093293 Total:	1,080.00							
3093294	9/15/2014	922.50	0.00	10/07/2014				False	0
101-230-511-5370	Legal - review			August 2014 legal - Zoning Code					
	3093294 Total:	922.50							
	HOLLAND Total:	18,635.00							
	Holland & Knight LLP Tot	18,635.00							

Illinois City/County Management Association

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
ILCMA									
091514	9/15/2014	161.00	0.00	10/07/2014				False	0
101-200-511-5570				Professional associations	2014-15 Membership renewal				
	091514 Total:	161.00							
	ILCMA Total:	161.00							
	Illinois City/County Manag	161.00							
Illinois Homicide Investigators Association									
IHIA									
ILHIA2014	9/15/2014	1,170.00	0.00	10/07/2014				False	0
101-300-512-5810				Conference & meeting registrat	Fifth annual training conference				
	ILHIA2014 Total:	1,170.00							
	IHIA Total:	1,170.00							
	Illinois Homicide Investiga	1,170.00							
Kane McKenna & Associates									
KANEMKEN									
091114	9/11/2014	3,087.50	0.00	10/07/2014				False	0
101-240-517-5399				Other professional services	Preparation of documents for proposed Lincoln/Touhy				
	091114 Total:	3,087.50							
091614	9/16/2014	4,825.00	0.00	10/07/2014				False	0
101-240-517-5399				Other professional services	Research/document preparation for Shoppes at Lincoln Poir				
	091614 Total:	4,825.00							
	KANEMKEN Total:	7,912.50							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Kane McKenna & Associa	7,912.50							
KC Fitness									
KCFITNES									
56080	9/15/2014	701.91	0.00	10/07/2014				False	0
101-350-512-5499 R&M - other				Treadmill repairs					
	56080 Total:	701.91							
	KCFITNES Total:	701.91							
	KC Fitness Total:	701.91							
Knezevic, Gordana									
KNEZEVIC									
091214	9/12/2014	65.00	0.00	10/07/2014				False	0
205-000-210-2430 Parks and Recs Control Deposi				Refund - Soccer					
	091214 Total:	65.00							
	KNEZEVIC Total:	65.00							
	Knezevic, Gordana Total:	65.00							
L3 Communications Mobile Vision, Inc.									
L3COMM									
0216609-IN	9/4/2014	252.00	0.00	10/07/2014				False	0
101-300-512-5730 Program supplies				Antennas for L3 Video-GPS					
	0216609-IN Total:	252.00							
	L3COMM Total:	252.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	L3 Communications Mobil	252.00							
Lund Industries									
LUNDIND									
80244	8/28/2014	125.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies				Install new docking stations for laptop in squads				
	80244 Total:	125.00							
80245	8/28/2014	125.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies				Install new docking stations for laptop in squads				
	80245 Total:	125.00							
80246	8/28/2014	139.12	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies				Install new docking stations for laptop in squads				
	80246 Total:	139.12							
80247	8/28/2014	139.12	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies				Install new docking stations for laptop in squads				
	80247 Total:	139.12							
	LUNDIND Total:	528.24							
	Lund Industries Total:	528.24							
Malnati Organization									
MALNATI									
E157067	9/16/2014	214.85	0.00	10/07/2014				False	0
	101-200-511-5840 Meals				Village Board Meeting Dinner - 9/16/14				
	E157067 Total:	214.85							
	MALNATI Total:	214.85							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
		214.85							
Malnati Organization Total									
Midwest Air Pro MIDWESTA 12469 101-350-512-5499 R&M - other	9/10/2014	360.00	0.00	10/07/2014	Repairs to vehicle exhaust system			False	0
	12469 Total:	360.00							
	MIDWESTA Total:	360.00							
	Midwest Air Pro Total:	360.00							
North East Multi-Regional Training NORTHEST 185949 101-300-512-5590 Training									
	9/4/2014	100.00	0.00	10/07/2014	Training - Laws of Arrest, search & seizure			False	0
	185949 Total:	100.00							
	NORTHEST Total:	100.00							
	North East Multi-Regional	100.00							
North Suburban Employee Benefit NSEBENEF Oct-14 102-000-210-2028 Dental insurance premium with									
	9/10/2014	10,034.00	0.00	10/07/2014	Dental Bill - October 2014			False	0
	Oct-14 Total:	10,034.00							
	NSEBENEF Total:	10,034.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	North Suburban Employee	10,034.00							
PEP Boys									
PEPBOYS									
04791017865	9/3/2014	59.95	0.00	10/07/2014				False	0
101-350-512-5740	Repair parts			Fuel tank reapi	kit, steel epoxy				
	04791017865 Total:	59.95							
	PEPBOYS Total:	59.95							
	PEP Boys Total:	59.95							
Personnel Strategies, LLC									
PERSONNE									
090614	9/6/2014	500.00	0.00	10/07/2014				False	0
101-200-511-5599	Other contractual			Pre-employment	psychological assessment				
090614	9/13/2014	500.00	0.00	10/07/2014				False	0
101-200-511-5599	Other contractual			Pre-employment	psychological assessment - Police Officer				
	090614 Total:	1,000.00							
	PERSONNE Total:	1,000.00							
	Personnel Strategies, LLC	1,000.00							
Pitney Bowes									
PITNEYBO									
2355016-SP14	9/13/2014	262.26	0.00	10/07/2014				False	0
101-210-511-5440	R&M - office equipment			Rental fees -	Augs 30 thur Sept 30				
	2355016-SP14 Total:	262.26							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	PITNEYBO Total:	262.26							
	Pitney Bowes Total:	262.26							
Promotional Capital, LLC PROMOCA 24677	9/8/2014	227.75	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Police badge stickers					
	24677 Total:	227.75							
	PROMOCA Total:	227.75							
	Promotional Capital, LLC	227.75							
Sam's Club SAMSCL 5312	9/9/2014	23.52	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Misc Dept supplies					
	5312 Total:	23.52							
	SAMSCL Total:	23.52							
	Sam's Club Total:	23.52							
Sec of State Renewals Section SECOFSTR SOS0914	9/15/2014	202.00	0.00	10/07/2014				False	0
	101-300-512-5599 Other contractual			License plate renewal					
	SOS0914 Total:	202.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	SECOFSTR Total:	202.00							
	Sec of State Renewals Sect	202.00							
SmartEvents Rentals									
SMARTEVE									
5115	8/13/2014	528.00	0.00	10/07/2014				False	0
101-200-511-5799	Other materials & supplies			Entertainment - Employee Appreciation event					
	5115 Total:	528.00							
	SMARTEVE Total:	528.00							
	SmartEvents Rentals Total:	528.00							
Springbrook Software Inc									
SPRINGBR									
INV29182	9/9/2014	2,459.17	0.00	10/07/2014				False	0
660-610-519-5330	Data processing			Travel expenses Week One 7/6 thru 7/11					
	INV29182 Total:	2,459.17							
INV29183	9/9/2014	2,176.84	0.00	10/07/2014				False	0
101-000-210-2650	Contractor Permits Payable			Travel expenses Week Two 7/14 thru 7/18					
	INV29183 Total:	2,176.84							
	SPRINGBR Total:	4,636.01							
	Springbrook Software Inc T	4,636.01							
State Industrial Products									
STATE									
96961089	9/11/2014	124.95	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
101-300-512-5730	Program supplies				Sanitizer for cells/dispatch				
	96961089 Total:	124.95							
	STATE Total:	124.95							
	State Industrial Products To	124.95							
Sullivan, Dan SULLIVAN									
11132014	2/4/2014	250.00	0.00	10/07/2014				False	0
205-570-515-5270	Purchased program services				Entertainer for Thanksgiving luncheon				
	11132014 Total:	250.00							
	SULLIVAN Total:	250.00							
	Sullivan, Dan Total:	250.00							
Trans Union Corp TRANSU									
08400695	8/25/2014	45.00	0.00	10/07/2014				False	0
101-300-512-5399	Other professional services				Credit check on applicants monthly subscription fee				
	08400695 Total:	45.00							
	TRANSU Total:	45.00							
	Trans Union Corp Total:	45.00							
White Egel WHITEAGL									
13518	9/15/2014	300.00	0.00	10/07/2014				False	0
205-570-515-5270	Purchased program services				Deposit for Social Club Holiday Party				

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number					Description	Reference			

13518 Total:	300.00
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WHITEAGL Total:	300.00
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White Eagel Total:	300.00
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Report Total:	97,436.35
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Accounts Payable

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Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description	Reference				
Active Electrical Supply Co., Inc. & Fox Lighting									
ACTIVELE									
10434098-00	9/5/2014	34.04	0.00	10/07/2014				False	0
101-420-511-5405 R&M - buildings				Electrical outlet for Village Hall					
10434098-00 Total:		34.04							
ACTIVELE Total:		34.04							
Active Electrical Supply C		34.04							
American Charge Service									
AMERCHAR									
95228	9/19/2014	41.00	0.00	10/07/2014				False	0
205-570-515-5280 Subsidized taxi program				Taxi Coupons - September					
95228 Total:		41.00							
AMERCHAR Total:		41.00							
American Charge Service T		41.00							
ARRP Trucking & Hauling Inc									
ARRP									
7495	9/15/2014	1,028.00	0.00	10/07/2014				False	0
660-620-519-5599 Other contractual				4 loads dirt hauled out					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	7495 Total:	1,028.00							
	ARRP Total:	1,028.00							
	ARRP Trucking & Hauling	1,028.00							
Auto Glass Tek, Inc.									
AUTOG									
41250	9/15/2014	215.00	0.00	10/07/2014	Windshield for Truck #4			False	0
	660-620-519-5480 R&M - vehicles								
	41250 Total:	215.00							
41251	9/15/2014	215.00	0.00	10/07/2014	Windshield for Truck #48			False	0
	205-430-515-5480 R&M - vehicles								
	41251 Total:	215.00							
	AUTOG Total:	430.00							
	Auto Glass Tek, Inc. Total:	430.00							
Barnett, Tim									
BARNETT									
14-9401	9/5/2014	100.00	0.00	10/07/2014	Reimbursement for 50/50 animal control			False	0
	101-400-511-5210 Animal control								
	14-9401 Total:	100.00							
	BARNETT Total:	100.00							
	Barnett, Tim Total:	100.00							

Bell Fuels, Inc.

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
BELLFUEL									
209734	7/2/2014	698.67	0.00	10/07/2014				False	0
660-620-519-5670 Fuel				Fuel for generator for PW					
	209734 Total:	698.67							
209735	7/2/2014	800.16	0.00	10/07/2014				False	0
101-440-513-5670 Fuel				Fuel for generator for Stand Pipe					
	209735 Total:	800.16							
	BELLFUEL Total:	1,498.83							
	Bell Fuels, Inc. Total:	1,498.83							
Bristol Hose & Fittings									
BRISTOL									
350315	5/1/2014	36.90	0.00	10/07/2014				False	0
101-440-513-5480 R&M - vehicles				Fabricate hose assembly for PW vehicles					
	350315 Total:	36.90							
	BRISTOL Total:	36.90							
	Bristol Hose & Fittings Tot	36.90							
Case Lots, Inc.									
CASELOTS									
434	9/10/2014	1,143.45	0.00	10/07/2014				False	0
101-420-511-5730 Program supplies				Pine sol, garbage bags, multi folding towels					
	434 Total:	1,143.45							
	CASELOTS Total:	1,143.45							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Case Lots, Inc. Total:	1,143.45							
Coca-Cola Bottling Company COCACOLA 338471710	9/22/2014	231.84	0.00	10/07/2014	Pop for Public Works pop machine			False	0
101-210-511-5700 Office supplies									
	338471710 Total:	231.84							
	COCACOLA Total:	231.84							
	Coca-Cola Bottling Compa	231.84							
David & Sons Concrete DAVID 092314	9/23/2014	1,000.00	0.00	10/07/2014	Refund - driveway deposit			False	0
101-000-210-2620 Contractor bonds payable									
	092314 Total:	1,000.00							
	DAVID Total:	1,000.00							
	David & Sons Concrete To	1,000.00							
Emcor Services Team Mechanical Inc EMCOR 3043559	9/3/2014	2,500.00	0.00	10/07/2014	Preventive contract maintenance - September			False	0
101-420-511-5405 R&M - buildings									
	3043559 Total:	2,500.00							
	EMCOR Total:	2,500.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Emcor Services Team Mec	2,500.00							
Fank, Jurgen FANK 14-9400	9/3/2014	72.50	0.00	10/07/2014				False	0
101-400-511-5210	Animal control			Reimbursement for 50/50 animal control					
	14-9400 Total:	72.50							
	FANK Total:	72.50							
	Fank, Jurgen Total:	72.50							
Fastframe FASTFRAM 26992	9/15/2014	83.38	0.00	10/07/2014				False	0
101-100-511-5799	Other materials & supplies			Reframe photo in Admin					
	26992 Total:	83.38							
	FASTFRAM Total:	83.38							
	Fastframe Total:	83.38							
Fedex FEDEX 2-783-68063	9/17/2014	28.18	0.00	10/07/2014				False	0
101-210-511-5720	Postage			Shipping - Administration					
	2-783-68063 Total:	28.18							
	FEDEX Total:	28.18							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Fedex Total:	28.18							
Global Emergency Products Inc GLOBALEN AGJ8449	9/16/2014	7,483.70	0.00	10/07/2014				False	0
101-350-512-5480 R&M - vehicles				Annual preventive maintenance and ladder testing					
	AGJ8449 Total:	7,483.70							
	GLOBALEN Total:	7,483.70							
	Global Emergency Product	7,483.70							
Golf Mill Ford GOLFMILL 360822P	9/6/2014	94.43	0.00	10/07/2014				False	0
101-440-513-5480 R&M - vehicles				Cover for Truck #16					
	360822P Total:	94.43							
361176P	9/11/2014	154.14	0.00	10/07/2014				False	0
101-300-512-5480 R&M - vehicles				Nut, bolt, joint assembly for PD					
	361176P Total:	154.14							
361178P	9/12/2014	16.02	0.00	10/07/2014				False	0
101-300-512-5480 R&M - vehicles				Nut, bolt for PD					
	361178P Total:	16.02							
361184P	9/11/2014	113.80	0.00	10/07/2014				False	0
101-300-512-5480 R&M - vehicles				Link for PD					
	361184P Total:	113.80							
	GOLFMILL Total:	378.39							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Golf Mill Ford Total:	378.39							
HMO Healthcare Service Corporation									
HMO									
091614	9/16/2014	28,482.72	0.00	10/07/2014				False	0
	102-000-210-2027 Health insurance premium with			Employee Health Insurance HMO - October, 2014					
	091614 Total:	28,482.72							
	HMO Total:	28,482.72							
	HMO Healthcare Service C	28,482.72							
Kellogg School of Management									
KELLOGG									
9182014	9/18/2014	2,500.00	0.00	10/07/2014				False	0
	101-200-511-5590 Training			Department Head Development					
	9182014 Total:	2,500.00							
	KELLOGG Total:	2,500.00							
	Kellogg School of Manage	2,500.00							
Lawson Products Inc									
LAWSNPRO									
9302554743	6/27/2014	280.18	0.00	10/07/2014				False	0
	101-410-511-5730 Program supplies			Shop supplies					
	9302554743 Total:	280.18							
9302555971	6/27/2014	422.13	0.00	10/07/2014				False	0
	101-410-511-5730 Program supplies			Shop supplies					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	9302555971 Total:	422.13							
9302628242	7/30/2014	85.17	0.00	10/07/2014				False	0
101-410-511-5730	Program supplies			Shop supplies					
	9302628242 Total:	85.17							
	LAWSNPRO Total:	787.48							
	Lawson Products Inc Total	787.48							
Lee Auto Parts									
LEEAUTOP									
442-286773	9/11/2014	101.98	0.00	10/07/2014				False	0
101-300-512-5480	R&M - vehicles			Sway bar for Squad #217					
	442-286773 Total:	101.98							
442-287017	9/15/2014	35.84	0.00	10/07/2014				False	0
101-300-512-5480	R&M - vehicles			Mini lamp for Squad #217					
	442-287017 Total:	35.84							
442-287179	9/16/2014	51.48	0.00	10/07/2014				False	0
101-300-512-5480	R&M - vehicles			Mercon for Squad #217					
	442-287179 Total:	51.48							
	LEEAUTOP Total:	189.30							
	Lee Auto Parts Total:	189.30							
Lowe's Business Acc/GECE									
LOWES									
2475	8/8/2014	101.52	0.00	10/07/2014				False	0
101-440-513-5730	Program supplies			Wasp and Hornet spray					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	2475 Total:	101.52							
	LOWES Total:	101.52							
	Lowe's Business Acc/GEC	101.52							
Madison National Life									
MADISON									
1144733	9/18/2014	148.11	0.00	10/07/2014				False	0
	101-200-511-5150 Insurance - group life & AD&D				Employee Life insurance - Oct - 14				
1144733	9/18/2014	90.34	0.00	10/07/2014				False	0
	101-210-511-5150 Insurance - group life & AD&D				Employee Life insurance - Oct - 14				
1144733	9/18/2014	84.91	0.00	10/07/2014				False	0
	101-240-517-5150 Insurance - group life & AD&D				Employee Life insurance - Oct - 14				
1144733	9/18/2014	628.58	0.00	10/07/2014				False	0
	101-300-512-5150 Insurance - group life & AD&D				Employee Life insurance - Oct - 14				
1144733	9/18/2014	16.65	0.00	10/07/2014				False	0
	101-350-512-5150 Insurance - group life & AD&D				Employee Life insurance - Oct - 14				
1144733	9/18/2014	70.70	0.00	10/07/2014				False	0
	101-400-511-5150 Insurance - group life & AD&D				Employee Life insurance - Oct - 14				
1144733	9/18/2014	34.41	0.00	10/07/2014				False	0
	101-410-511-5150 Insurance - group life & AD&D				Employee Life insurance - Oct - 14				
1144733	9/18/2014	95.00	0.00	10/07/2014				False	0
	101-440-513-5150 Insurance - group life & AD&D				Employee Life insurance - Oct - 14				
1144733	9/18/2014	57.04	0.00	10/07/2014				False	0
	205-430-515-5150 Insurance - group life & AD&D				Employee Life insurance - Oct - 14				
1144733	9/18/2014	90.89	0.00	10/07/2014				False	0
	205-500-515-5150 Insurance - group life & AD&D				Employee Life insurance - Oct - 14				
1144733	9/18/2014	93.14	0.00	10/07/2014				False	0
	660-620-519-5150 Insurance - group life & AD&D				Employee Life insurance - Oct - 14				
	1144733 Total:	1,409.77							
	MADISON Total:	1,409.77							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
		1,409.77							
Madison National Life Tot									
Martinez, Ed									
MARTINED									
09222014	9/22/2014	270.00	0.00	10/07/2014				False	0
205-000-210-2430 Parks and Recs Control Deposi				Refund - Aquatic Center					
	09222014 Total:	270.00							
	MARTINED Total:	270.00							
	Martinez, Ed Total:	270.00							
Metro Tank and Pump Company									
METROENV									
12081	9/10/2014	1,144.86	0.00	10/07/2014				False	0
101-420-511-5405 R&M - buildings				Diesel nozzles, unleaded nozzles					
	12081 Total:	1,144.86							
	METROENV Total:	1,144.86							
	Metro Tank and Pump Com	1,144.86							
MGP, Inc.									
MGPINC									
379	9/18/2014	390.86	0.00	10/07/2014				False	0
101-250-511-5599 Other contractual				GISC Staffing					
379	9/18/2014	390.86	0.00	10/07/2014				False	0
101-000-210-2650 Contractor Permits Payable				GISC Staffing					
379	9/18/2014	781.71	0.00	10/07/2014				False	0
660-620-519-5599 Other contractual				GISC Staffing					
	379 Total:	1,563.43							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	MGPINC Total:	1,563.43							
	MGP, Inc. Total:	1,563.43							
Northern Illinois University-Grants Fiscal Admin NIUG									
PRI091614	9/16/2014	1,056.00	0.00	10/07/2014				False	0
101-240-517-5399	Other professional services			Student Intern - Community Dev. - August					
	PRI091614 Total:	1,056.00							
	NIUG Total:	1,056.00							
	Northern Illinois Universit	1,056.00							
Novoselov, Sergey NOVOSELO									
092214	9/22/2014	665.60	0.00	10/07/2014				False	0
101-350-512-4315	Ambulance & EMS fees			Reimbursement - overpayment ambulance bill					
	092214 Total:	665.60							
	NOVOSELO Total:	665.60							
	Novoselov, Sergey Total:	665.60							
O'Leary's Contractor Equip OLEARYS									
112363	7/29/2014	257.23	0.00	10/07/2014				False	0
660-620-519-5745	Small tools			Spark plug, lever control, O rings, oil for Water					
	112363 Total:	257.23							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	OLEARYS Total:	257.23							
	O'Leary's Contractor Equip	257.23							
Orange Crush LLC									
ORANGCRH									
467600	9/8/2014	338.66	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod surface					
	467600 Total:	338.66							
467839	9/9/2014	397.07	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod surface					
	467839 Total:	397.07							
467972	9/10/2014	585.65	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod surface					
	467972 Total:	585.65							
468082	9/11/2014	442.80	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod surface					
	468082 Total:	442.80							
468182	9/12/2014	467.28	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod surface					
	468182 Total:	467.28							
468803	9/17/2014	647.23	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod surface					
	468803 Total:	647.23							
	ORANGCRH Total:	2,878.69							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Orange Crush LLC Total:	2,878.69							
Russo Power Equipment RUSSO									
2070185	7/14/2014	92.63	0.00	10/07/2014				False	0
101-440-513-5730 Program supplies				Blade set 500 for streets					
	2070185 Total:	92.63							
2070196	7/14/2014	143.93	0.00	10/07/2014				False	0
101-440-513-5730 Program supplies				Grain, supersedes for streets					
	2070196 Total:	143.93							
	RUSSO Total:	236.56							
	Russo Power Equipment T	236.56							
Vermeer-Illinois Inc VRMERIL									
P79395	9/8/2014	344.36	0.00	10/07/2014				False	0
101-440-513-5480 R&M - vehicles				Clutch switch and belt tensioner for chipper					
	P79395 Total:	344.36							
	VRMERIL Total:	344.36							
	Vermeer-Illinois Inc Total:	344.36							
West Side Tractor Sales WESTSIDE									
N08416	9/15/2014	29.88	0.00	10/07/2014				False	0
101-440-513-5480 R&M - vehicles				Elbow fittings for Truck #9					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	N08416 Total:	29.88							
	WESTSIDE Total:	29.88							
	West Side Tractor Sales To	29.88							
Westmont Auto Parts WESTMONT 5166	9/4/2014	20.13	0.00	10/07/2014				False	0
101-300-512-5480 R&M - vehicles				Resistor					
	5166 Total:	20.13							
	WESTMONT Total:	20.13							
	Westmont Auto Parts Total	20.13							
	Report Total:	58,027.74							

Accounts Payable

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Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description	Reference				
Ace Hardware Skokie									
ACEHRDS									
186030	9/5/2014	36.96	0.00	10/07/2014				False	0
205-430-515-5730 Program supplies				Extension cord for Touch a Truck					
186030 Total:		36.96							
ACEHRDS Total:		36.96							
Ace Hardware Skokie Tota		36.96							
Active Electrical Supply Co., Inc. & Fox Lighting									
ACTIVELE									
10435019-00	9/12/2014	83.73	0.00	10/07/2014				False	0
101-420-511-5405 R&M - buildings				Electical supplies for Police Dept					
10435019-00 Total:		83.73							
ACTIVELE Total:		83.73							
Active Electrical Supply C		83.73							
Anderson Lock									
ANDERSON									
854191	9/10/2014	136.12	0.00	10/07/2014				False	0
660-620-519-5405 R&M - buildings				Standard cut key for Pump House					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	854191 Total:	136.12							
	ANDERSON Total:	136.12							
	Anderson Lock Total:	136.12							
Atkinson, David ATKINDAV 14-8404	8/26/2014	45.00	0.00	10/07/2014				False	0
101-400-511-5210	Animal control			Reimbursement for 50/50 animal control					
	14-8404 Total:	45.00							
	ATKINDAV Total:	45.00							
	Atkinson, David Total:	45.00							
Atlas Bobcat Inc ATLASB BA7682	9/17/2014	80.67	0.00	10/07/2014				False	0
101-440-513-5730	Program supplies			Filter for Bobcat					
	BA7682 Total:	80.67							
	ATLASB Total:	80.67							
	Atlas Bobcat Inc Total:	80.67							
Calalang, Alvin CALALANG 1SFTSUM14	9/24/2014	400.00	0.00	10/07/2014				False	0
205-540-515-5615	Awards			Sunday Softball Summer 1st place					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
		400.00							
1SFTSUM14 Total:		400.00							
1TRNSUM14	9/24/2014	350.00	0.00	10/07/2014				False	0
205-540-515-5615 Awards				Sunday Softball Tournament 1st place					
		350.00							
1TRNSUM14 Total:		350.00							
		750.00							
CALALANG Total:		750.00							
		750.00							
Calalang, Alvin Total:		750.00							
Cassidy Tire									
CASSIDYT									
3183936	9/16/2014	393.28	0.00	10/07/2014				False	0
101-300-512-5480 R&M - vehicles				Tires and Wheel alignment					
		393.28							
3183936 Total:		393.28							
		393.28							
CASSIDYT Total:		393.28							
		393.28							
Cassidy Tire Total:		393.28							
CDW Government									
CDWGOV									
PL28023	9/15/2014	2,910.45	0.00	10/07/2014				False	0
101-000-210-2480 Unadjudicated forfeitures				Laptop for Police/Fire training room					
		2,910.45							
PL28023 Total:		2,910.45							
		2,910.45							
CDWGOV Total:		2,910.45							
		2,910.45							
CDW Government Total:		2,910.45							

Douglas Truck Parts

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
DOUGTK 77467	9/19/2014	282.00	0.00	10/07/2014				False	0
101-410-511-5730 Program supplies				Gloves, hook, pintle mount, brake clean for Shop					
	77467 Total:	282.00							
	DOUGTK Total:	282.00							
	Douglas Truck Parts Total:	282.00							
EJ Equipment EJ EQUIP 63817	9/12/2014	1,347.13	0.00	10/07/2014				False	0
660-620-519-5480 R&M - vehicles				Valve kit, valve seat for Truck #6					
	63817 Total:	1,347.13							
	EJ EQUIP Total:	1,347.13							
	EJ Equipment Total:	1,347.13							
Elite Printer Solutions ELITE 2552	9/8/2014	134.97	0.00	10/07/2014				False	0
101-240-517-5700 Office supplies				Office supplies					
	2552 Total:	134.97							
2559	9/10/2014	287.94	0.00	10/07/2014				False	0
101-300-512-5640 Computer supplies				Office supplies					
	2559 Total:	287.94							
	ELITE Total:	422.91							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Elite Printer Solutions Tota	422.91							
Garvey, Diane									
GARVEY									
WinterSpring I	9/15/2014	858.48	0.00	10/07/2014				False	0
205-503-515-5270	Purchased program services			Sunday Zumba Spring Session 1					
WinterSpring I	9/15/2014	419.44	0.00	10/07/2014				False	0
205-503-515-5270	Purchased program services			Tuesday Zumba Spring Session 1					
WinterSpring I	9/15/2014	788.90	0.00	10/07/2014				False	0
205-503-515-5270	Purchased program services			Thursday RIPPED Spring Session 1					
WinterSpring I	9/15/2014	196.00	0.00	10/07/2014				False	0
205-503-515-5270	Purchased program services			Drop in fees Spring Session 1					
	WinterSpring I Total:	2,262.82							
WinterSpring II	9/15/2014	891.80	0.00	10/07/2014				False	0
205-503-515-5270	Purchased program services			Sunday Zumba Spring Session 2					
WinterSpring II	9/15/2014	485.10	0.00	10/07/2014				False	0
205-503-515-5270	Purchased program services			Tuesday Zumba Spring Session 2					
WinterSpring II	9/15/2014	661.50	0.00	10/07/2014				False	0
205-503-515-5270	Purchased program services			Thursday RIPPED Spring Session 2					
WinterSpring II	9/15/2014	266.00	0.00	10/07/2014				False	0
205-503-515-5270	Purchased program services			Drop in fees Spring Session 2					
	WinterSpring II Total:	2,304.40							
	GARVEY Total:	4,567.22							
	Garvey, Diane Total:	4,567.22							
Gorenstein, Aron									
GOREN									
2TRNSUN14	9/24/2014	150.00	0.00	10/07/2014				False	0
205-540-515-5615	Awards			Sunday Tournament 2nd place					
	2TRNSUN14 Total:	150.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	GOREN Total:	150.00							
	Gorenstein, Aron Total:	150.00							
Got Laundry Chicago?, Inc.									
GOTLAUND									
257681	7/21/2014	12.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Dry clean cell blankets					
	257681 Total:	12.00							
258103	7/29/2014	30.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Dry clean cell blankets					
	258103 Total:	30.00							
258331	8/12/2014	18.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Dry clean cell blankets					
	258331 Total:	18.00							
258377	8/4/2014	12.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Dry clean cell blankets					
	258377 Total:	12.00							
259591	8/26/2014	36.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Dry clean cell blankets					
	259591 Total:	36.00							
259961	9/1/2014	36.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Dry clean cell blankets					
	259961 Total:	36.00							
260318	9/7/2014	24.00	0.00	10/07/2014				False	0
	101-300-512-5730 Program supplies			Dry clean cell blankets					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	260318 Total:	24.00							
	GOTLAUND Total:	168.00							
	Got Laundry Chicago?, Inc	168.00							
Grainger GRAINGER 9539332529	9/10/2014	516.60	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings			Toilet paper dispenser for Shelter house					
	9539332529 Total:	516.60							
	GRAINGER Total:	516.60							
	Grainger Total:	516.60							
Grais, Ben GRAIS 1SFTSUM14	9/24/2014	250.00	0.00	10/07/2014				False	0
	205-540-515-5615 Awards			Friday Softball Summer 1st place					
	1SFTSUM14 Total:	250.00							
1TRNFRI14	9/24/2014	150.00	0.00	10/07/2014				False	0
	205-540-515-5615 Awards			Friday Softball Tournament 1st place					
	1TRNFRI14 Total:	150.00							
	GRAIS Total:	400.00							
	Grais, Ben Total:	400.00							

Hach Company

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
HACHC									
9030561	9/16/2014	372.89	0.00	10/07/2014				False	0
660-620-519-5635	Chemicals - water system			DPD Free chlorine 25 ML					
	9030561 Total:	372.89							
	HACHC Total:	372.89							
	Hach Company Total:	372.89							
Joyce, Patrick									
JOYCEP									
2SFTSUM14	9/24/2014	225.00	0.00	10/07/2014				False	0
205-540-515-5615	Awards			Sunday Softball Summer 2nd place					
	2SFTSUM14 Total:	225.00							
	JOYCEP Total:	225.00							
	Joyce, Patrick Total:	225.00							
Kraemer, Danny									
KRAEMER									
2SFTSUM14	9/24/2014	150.00	0.00	10/07/2014				False	0
205-540-515-5615	Awards			Firday Softball Summer 2nd place					
	2SFTSUM14 Total:	150.00							
2TRNFRI14	9/24/2014	75.00	0.00	10/07/2014				False	0
205-540-515-5615	Awards			Firday Softball Tournament 2nd place					
	2TRNFRI14 Total:	75.00							
	KRAEMER Total:	225.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Kraemer, Danny Total:	225.00							
Landscape Concepts Management									
LANDSCAP									
75170	8/20/2014	120.00	0.00	10/07/2014	Mow, blow and trim - 6900 Kenton			False	0
	101-000-410-4399 Other charges for services								
	75170 Total:	120.00							
75171	8/20/2014	60.00	0.00	10/07/2014	Mow, blow and trim - 6546 Christiana			False	0
	101-000-410-4399 Other charges for services								
	75171 Total:	60.00							
	LANDSCAP Total:	180.00							
	Landscape Concepts Mana	180.00							
Lurvey Landscape Supply									
LURVEY									
T1-10074421	8/4/2014	-12.80	0.00	10/07/2014	Adhesive titebond wall for Community Ctr.			False	0
	205-430-515-5680 Landscaping supplies								
	T1-10074421 Total:	-12.80							
T1-10074442	8/4/2014	309.75	0.00	10/07/2014	Boulders granite for Community Ctr.			False	0
	205-430-515-5680 Landscaping supplies								
	T1-10074442 Total:	309.75							
T1-10074449	8/4/2014	-12.80	0.00	10/07/2014	Adhesive titebond wall for Community Ctr.			False	0
	205-430-515-5680 Landscaping supplies								
	T1-10074449 Total:	-12.80							
T1-10075181	8/7/2014	58.44	0.00	10/07/2014	Gravel screening lime for Community Ctr.			False	0
	205-430-515-5680 Landscaping supplies								

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	T1-10075181 Total:	58.44							
T1-10076915	8/18/2014	740.50	0.00	10/07/2014				False	0
205-430-515-5680	Landscaping supplies			Gravel, unicare sealer, sand polimeric					
	T1-10076915 Total:	740.50							
T1-10078157	8/26/2014	432.40	0.00	10/07/2014				False	0
205-430-515-5680	Landscaping supplies			Unicare sealer, gravel screening for Community Ctr.					
	T1-10078157 Total:	432.40							
T1-10078401	8/27/2014	63.50	0.00	10/07/2014				False	0
205-430-515-5680	Landscaping supplies			Gravel screening, holland river for Community Ctr.					
	T1-10078401 Total:	63.50							
	LURVEY Total:	1,578.99							
	Lurvey Landscape Supply	1,578.99							
Marc Printing									
MARCP									
108511	9/17/2014	249.84	0.00	10/07/2014				False	0
660-610-519-5720	Postage			Mailing of water bills - Residential & Commercial					
	108511 Total:	249.84							
108518	9/18/2014	428.84	0.00	10/07/2014				False	0
660-610-519-5700	Office supplies			Laser water bill statements & shipping					
	108518 Total:	428.84							
	MARCP Total:	678.68							
	Marc Printing Total:	678.68							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
Martin Implement Sales Inc									
MARTINIM									
P87069	9/16/2014	87.65	0.00	10/07/2014				False	0
205-430-515-5480 R&M - vehicles				Door handle for Tractor #3					
	P87069 Total:	87.65							
	MARTINIM Total:	87.65							
	Martin Implement Sales In	87.65							
Niles Township Interagency Network									
NILESTOW									
NTIN2015	9/22/2014	20.00	0.00	10/07/2014				False	0
101-300-512-5570 Professional associations				Membership renewal - Social worker					
	NTIN2015 Total:	20.00							
	NILESTOW Total:	20.00							
	Niles Township Interagenc	20.00							
North East Multi-Regional Training									
NORTHEST									
186133	9/17/2014	3,500.00	0.00	10/07/2014				False	0
101-300-512-5590 Training				350 Hour Police Staff & Command					
	186133 Total:	3,500.00							
186442	9/17/2014	255.00	0.00	10/07/2014				False	0
101-300-512-5590 Training				40 hour field training - Officer					
	186442 Total:	255.00							
	NORTHEST Total:	3,755.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	North East Multi-Regional	3,755.00							
Northern Illinois Police Alarm System NTILPALS									
9778	9/20/2014	6.60	0.00	10/07/2014				False	0
101-300-512-5580 Telephone				Language Line - Spanish					
	9778 Total:	6.60							
	NTILPALS Total:	6.60							
	Northern Illinois Police Al	6.60							
Northwest Police Academy NWPDACAD									
NWPA91114	9/11/2014	75.00	0.00	10/07/2014				False	0
101-300-512-5590 Training				Membership renewal - Employee Discipline					
	NWPA91114 Total:	75.00							
	NWPDACAD Total:	75.00							
	Northwest Police Academy	75.00							
Orange Crush LLC ORANGCRH									
469546	9/23/2014	720.39	0.00	10/07/2014				False	0
213-000-561-5490 R&M Road Repairs				Mod surface					
	469546 Total:	720.39							
	ORANGCRH Total:	720.39							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Orange Crush LLC Total:	720.39							
Rudek, Lisa RUDEK 007855-00	9/26/2014	52.97	0.00	10/07/2014				False	0
	660-000-110-1230 Water customer receivables			Refund - Water account payment on sold property					
	007855-00 Total:	52.97							
	RUDEK Total:	52.97							
	Rudek, Lisa Total:	52.97							
Rush Truck Centers of Illinois, Inc RUSHTRUC 95270685	9/22/2014	613.23	0.00	10/07/2014				False	0
	660-620-519-5480 R&M - vehicles			Mirror assembly for Truck #3					
	95270685 Total:	613.23							
95284064	9/23/2014	21.21	0.00	10/07/2014				False	0
	660-620-519-5480 R&M - vehicles			Hot flag for Truck #3					
	95284064 Total:	21.21							
	RUSHTRUC Total:	634.44							
	Rush Truck Centers of Illin	634.44							
Sam's Club SAMSCCL 436	8/8/2014	52.20	0.00	10/07/2014				False	0
	205-530-515-5645 Concessions & food			Camp ice cream					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	436 Total:	52.20							
999999	10/8/2014	120.00	0.00	10/07/2014				False	0
101-210-511-5725	Bank & Credit Card Fees			Annual membership fee					
	999999 Total:	120.00							
CF140821	10/8/2014	50.00	0.00	10/07/2014				False	0
101-210-511-5725	Bank & Credit Card Fees			Annual admin fee					
	CF140821 Total:	50.00							
	SAMSCL Total:	222.20							
	Sam's Club Total:	222.20							
Suburban Laboratories, Inc.									
SUBURB									
115945	9/16/2014	67.50	0.00	10/07/2014				False	0
660-620-519-5320	Consulting			Coliform testing and disinfectant by products					
	115945 Total:	67.50							
	SUBURB Total:	67.50							
	Suburban Laboratories, Inc	67.50							
Warehouse Direct									
WAREHOUS									
2438071-0	9/11/2014	198.12	0.00	10/07/2014				False	0
101-350-512-5730	Program supplies			Office supplies					
	2438071-0 Total:	198.12							
2438139-0	9/11/2014	483.91	0.00	10/07/2014				False	0
101-200-511-5700	Office supplies			Office supplies					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	2438139-0 Total:	483.91							
2438924-0	9/11/2014	99.06	0.00	10/07/2014				False	0
101-300-512-5730	Program supplies			Office supplies					
	2438924-0 Total:	99.06							
2447710-0	9/19/2014	319.30	0.00	10/07/2014				False	0
101-210-511-5700	Office supplies			Office supplies					
	2447710-0 Total:	319.30							
	WAREHOUS Total:	1,100.39							
	Warehouse Direct Total:	1,100.39							
West Central Municipal Confere									
WESTCENT									
0006036-IN	8/31/2014	575.00	0.00	10/07/2014				False	0
101-440-513-5250	Landscaping services			Tree consortium membership fee					
	0006036-IN Total:	575.00							
	WESTCENT Total:	575.00							
	West Central Municipal Co	575.00							
West Payment Center									
WESTPAY									
0829950040	8/3/2014	277.08	0.00	10/07/2014				False	0
101-300-512-5620	Books & publications			Subscription to Search & Seizure Bulletin					
	0829950040 Total:	277.08							
	WESTPAY Total:	277.08							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	West Payment Center Total	277.08							
Westmont Auto Parts WESTMONT									
5271	9/15/2014	155.00	0.00	10/07/2014	Motor and core for Squad #218			False	0
	101-300-512-5480 R&M - vehicles								
	5271 Total:	155.00							
	WESTMONT Total:	155.00							
	Westmont Auto Parts Total	155.00							
Wholesale Direct Inc WHOLESAL									
209535	8/27/2014	15.99	0.00	10/07/2014	Blade for PW truck			False	0
	101-440-513-5480 R&M - vehicles								
	209535 Total:	15.99							
209871	9/12/2014	697.50	0.00	10/07/2014	Rear camera, red lens - Truck #1			False	0
	101-440-513-5480 R&M - vehicles								
	209871 Total:	697.50							
	WHOLESAL Total:	713.49							
	Wholesale Direct Inc Total	713.49							
Woo, Eun Mi WOOEUN									
092414	9/24/2014	45.90	0.00	10/07/2014	Refund - Baseball			False	0
	205-000-210-2430 Parks and Recs Control Deposi								

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number					Description	Reference			

092414 Total:	45.90
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WOOEUN Total:	45.90
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Woo, Eun Mi Total:	45.90
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Report Total:	24,059.24
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Accounts Payable

To Be Paid Proof List

User: jmazzeffi
 Printed: 09/29/2014 - 2:07PM
 Batch: 00105.10.2014



Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description	Reference				
4Imprint									
4IMPRINT									
9618021	9/18/2014	511.65	0.00	10/07/2014				False	0
101-200-511-5799	Other materials & supplies			House measuring key chain for new resident packets					
	9618021 Total:	511.65							
	4IMPRINT Total:	511.65							
	4Imprint Total:	511.65							
ARRP Trucking & Hauling Inc									
ARRP									
7500	9/18/2014	514.00	0.00	10/07/2014				False	0
660-620-519-5599	Other contractual			2 loads dirt hauled out					
	7500 Total:	514.00							
	ARRP Total:	514.00							
	ARRP Trucking & Hauling	514.00							
AT Group , Inc									
ATGROUP									
571	6/25/2014	728.30	0.00	10/07/2014				False	0
101-290-511-5920	Administration Engineer Costs			Retainer & Mileage - June					
571	6/25/2014	728.30	0.00	10/07/2014				False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
660-620-519-5399 Other professional services				Retainer & Mileage - June					
571	6/25/2014	750.00	0.00	10/07/2014				False	0
101-290-511-5942 PW Building Engineer Costs				June Project Management - Crawford - fees					
571	6/25/2014	61.20	0.00	10/07/2014				False	0
101-290-511-5942 PW Building Engineer Costs				June Project Management - Crawford - expenses					
	571 Total:	2,267.80							
	ATGROUP Total:	2,267.80							
	AT Group , Inc Total:	2,267.80							
Best Quality Cleaning, Inc.									
BESTQU									
5932	9/20/2014	2,813.34	0.00	10/07/2014				False	0
101-420-511-5240 Janitorial				Cleaning service - September 2014					
5932	9/20/2014	416.66	0.00	10/07/2014				False	0
205-571-515-5240 Janitorial				Cleaning service - September 2014					
	5932 Total:	3,230.00							
	BESTQU Total:	3,230.00							
	Best Quality Cleaning, Inc.	3,230.00							
Car Quest Auto Parts									
CARQ									
2050-467825	9/17/2014	321.07	0.00	10/07/2014				False	0
101-440-513-5480 R&M - vehicles				Hydraulic fittings					
	2050-467825 Total:	321.07							
2050-467827	9/17/2014	370.13	0.00	10/07/2014				False	0
101-440-513-5480 R&M - vehicles				Hydraulic fittings					
	2050-467827 Total:	370.13							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
		691.20							
		691.20							
Cassidy Tire CASSIDYT 3182648	6/13/2014	-25.00	0.00	10/07/2014				False	0
205-430-515-5480 R&M - vehicles				Credit					
		-25.00							
3182907	7/2/2014	876.94	0.00	10/07/2014				False	0
205-430-515-5480 R&M - vehicles				Tires for Park trailer					
		876.94							
3183917	9/15/2014	921.44	0.00	10/07/2014				False	0
101-440-513-5480 R&M - vehicles				Tires for Truck #1					
		921.44							
		1,773.38							
		1,773.38							
City Welding Sales & Services CITYWELD 31647	5/13/2014	32.70	0.00	10/07/2014				False	0
101-410-511-5730 Program supplies				Carbon dioxide for shop					
		32.70							
		32.70							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	City Welding Sales & Serv	32.70							
COM2 Recycling Solutions									
COM2									
30383	9/23/2014	150.00	0.00	10/07/2014				False	0
101-250-511-5599	Other contractual			Pickup charge					
	30383 Total:	150.00							
	COM2 Total:	150.00							
	COM2 Recycling Solution	150.00							
Health Endeavors									
HEALTHEN									
3755	6/26/2014	1,358.00	0.00	10/07/2014				False	0
101-200-511-5599	Other contractual			Annual screenings for Public Works					
	3755 Total:	1,358.00							
	HEALTHEN Total:	1,358.00							
	Health Endeavors Total:	1,358.00							
Lincolnwood Chamber of Commerce & Industry									
LWDCHAMB									
092314	9/23/2014	10.00	0.00	10/07/2014				False	0
101-200-511-5840	Meals			Chamber of Commerce business breakfast connections					
	092314 Total:	10.00							
	LWDCHAMB Total:	10.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Lincolnwood Chamber of C	10.00							
Lowe's Business Acc/GECF									
LOWES									
1065	9/4/2014	140.56	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings			Electrical outlet for Village Hall					
	1065 Total:	140.56							
1156	9/5/2014	209.74	0.00	10/07/2014				False	0
	101-440-513-5730 Program supplies			Tap wrench, drill bit pack, weed killer					
	1156 Total:	209.74							
1287	9/10/2014	38.73	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings			Copper wire for PD					
	1287 Total:	38.73							
14327	9/19/2014	-23.75	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings			Rental deposit return					
	14327 Total:	-23.75							
16211	9/3/2014	-17.08	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings			Return					
	16211 Total:	-17.08							
16212	9/3/2014	34.16	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings			Smooth hammer for Village Hall					
	16212 Total:	34.16							
1633	8/28/2014	26.44	0.00	10/07/2014				False	0
	101-410-511-5730 Program supplies			Mops and spray bottle for shop					
	1633 Total:	26.44							
17851	9/17/2014	66.48	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings			Bissell rental for PD					

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	17851 Total:	66.48							
19053	9/18/2014	142.05	0.00	10/07/2014				False	0
	205-430-515-5680 Landscaping supplies				LED flood light bulb, peat moss for PD				
19053	9/18/2014	51.24	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings				LED flood light bulb, peat moss for PD				
	19053 Total:	193.29							
1992	9/3/2014	45.52	0.00	10/07/2014				False	0
	101-440-513-5730 Program supplies				Heavy duty toters for forestry dept				
	1992 Total:	45.52							
2014	8/21/2014	55.99	0.00	10/07/2014				False	0
	101-410-511-5730 Program supplies				Tape measure, hose for Shop				
	2014 Total:	55.99							
2097	9/18/2014	304.11	0.00	10/07/2014				False	0
	205-430-515-5680 Landscaping supplies				Peat moss for Proesel Park				
	2097 Total:	304.11							
2098	9/18/2014	-25.11	0.00	10/07/2014				False	0
	205-430-515-5680 Landscaping supplies				Tax refund				
	2098 Total:	-25.11							
2269	9/11/2014	41.14	0.00	10/07/2014				False	0
	101-410-511-5730 Program supplies				Plug fitting, reflective stickers for Shop				
	2269 Total:	41.14							
2353	9/11/2014	138.18	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings				Electrical box, wire, conduit body for VH				
	2353 Total:	138.18							
2415	9/12/2014	47.52	0.00	10/07/2014				False	0
	101-440-513-5480 R&M - vehicles				Spray paints for PW truck hinges				

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	2415 Total:	47.52							
2638	9/4/2014	94.90	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings				Track light kit, LED flood lights for PD				
	2638 Total:	94.90							
2700	9/5/2014	120.75	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings				Electrical box, dimmer, wires for PD				
	2700 Total:	120.75							
2702	9/15/2014	35.70	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings				Phone line cord, drill bit for Village Hall				
	2702 Total:	35.70							
2727	9/15/2014	4.73	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings				Carpet tape for Village Hall				
	2727 Total:	4.73							
2851	9/16/2014	22.20	0.00	10/07/2014				False	0
	205-430-515-5730 Program supplies				Rope, stakes for Proesel Park				
	2851 Total:	22.20							
2867	9/16/2014	17.01	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings				Electrical box, electrical square for PD				
	2867 Total:	17.01							
2947	9/17/2014	48.04	0.00	10/07/2014				False	0
	205-430-515-5730 Program supplies				Rope, spike for Proesel Park				
	2947 Total:	48.04							
2983	9/17/2014	4.14	0.00	10/07/2014				False	0
	101-420-511-5405 R&M - buildings				Bolts, lock washer for PD				
	2983 Total:	4.14							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
3919	9/12/2014	14.21	0.00	10/07/2014				False	0
101-420-511-5405 R&M - buildings				Drill bit, anchors for buildings					
	3919 Total:	14.21							
7820	9/5/2014	56.94	0.00	10/07/2014				False	0
101-440-513-5730 Program supplies				Masking tape for streets					
	7820 Total:	56.94							
	LOWES Total:	1,694.54							
	Lowe's Business Acc/GEC	1,694.54							
Marc Printing MARCP									
092914	9/29/2014	961.87	0.00	10/07/2014				False	0
660-610-519-5720 Postage				Postage-water bills, Cycle 1 & 4					
	092914 Total:	961.87							
	MARCP Total:	961.87							
	Marc Printing Total:	961.87							
Misericordia Holiday Selections MISERICO									
MIS09242014	9/24/2014	166.00	0.00	10/07/2014				False	0
101-100-511-5560 Printing & copying services				Holiday Greeting cards 2014					
	MIS09242014 Total:	166.00							
	MISERICO Total:	166.00							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Misericordia Holiday Selec	166.00							
Orange Crush LLC ORANGCRH 468970	9/18/2014	782.93	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod surface					
	468970 Total:	782.93							
469109	9/19/2014	1,147.05	0.00	10/07/2014				False	0
	213-000-561-5490 R&M Road Repairs			Mod surface					
	469109 Total:	1,147.05							
	ORANGCRH Total:	1,929.98							
	Orange Crush LLC Total:	1,929.98							
PEP Boys PEPBOYS 04791007043	2/12/2014	16.97	0.00	10/07/2014				False	0
	101-350-512-5740 Repair parts			Grease, cartridge					
	04791007043 Total:	16.97							
04791007865	9/3/2014	59.95	0.00	10/07/2014				False	0
	101-350-512-5740 Repair parts			Fuel tank repair kit, metal tank					
	04791007865 Total:	59.95							
04791019167	9/25/2014	13.90	0.00	10/07/2014				False	0
	101-350-512-5740 Repair parts			Brake pads, fluid					
	04791019167 Total:	13.90							
	PEPBOYS Total:	90.82							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
		90.82							
	PEP Boys Total:								
Pioneer Press PIONEERP 3053449	9/18/2014	32.00	0.00	10/07/2014				False	0
101-350-512-5620 Books & publications				Subscription					
	3053449 Total:	32.00							
	PIONEERP Total:	32.00							
	Pioneer Press Total:	32.00							
Regional Emergency Dispatch REGIONAL 222-15-10	9/15/2014	11,797.83	0.00	10/07/2014				False	0
101-350-512-5599 Other contractual				October 2014 dues					
	222-15-10 Total:	11,797.83							
	REGIONAL Total:	11,797.83							
	Regional Emergency Dispa	11,797.83							
Sam's Club SAMSCCL 001334	9/23/2014	577.67	0.00	10/07/2014				False	0
101-350-512-5730 Program supplies				Brat fest supplies					
	001334 Total:	577.67							
	SAMSCCL Total:	577.67							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
	Sam's Club Total:	577.67							
Sherill Inc. SHERILL INV-307459 101-440-513-5745 Small tools	9/12/2014	1,572.65	0.00	10/07/2014	Helmet, face shield, wedge, measure tape			False	0
	INV-307459 Total:	1,572.65							
INV-307566 101-440-513-5745 Small tools	9/16/2014	74.95	0.00	10/07/2014	Laser point with pouch			False	0
	INV-307566 Total:	74.95							
	SHERILL Total:	1,647.60							
	Sherill Inc. Total:	1,647.60							
Traffic Control & Protection TRAFFICC 81238 101-440-513-5768 Street materials - signs & bar	9/15/2014	732.45	0.00	10/07/2014	Speed limit signs and No Outlet signs			False	0
	81238 Total:	732.45							
	TRAFFICC Total:	732.45							
	Traffic Control & Protectio	732.45							
Verizon Wireless VERIZON 9732171635 101-210-511-5580 Telephone	9/6/2014	2,038.91	0.00	10/07/2014	Wireless phones - Aug 17 thru Sept 16			False	0
9732171635 205-508-515-5580 Telephone	9/6/2014	80.84	0.00	10/07/2014	Wireless phones - Aug 17 thru Sept 16			False	0

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
9732171635	9/6/2014	39.53	0.00	10/07/2014				False	0
205-520-515-5580 Telephone				Wireless phones - Aug 17 thru Sept 16					
9732171635	9/6/2014	184.02	0.00	10/07/2014				False	0
205-530-515-5580 Telephone				Wireless phones - Aug 17 thru Sept 16					
9732171635	9/6/2014	44.43	0.00	10/07/2014				False	0
205-560-515-5580 Telephone				Wireless phones - Aug 17 thru Sept 16					
9732171635	9/6/2014	0.20	0.00	10/07/2014				False	0
205-550-515-5270 Purchased program services				Wireless phones - Aug 17 thru Sept 16					
9732171635	9/6/2014	126.78	0.00	10/07/2014				False	0
101-000-210-2650 Contractor Permits Payable				Wireless phones - Aug 17 thru Sept 16					
9732171635	9/6/2014	949.58	0.00	10/07/2014				False	0
660-610-519-5580 Telephone				Wireless phones - Aug 17 thru Sept 16					
9732171635	9/6/2014	650.70	0.00	10/07/2014				False	0
101-250-511-5580 Telephone				Wireless phones - Aug 17 thru Sept 16					
	9732171635 Total:	4,114.99							
	VERIZON Total:	4,114.99							
	Verizon Wireless Total:	4,114.99							
Work' N Gear, LLC									
WRKNGEAR									
HA34033	9/18/2014	249.96	0.00	10/07/2014				False	0
101-440-513-5070 Uniform allowance				Clothing allowance					
	HA34033 Total:	249.96							
HA34034	9/18/2014	39.98	0.00	10/07/2014				False	0
101-440-513-5070 Uniform allowance				Clothing allowance					
	HA34034 Total:	39.98							
	WRKNGEAR Total:	289.94							
	Work' N Gear, LLC Total:	289.94							

Invoice Number	Invoice Date	Amount	Quantity	Payment Date	Task Label	Type	PO #	Close PO	Line #
Account Number				Description		Reference			
Worldpoint ECC WORLDPOI 5440265 101-350-512-5660 EMS supplies	9/18/2014	66.20	0.00	10/07/2014	CPR face shields			False	0
5440265 Total:		66.20							
WORLDPOI Total:		66.20							
Worldpoint ECC Total:		66.20							
Report Total:		34,640.62							

Request For Board Action

REFERRED TO BOARD: October 7, 2014

AGENDA ITEM NO: 1

ORIGINATING DEPARTMENT: Police

SUBJECT: Approval of a Resolution Authorizing the Village Manager to Execute a Renewal Agreement with the Lincolnwood Town Center, LLC, Inc. to Regulate Parking and Traffic within the Parking Lots

SUMMARY AND BACKGROUND OF SUBJECT MATTER:

In 1994, the Village Board approved an Ordinance authorizing the execution of a parking and traffic control agreement for the property commonly referred to as Lincolnwood Town Center. The original agreement was for 20 years and expired in August.

The agreement has allowed the Village to regulate the parking of motor vehicles and vehicular traffic within parking lots, pursuant to Section 11-209 of the Illinois Vehicle Code, 625 Illinois Compiled Statutes (ILCS) 5/11-209. The agreement has proven to be an effective tool to facilitate the safe and efficient movement of traffic for the past 20 years.

Staff recommends the Board authorize the Village Manager to execute a renewal agreement for 20 years with the Lincolnwood Town Center, LLC.

FINANCIAL IMPACT:

None

DOCUMENTS ATTACHED:

1. Proposed Resolution
2. Traffic and Parking Agreement
3. Lincolnwood Town Center Legal Description
4. 1994 Ordinance Authorizing the Execution of a Parking and Traffic Control Agreement

RECOMMENDED MOTION:

Move to approve a Resolution authorizing the Village Manager to execute a 20 year renewal agreement with Lincolnwood Town Center, LLC, Inc. to regulate parking and traffic within the parking lots.

VILLAGE OF LINCOLNWOOD

RESOLUTION NO. R2014-_____

A RESOLUTION APPROVING A
PARKING AND TRAFFIC ENFORCEMENT AGREEMENT
FOR THE LINCOLNWOOD TOWN CENTER

WHEREAS, Lincolnwood Town Center, LLC, a Delaware limited liability company ("**Owner**"), is the record title owner of the Lincolnwood Town Center in the Village, and other adjacent commercial properties in the immediate vicinity of the Lincolnwood Town Center (collectively, the "**Property**"); and

WHEREAS, pursuant to Section 11-209 of the Illinois Vehicle Code, 625 ILCS 5/11-209, the Village has the authority to enter into agreements with owners of commercial and industrial facilities, shopping centers, apartment complexes and other owners of property that contain a parking area located within the limits of the Village to regulate parking and traffic in such parking areas; and

WHEREAS, in 1994, pursuant to the Illinois Vehicle Code, the Village entered into an agreement with the then-owner of the Property for the regulation by the Village of parking and traffic at the Property ("**1994 Agreement**"); and

WHEREAS, the 1994 Agreement has expired; and

WHEREAS, pursuant to Section 11-209 of the Vehicle Code and the home rule powers of the Village, the Village and the Owner now desire to enter into a new agreement for the regulation by the Village of parking and traffic at the Property ("**New Agreement**"); and

WHEREAS, the Village President and Board of Trustees have determined that entering into the New Agreement with the Owner will serve and be in the best interest of the Village;

NOW, THEREFORE, BE IT RESOLVED BY THE PRESIDENT AND BOARD OF TRUSTEES OF THE VILLAGE OF LINCOLNWOOD, COOK COUNTY, ILLINOIS, as follows:

SECTION 1. RECITALS. The facts and statements contained in the preamble to this Resolution are found to be true and correct and are hereby adopted as part of this Resolution.

SECTION 2. APPROVAL OF NEW AGREEMENT. The New Agreement by and between the Village and the Owner is hereby approved in substantially the form attached to this Resolution as **Exhibit A**.

SECTION 3. EXECUTION OF NEW AGREEMENT. The Village Manager and the Village Clerk are hereby authorized and directed to execute and attest, on behalf of the Village, the New Agreement upon receipt by the Village Clerk of at least one original copy of the New Agreement executed by the Owner; provided, however, that if the executed copy of the New

Agreement is not received by the Village Clerk within 30 days after the effective date of this Resolution, then this authority to execute and attest will, at the option of the President and Board of Trustees, be null and void.

SECTION 4. EFFECTIVE DATE. This Resolution will be in full force and effect from and after its passage and approval as provided by law.

PASSED this ___ day of _____, 2014.

AYES: _____

NAYS: _____

ABSENT: _____

ABSTENTION: _____

APPROVED by me this _____ day of _____, 2014.

Gerald C. Turry, President
Village of Lincolnwood, Cook County, Illinois

ATTESTED and FILED in my office this
_____ day of _____, 2014

Beryl Herman, Village Clerk
Village of Lincolnwood, Cook County, Illinois

EXHIBIT A
NEW AGREEMENT

TRAFFIC AND PARKING ENFORCEMENT AGREEMENT

This Traffic and Parking Enforcement Agreement ("**Agreement**") is made and entered into as of the ____ day of _____, 2014 ("**Effective Date**") between and among the **VILLAGE OF LINCOLNWOOD**, an Illinois home rule municipal corporation ("**Village**"), and **LINCOLNWOOD TOWN CENTER, LLC**, a Delaware limited liability company ("**Owner**").

SECTION 1. RECITALS.

A. Owner is the record title owner of that certain tract of land, consisting of approximately 31 acres, located at 3333 West Touhy, in Lincolnwood, Illinois, and legally described in **Exhibit A** attached to this Agreement ("**Property**").

B. The Property is currently improved with a shopping center building and numerous outlet retail and commercial buildings, collectively and commonly known as the Lincolnwood Town Center, and with vehicular parking lots that serve the Lincolnwood Town Center (collectively, the "**Parking Lots**").

C. Owner desires to enter into an agreement with the Village in order to empower the Village to regulate the parking of motor vehicles and vehicular traffic within the Parking Lots, pursuant to the provisions of Section 11-209 of the Illinois Vehicle Code, 625 ILCS 5/11-209.

D. The Village and Owner desire to enter into this Agreement to set forth their rights and responsibilities regarding the enforcement of traffic and parking regulations by the Village within the Parking Lots.

SECTION 2. REGULATION OF TRAFFIC AND PARKING.

A. Enforcement by Village. Owner hereby authorizes the Village, and the Village hereby agrees, to enforce within the Parking Lots all generally-applicable traffic and parking regulations set forth in the Municipal Code of Lincolnwood, as may be amended from time to time. Specifically, and without limitation of the foregoing, the Village is hereby authorized and empowered to do each of the following within the Parking Lots, in compliance with all applicable Federal, State, and local laws, statutes, and regulations (including, without limitation, the Americans with Disabilities Act):

1. Erect stop signs, flashing signals, handicapped parking area signs or yield signs, and adopt appropriate regulations pertaining thereto or pertaining to the designation of any intersection in the Parking Lots as a stop intersection or as a yield intersection and the ordering of like signs or signals at one or more entrances to said intersection;

2. Prohibit and regulate the turning of vehicles or specified types of vehicles at intersections or other locations;

3. Regulate the crossing of any roadway in the Parking Lots by pedestrians;

4. Designate any separate roadway on the Property for one-way traffic;

5. Establish and regulate loading zones;

6. Prohibit, regulate, restrict or limit stopping, standing or parking of vehicles in specified areas on the Property;

7. Designate safety zones and fire lanes in the Parking Lots;

8. Remove and store vehicles parked or abandoned in the Parking Lots during snow storms, floods, fires and other public emergencies or found unattended in the Parking Lots, where such vehicles constitute an obstruction to traffic, or where stopping, standing or parking is prohibited; provided, however, that the cost of any such removal or storage shall be borne by the owner or operator of such vehicles;

9. Install signs designating the reservation of specified parking spaces for persons with disabilities; and

10. Establish and adopt additional reasonable rules and regulations for the respect of traffic and parking on the Property as local conditions may require for the safety and convenience of the public or the users of the Property.

B. License Granted. Owner hereby grants a non-exclusive License to the Village, and to its authorized officials, officers, employees, agents, and representatives, to enter the Property for the purpose of performing the traffic and parking enforcement authorized pursuant to this Agreement.

C. Limitation of Service. Owner acknowledges and agrees that the Village, in the exercise of its sole discretion, may determine when the press of regular police business shall preclude the furnishing of traffic and parking enforcement pursuant to this Agreement.

SECTION 3. COSTS.

Owner acknowledges and agrees that it is solely responsible, and that the Village will have no obligation, for payment of the costs of installation and maintenance of all traffic control or parking signs or devices located on the Property and used in connection with the traffic and parking enforcement contemplated by this Agreement. In the event that the Village incurs any such costs, Owner must reimburse Village for such costs within 30 days after receipt of an invoice therefor.

SECTION 4. MAINTENANCE.

Except as specifically provided to the contrary in this Agreement, Owner is responsible, at its sole cost and expense, to maintain the Parking Lots and the Property in a safe condition and in compliance with all applicable laws.

SECTION 5. TERM.

This Agreement is for a 20-year term, beginning on the Effective Date. Either Party may terminate this Agreement at any time, with or without cause, by delivering written notice to the other Party not less than 30 days prior to the date of termination.

SECTION 6. RELEASE; INDEMNIFICATION.

A. Release. Owner hereby releases the Village and its officials, officers, employees, agents, attorneys, and representatives in connection with any and all actions or claims for any loss or damage to the Parking Lots or the Property, or any other property of Owner, as a consequence of the performance of this Agreement.

B. Indemnification. Owner shall, without regard to the availability or unavailability of any insurance of the Village, indemnify the Village and its respective officers, members, officials, employees, agents, attorneys, and representatives against, and hold the Village and its respective officers, members, officials, employees, agents, attorneys, and representatives, harmless from, any and all lawsuits, claims, demands, damages, liabilities, losses, and expenses (including attorneys' fees and administrative expenses), that may be sustained or incurred as a result of or in connection with the Village's enforcement of traffic and parking regulations on the Property pursuant to this Agreement, or with Owner's performance of, or failure to perform, its obligations under this Agreement, whether or not due or claimed to be due in whole or in part to the active, passive, or concurrent negligence or willful misconduct of Owner, but not including lawsuits, claims, demands, damages, liabilities, losses, and expenses (including attorneys' fees and administrative expenses) sustained or incurred solely as a result of the gross negligence or willful misconduct of the Village. Owner's obligations required pursuant to this Section 6.B shall survive the termination of this Agreement.

SECTION 7. ENFORCEMENT.

A. General. The Parties may, in law or in equity, by suit, action, mandamus, or any other proceeding, enforce this Agreement. Owner agrees that: (1) it will not seek, and do not have the right to seek, to recover a judgment for monetary damages against the Village or any elected or appointed officials, officers, employees, agents, representatives, engineers, or attorneys of the Village, on account of the negotiation, execution or breach of any of the terms and conditions of this Agreement; and (2) the sole remedy available to Owner, upon any breach of this Agreement by the Village, is the termination of this Agreement under its terms.

B. Prevailing Party. In the event of a judicial proceeding brought by one or more Parking against one or more other Parties, the prevailing Party or Parties in such judicial proceeding shall be entitled to reimbursement from the unsuccessful Party or Parties of all costs and expenses, including reasonable attorneys' fees, incurred in connection with such judicial proceeding.

SECTION 8. OWNER WARRANTIES.

Owner hereby represents and warrants to the Village, to the best of its knowledge and information as of the date of this Agreement, as follows:

A. Owner has the authority and the legal right to make, deliver, and perform this Agreement, and has taken or will take all necessary actions and obtain all required consents and approvals to authorize the execution, delivery, and performance of this Agreement.

B. This Agreement constitutes the legal, valid, and binding obligation of Owner and is enforceable against Owner in accordance with its terms.

C. To the best of the knowledge of the legal representatives of Owner, the execution, delivery, and performance of this Agreement is not prohibited by or in conflict with any requirement of law or under any contractual obligation of Owner, will not result in a breach

or default under any agreement to which Owner is a party or is bound, and will not violate any restriction, court order, or agreement to which Owner is subject.

D. The parties executing this Agreement on behalf of Owner and executing and delivering any other agreement or other item contemplated by this Agreement or otherwise required to fulfill Owner's obligations under this Agreement, have full authority to bind Owner to those obligations and to act on behalf of Owner.

SECTION 9. GENERAL PROVISIONS.

A. Notices. All notices and payments required or permitted to be given under this Agreement shall be given by the Parties by (i) personal delivery, (ii) deposit in the United States mail, enclosed in a sealed envelope with first class postage thereon, or (iii) deposit with a nationally recognized overnight delivery service, addressed as stated in this Section 9.A. The address of either Party may be changed by written notice to the other Party. Any mailed notice shall be deemed to have been given and received within three days after the same has been mailed and any notice given by overnight courier shall be deemed to have been given and received within 24 hours after deposit.

Notices and communications to the Parties shall be addressed to, and delivered at, the following address:

If to the Village:	Village of Lincolnwood 6900 North Lincoln Avenue Lincolnwood, IL 60712 Attention: Chief of Police
If to Owner:	_____ _____ _____ Attention: _____

B. Time of the Essence. Time is of the essence in the performance of all terms, covenants, and conditions of this Agreement.

C. Consents. Whenever the consent or approval of either party is required in this Agreement, such consent or approval shall be in writing and shall not be unreasonably withheld or delayed, and, in all matters contained herein, both parties shall have an implied obligation of reasonableness, except as may be expressly set forth otherwise.

D. Exhibits. Exhibit A attached to this Agreement is, by this reference, incorporated in and made a part of this Agreement. In the event of a conflict between the exhibit and the text of this Agreement, the text of this Agreement shall control.

E. Amendments and Modifications. No amendment or modification to this Agreement shall be effective unless and until it is reduced to writing and approved and executed by the parties to this Agreement in accordance with all applicable statutory procedures.

F. No Joint Venture. It is hereby understood and agreed that nothing contained in this Agreement shall be deemed or construed as creating the relationship of principal and agent, partnership or joint venture between the parties to this Agreement, it being agreed that no

provision of this Agreement and no acts of the parties to this Agreement shall be deemed to create any relationship between the parties other than the relationship set forth specifically by the terms of this Agreement.

G. Entire Agreement. This Agreement constitutes the entire agreement between the parties to this Agreement, and supersedes all prior agreements and negotiations between the parties, whether written or oral, relating to the specific subject matter of this Agreement.

H. No Third Party Beneficiaries. No claim as a third party beneficiary under this Agreement by any person, firm, or corporation shall be made, or be valid, against the Village or Owner.

I. Recording. The Parties acknowledge and agree that a copy of this Agreement will be recorded in the Office of the Cook County Recorder of Deeds.

IN WITNESS WHEREOF, the parties have caused this Agreement to be executed by their duly authorized representatives as of the date first above written.

ATTEST:

VILLAGE OF LINCOLNWOOD, an Illinois home rule municipal corporation

By: _____
Beryl Herman, Village Clerk

By: _____
Timothy Wiberg, Village Manager

ATTEST:

LINCOLNWOOD TOWN CENTER, LLC, a Delaware limited liability company

By: _____

By: _____

Title: _____

Its: _____

ACKNOWLEDGMENTS

STATE OF ILLINOIS)
) SS.
COUNTY OF LAKE)

 This instrument was acknowledged before me on _____, 2014, by _____, the Village Manager of the **VILLAGE OF LINCOLNWOOD**, an Illinois home rule municipal corporation, and by _____, the Village Clerk of said municipal corporation.

 Given under my hand and official seal this ____ day of _____, 2014.

Notary Public

My Commission expires: _____

SEAL

STATE OF ILLINOIS)
) SS.
COUNTY OF LAKE)

 This instrument was acknowledged before me on _____, 2014, by _____, the _____ of **LINCOLNWOOD TOWN CENTER, LLC**, a Delaware limited liability company, and by _____, the _____ of said limited liability company.

 Given under my hand and official seal this ____ day of _____, 2014.

Notary Public

My Commission expires: _____

SEAL

EXHIBIT A

LEGAL DESCRIPTION OF THE PROPERTY

LINCOLNWOOD TOWNE CENTER
DEVELOPERS PARCEL
30.83 ACRES

A tract of land in the North Half of Section 35, Township 41 North, Range 13 East of the Third Principal Meridian, bounded and described as follows:

Commencing at the intersection of the West line of the East 660.00 feet of the Northeast Quarter of said Section 35 and the South line of the North Half of the South Half of the Northeast Quarter of said Section 35;

Thence North 00°03'49" West 418.00 feet along last said West line; thence North 45°01'55" West 42.45 feet; thence North 00°03'49" West 100.00 feet to the Point of Beginning;

thence South 89°56'11" West, a distance of 230.00 feet to a point of curvature;

thence 142.81 feet along the arc of a curve, deflecting to the right, having a delta of 43°03'51", a radius of 190.00 feet and a chord bearing and distance of North 68°31'53" West, 139.49 feet to the end of said curve;

thence North 45°55'00" West, a distance of 68.00 feet to a point of curvature;

thence 152.02 feet along the arc of a curve, deflecting to the right, having a delta of 45°50'37", a radius of 190.00 feet and a chord bearing and distance of North 22°59'42" West, 148.00 feet to the end of said curve;

thence North 00°04'24" West, a distance of 5.00 feet;

thence North 44°55'36" East, a distance of 21.21 feet;

thence South 89°55'36" West, a distance of 587.00 feet;

thence North 00°00'00" East, a distance of 95.00 feet;

thence South 90°00'00" West, a distance of 262.00 feet;

thence North 00°00'00" East, a distance of 248.96 feet;

thence North 39°33'48" West, a distance of 99.72 feet;

thence North 50°26'12" East, a distance of 11.59 feet to a point of curvature;

thence 225.05 feet along the arc of a curve, deflecting to the left, having a delta of 28°36'16", a radius of 450.78 feet and a chord bearing and distance of North 36°08'09" East, 222.72 feet to the end of said curve;

thence North 21°50'01" East, a distance of 422.85 feet;

thence North 88°20'30" East, a distance of 315.64 feet;

thence North 01°39'30" West, a distance of 82.00 feet;

thence North 88°20'30" East, a distance of 120.00 feet;

thence North 01°39'30" West, a distance of 14.00 feet;

thence North 88°20'30" East, a distance of 317.49 feet;

thence South 01°39'30" East, a distance of 152.00 feet;

thence South 45°56'21" East, a distance of 216.13 feet;

thence North 89°56'11" East, a distance of 172.00 feet;

thence South 00°03'49" East, a distance of 328.00 feet;

thence North 89°56'11" East, a distance of 13.50 feet;

thence South 00°03'49" East, a distance of 385.43 feet;

thence South 89°56'11" West, a distance of 13.50 feet;

thence South 00°03'49" East, a distance of 340.00 feet;

thence South 44°56'11" West a distance 23.33 feet to the Point of Beginning.

This parcel contains 30.83 acres of land, more or less.

SUBJECT to an actual field survey to be performed by a licensed land surveyor, licensed in the State where this transaction is taking place, and any and all existing covenants, conveyances, easements and rights-of-way of record.

10-35-201-010 *marking*
SW Corner of Tarky Ave + McCormick Blvd
Lincolnwood, IL 60112

thence 225.05 feet along the arc of a curve, deflecting to the left, having a delta of 28°36'16", a radius of 450.78 feet and a chord bearing and distance of North 36°08'09" East, 222.72 feet to the end of said curve;

thence North 21°50'01" East, a distance of 422.85 feet;

thence North 88°20'30" East, a distance of 315.64 feet;

thence North 01°39'30" West, a distance of 82.00 feet;

thence North 88°20'30" East, a distance of 120.00 feet;

thence North 01°39'30" West, a distance of 14.00 feet;

thence North 88°20'30" East, a distance of 317.49 feet;

thence South 01°39'30" East, a distance of 152.00 feet;

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thence South 00°03'49" East, a distance of 328.00 feet;

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thence South 00°03'49" East, a distance of 385.43 feet;

thence South 89°56'11" West, a distance of 13.50 feet;

thence South 00°03'49" East, a distance of 340.00 feet;

thence South 44°56'11" West a distance 23.33 feet to the Point of Beginning.

This parcel contains 30.83 acres of land, more or less.

SUBJECT to an actual field survey to be performed by a licensed land surveyor, licensed in the State where this transaction is taking place, and any and all existing covenants, conveyances, easements and rights-of-way of record.

VILLAGE OF LINCOLNWOOD

ORDINANCE NO. 94-2160

AN ORDINANCE AUTHORIZING THE EXECUTION OF A PARKING AND
TRAFFIC CONTROL AGREEMENT FOR THE PROPERTY COMMONLY REFERRED TO
AS LINCOLNWOOD TOWN CENTER

ADOPTED BY THE
PRESIDENT AND BOARD OF TRUSTEES
OF THE
VILLAGE OF LINCOLNWOOD

THIS 18th DAY OF August, 1994

Published in pamphlet form by
the authority of the President
and Board of Trustees of the
Village of Lincolnwood, Cook
County, Illinois,

this _____ day of _____ 1994.

ORDINANCE NO. 94-2160

AN ORDINANCE AUTHORIZING THE EXECUTION OF A PARKING AND TRAFFIC CONTROL AGREEMENT FOR THE PROPERTY COMMONLY REFERRED TO AS LINCOLNWOOD TOWN CENTER

WHEREAS, pursuant to the Illinois Motor Vehicle Code, Illinois Compiled Statutes, 1992, Chapter 625, Act 5, Sections 11-209, 11-209.1 and 11-210, the corporate authorities of any municipality have the authority to contract with the owner of certain parking areas located within the limits of the municipality for the purpose of regulating the parking of automobiles and traffic flow of motor vehicles within said parking areas; and,

WHEREAS, Lincolnwood Town Center is located completely within the corporate limits of the Village of Lincolnwood, Cook County, Illinois; and,

WHEREAS, Simon Property Group, L.P., a Delaware limited partnership, as owner authorized to enter into a contract with the Village for the regulation of traffic and parking within the parking area of the shopping center; and,

WHEREAS, the owner has negotiated an agreement with the Village of Lincolnwood, whereby the Village may adopt regulations governing motor vehicle traffic in the parking area of the shopping center, which agreement sets forth the responsibilities and obligations of the Village and owner with respect to the parking area; and,

WHEREAS, it is deemed to be in the best interest of the health, safety and welfare of the residents of the Village of Lincolnwood to execute said parking and traffic control agreement.

NOW, THEREFORE, BE IT ORDAINED BY THE PRESIDENT AND BOARD OF TRUSTEES OF THE VILLAGE OF LINCOLNWOOD, COOK COUNTY, ILLINOIS, as follows:

SECTION 1: That the Village of Lincolnwood does hereby enter into the parking and traffic control agreement attached hereto and made a part hereof. That the President and Clerk of the Village of Lincolnwood are hereby authorized to execute and attest to said agreement.

SECTION 2: That the parking and traffic restrictions and designations as indicated on the plan attached to and made part of the agreement are hereby incorporated as regulations governing the parking and traffic control of the parking area as described within the agreement. Violations of said regulations shall be deemed offenses punishable in the same manner as other parking and traffic offenses within the Village of Lincolnwood.

SECTION 3: This Ordinance shall be in full force and effect from and after its passage, approval and publication in pamphlet form as provided by law.

PASSED this 18th day of August, 1994.

AYES: Trustees Coban, Bender, Schwartz and Abelson

NAYS: none

ABSENT: Trustees Angarola and Levine

APPROVED this _____ day of _____ 1994.

VILLAGE OF LINCOLNWOOD

By: _____
Village President

(cont'd)

SIMON PROPERTY GROUP, L.P.
a Delaware limited partnership
d/b/a/ Simon Real Estate Group

By: _____
Simon Property Group, Inc.
a Maryland Corporation, its
General Partner

ATTEST:

Village Clerk

FILED IN THE VILLAGE CLERK'S OFFICE
ON THE 23rd DAY OF Aug. 1997
B. Kumbel VILLAGE CLERK

AGREEMENT

THIS AGREEMENT entered into this _____ day of _____
19 ____, by and between the Village of Lincolnwood, Cook County,
Illinois, a municipal corporation (hereinafter referred to as
"Village") and Simon Property Group, L.P., a Delaware limited
partnership, (hereinafter referred to as "Owner").

WITNESSETH

WHEREAS, pursuant to the Illinois Motor Vehicle Code, Illinois
Compiled Statutes, Chapter 625, Act 5, Sections 11-209, 11-209.1 and
11.210, the corporate authorities of any municipality have the power
of regulating the parking of automobiles and traffic flow of motor
vehicles in said shopping center parking area; and,

WHEREAS, owner owns that certain shopping center commonly known as
Lincolnwood Town Center (hereinafter referred to as the "Center")
which is located completely within the corporate limits of the Village
of Lincolnwood, Cook County, Illinois; and,

WHEREAS, owner owns and controls a parking area within the Center
and,

WHEREAS, for purposes of this Agreement, the term "parking area"
of the shopping center shall include the area or areas of land near or
contiguous to the shopping center, including interior private roadways
and drives used by the public as a means of ingress and egress to and
from the buildings and stores and business establishments within the
shopping center and for the parking of motor vehicles; and,

WHEREAS, a certain plan and drawing has been prepared for the coordination and control of parking and traffic within the parking area, said document being a plan dated _____ and designated Exhibit "A" and incorporated herein by reference, a true and correct copy of which is available for inspection at the Office of the Village Clerk of the Village of Lincolnwood, Cook County, Illinois; and

WHEREAS, Village and owner have determined that in order to promote the safety and welfare of the residents of the Village it is necessary and desirable that the Village establish regulations governing the use of motor vehicles in the parking area of the shopping center and that police officers of the Village be authorized to enter the parking area of the shopping center to enforce those regulations and to remove and store vehicles in violation of those regulations.

NOW, THEREFORE, IN CONSIDERATION OF THE MUTUAL UNDERTAKINGS HEREIN SET FORTH, it is hereby agreed as follows:

SECTION 1: Village may adopt regulations governing motor vehicle traffic in the parking area of the shopping center.

SECTION 2: The Lincolnwood Code of Ordinances may be enforced on the property of the shopping center.

SECTION 3: The parking area of the shopping center shall be designated as shown in the plan prepared for the coordination and control of traffic as herein before described.

SECTION 4: The Village is granted a license to enter the parking area of the Center for the purpose of carrying out the terms of this Agreement.

SECTION 5: For the purpose of regulating parking and traffic in the parking area of the shopping center, this Agreement specifically empowers the Village to exercise its police powers within the parking area as follows:

1. The erection of stop signs, flashing signals, handicapped parking area signs, yield signs and the placement of other road markings in the parking area at the locations and intersections designated on the plan, and the adoption of appropriate regulations pertaining thereto, subject to the provisions of the Illinois Vehicle Code.
2. The prohibition or regulation of the turning of vehicles or specified types of vehicles at intersections or other locations in the parking areas designated on the plan.
3. The regulation of a crossing of any roadway in the parking area by pedestrians at the locations designated on the plan.
4. The adoption of appropriate parking and traffic control regulations with respect to any separate roadway, drive or aisle in the parking area designated for one-way traffic on the plan.
5. The adoption of appropriate parking and traffic control regulations with respect to the loading zones and service areas in the parking area designated on the plan.
6. The prohibition, regulation, restriction or limitation of the stopping, standing or parking of vehicles in specified areas of the parking area as designated on the plan.
7. The adoption of appropriate parking and fire control regulations with respect to safety zones and fire lanes in the parking area designated on the plan.
8. The designation of safety zones in the parking area and fire lanes.
9. The providing for the removal and storage of vehicles parked or abandoned in the parking area during snow storms, floods, fires, or other public emergencies, or found unattended in the parking area, (a) where they constitute an obstruction to traffic; or (b) where stopping, standing or parking is prohibited in those portions of the parking area designated on the plan; or (c) where such vehicles have been left unattended or abandoned in parking spaces in violation of posted parking limits, and for the payment of reasonable charges for such

removal and storage by the owner or operator of any such vehicles.

10. The installation and maintenance of signs in the parking area, including signs indicating parking spaces reserved for handicapped persons.
11. Establishing such additional regulations as local conditions may require for the safety and convenience of the public and of the users of the parking area.

SECTION 6: The designation of streets, drives, loading zones, safety zones and fire lanes and the location and kind of traffic control signals, signs and devices shall be as shown on the plan and such initial traffic control scheme shall govern traffic control and parking in the parking area of the shopping center. Changes in the plan shall not be made without the approval of both the Village and owners.

SECTION 7: The cost of planning, installation, maintenance and enforcement of the parking and traffic signs, devices and regulations pursuant to this Agreement shall be borne by owner, with the exception that Village shall pay the salary of its police officers and the cost of the vehicles operated by them. Owner shall pay any and all other costs, including, but not limited to, the manufacture and installation of any signs, posts and markers, including maintenance and replacement of same; the cost of survey and planning; and, uncollected costs of removing and storing of vehicles. The Village agrees to undertake the regulation of traffic control and parking in the parking area of the shopping center in accordance with this Agreement.

SECTION 8: Owner further waives any right of recovery it may have against Village for damage to its property or the property of its agents, servants and employees arising out of the performance of this Agreement to the extent of the proceeds which the owner is entitled to

recover from its insurer.

SECTION 9: The owner and Village shall indemnify one another for issues of sole negligence.

SECTION 10: This Agreement shall remain in force for a period of one (1) year from the date hereof and shall continue from year to year thereafter, but in no event shall this Agreement exceed a period of twenty (20) years. Either party may terminate this Agreement upon thirty (30) days written notice to the other party.

SECTION 11: This Agreement shall be recorded in the Office of the Recorder of Deeds in Cook County, Illinois, pursuant to statute.

SECTION 12: Any notice given pursuant to the terms hereof shall be in writing and shall be delivered by personal service or Certified or Registered Mail, Return Receipt Requested, postage prepaid, addressed as follows:

If to Owner: Simon Property Group, L.P.
Lincolnwood Town Center
3333 W. Touhy Avenue
Lincolnwood, IL 60645

If to Village: Village Administrator
Village of Lincolnwood
6900 N. Lincoln Avenue
Lincolnwood, IL 60646

in favor of either party, but is the product of negotiations between the parties.

SECTION 13: This Agreement may be modified or amended only by a written instrument signed by both the Village and owner.

SECTION 14: The failure of owner or Village to insist on the other party's strict compliance with the terms and conditions contained in this Agreement shall not constitute a waiver of Village's or owner's rights under this Agreement.

SECTION 15: This Agreement shall be binding upon the parties hereto and shall be binding upon and inure to the benefit of their respective heirs, executors, administrators, personal representatives, successors and assigns.

SECTION 16: This Agreement may be executed in several counterparts, each of which shall be deemed to be an original, and all of which constitute one and the same instrument.

IN WITNESS WHEREOF, the parties have caused this Agreement to be executed by their proper officers duly authorized to execute the same the day and year first above written.

VILLAGE OF LINCOLNWOOD, an Illinois
municipal corporation

By: _____
Village President

SIMON PROPERTY GROUP, L.P.
a Delaware limited partnership
d/b/a Simon Real Estate Group

By: _____
Simon Property Group, Inc.
a Maryland Corporation, its
General Partner

ATTEST:

Village Clerk

FILED IN THE VILLAGE CLERK'S OFFICE
ON THE 23rd DAY OF Aug 1994
B. Humbel
VILLAGE CLERK

Request For Board Action

REFERRED TO BOARD: October 7, 2014

AGENDA ITEM NO: 2

ORIGINATING DEPARTMENT: Community Development

SUBJECT: Approval of an Ordinance Granting a Zoning Map Amendment to Rezone 6755 North Cicero Avenue to the B-2 General Business Zoning District

SUMMARY AND BACKGROUND OF SUBJECT MATTER:

At the September 2, 2014 Village Board meeting, the Village Board considered the Plan Commission's recommendation concerning a Zoning Map Amendment to rezone 6755 North Cicero Avenue to the B-2 General Business District. The subject property is the location of the Kow Kow Restaurant.

As currently illustrated on the Village Zoning Map, part of the subject property is within the B-2 General Business District and part of this property located closest to the townhomes, is shown as the R-4 Residential District. A review of Village records shows no zoning action on the Kow Kow Restaurant property to change a portion of the site to R-4 designation and, as such, it is believed the current R-4 designation of a portion of the Kow Kow site to be a map error which occurred at the time of the approval of the Map Amendment for the adjacent townhomes. To remedy this error, the Plan Commission held a Public Hearing on a proposed Map Amendment to return the entire Kow Kow Restaurant property to B-2 designation. The Plan Commission unanimously recommended approval of the Map Amendment.

At its September 2, 2014 meeting, by a 6-0 vote, the Village Board moved to concur with the Plan Commission's recommendation and directed the Village Attorney to prepare the requisite Ordinance. Attached for approval is this proposed Ordinance prepared by the Village Attorney consistent with the direction of the Village Board.

FINANCIAL IMPACT:

None

DOCUMENTS ATTACHED:

1. Proposed Ordinance

RECOMMENDED MOTION:

Move to approve an Ordinance approving a Zoning Map Amendment to rezone 6755 North Cicero Avenue to the B-2 General Business District.

VILLAGE OF LINCOLNWOOD

ORDINANCE NO. 2014-_____

**AN ORDINANCE REZONING PROPERTY
TO THE B-2 GENERAL BUSINESS ZONING DISTRICT**
(6755 North Cicero Avenue)

ADOPTED BY THE
PRESIDENT AND BOARD OF TRUSTEES
OF THE VILLAGE OF LINCOLNWOOD
THIS ____ DAY OF OCTOBER, 2014.

Published in pamphlet form
by the authority of the
President and Board of Trustees
of the Village of Lincolnwood,
Cook County, Illinois
this ____ day of _____, 2014

ORDINANCE NO. 2014-_____

**AN ORDINANCE REZONING PROPERTY
TO THE B-2 GENERAL BUSINESS ZONING DISTRICT**
(6755 North Cicero Avenue)

WHEREAS, the Village of Lincolnwood is a home rule municipal corporation in accordance with Article VII, Section 6(a) of the Constitution of the State of Illinois of 1970; and

WHEREAS, the Village has the authority to adopt ordinances and to promulgate rules and regulations that pertain to its government and affairs; and

WHEREAS, the Village of Lincolnwood Official Zoning Map ("**Zoning Map**") depicts the real properties located on the east side of Cicero Avenue, between North Shore Avenue and Pratt Avenue, as located within the B-2 General Business District ("**B-2 District**") and R-4 Residential District ("**R-4 District**") of the Village; and

WHEREAS, upon investigation, Village Staff has determined that those certain five lots commonly known as 6755 North Cicero Avenue, and legally described and depicted in **Exhibit A** to this Resolution ("**Subject Properties**"), are incorrectly depicted in the Zoning Map as being located within the R-4 District; and

WHEREAS, the Village President and Board of Trustees desire to rezone the Subject Properties into the B-2 District ("**Proposed Rezoning**"); and

WHEREAS, pursuant to notice duly published in the *Lincolnwood Review* on July 17, 2014, the Village Plan Commission conducted a public hearing on August 6, 2014, concerning the Proposed Rezoning of the Subject Properties; and

WHEREAS, at the conclusion of the public hearing, the Plan Commission made findings and recommended that the President and Board of Trustees approve the Proposed Rezoning of the Subject Properties, as set forth in this Ordinance; and

WHEREAS, having considered the findings and recommendations of the Plan Commission, the President and Board of Trustees have found and determined that it will serve and be in the best interest of the Village and its residents to rezone the Subject Properties into the B-2 District;

NOW, THEREFORE, BE IT ORDAINED BY THE PRESIDENT AND BOARD OF TRUSTEES OF THE VILLAGE OF LINCOLNWOOD, COOK COUNTY, ILLINOIS, as follows:

SECTION 1. RECITALS. The facts and statements contained in the preamble to this Ordinance are found to be true and correct and are hereby adopted as part of this Ordinance.

SECTION 2. ZONING MAP AMENDMENT. The Zoning Map of the Village is hereby amended to rezone the Subject Properties into the B-2 District, in accordance with, and pursuant to, Section 5.16 of the Zoning Ordinance and the home rule powers of the Village.

SECTION 3. SEVERABILITY. If any provision of this Ordinance or part thereof is held invalid by a court of competent jurisdiction, the remaining provisions of this Ordinance are to remain in full force and effect, and are to be interpreted, applied, and enforced so as to achieve, as near as may be, the purpose and intent of this Ordinance to the greatest extent permitted by applicable law.

SECTION 4. EFFECTIVE DATE. This Ordinance will be in full force and effect from and after its passage, approval, and publication in the manner provided by law.

PASSED this _____ day of October, 2014.

AYES: _____

NAYS: _____

ABSENT: _____

ABSTENTION: _____

APPROVED by me this _____ day of October, 2014.

Gerald C. Turry, President
Village of Lincolnwood, Cook County, Illinois

ATTESTED and FILED in my office the
_____ day of October, 2014.

Beryl Herman, Village Clerk
Village of Lincolnwood, Cook County, Illinois

#33205268_v1

EXHIBIT A

LEGAL DESCRIPTION OF THE SUBJECT PROPERTIES

PARCEL 3: LOT 31 TO 35, BOTH INCLUSIVE, TOGETHER WITH THE WEST HALF (1/2) OF THE NORTH AND SOUTH VACATED ALLEY LYING EAST OF AND ADJOINING SAID LOTS 31 TO 35, INCLUSIVE, IN BLOCK 7 IN GOODSON AND WILSON'S PRATT BOULEVARD AND CICERO AVENUE HIGHLANDS. BEING A SUBDIVISION OF THAT PART LYING NORTH OF THE SOUTH 35 ACRES OF THE WEST HALF (1/2) OF THE SOUTH WEST QUARTER (1/4) OF SECTION 34, TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, AND THAT PART OF THE WEST HALF (1/2) OF THE NORTH EAST QUARTER (1/4) OF THE SOUTH WEST QUARTER (1/4) LYING WEST OF THE SOUTHWESTERLY LINE OF THE CHICAGO AND NORTHWESTERN RAILROAD RIGHT OF WAY IN SECTION 34, TOWNSHIP 41 NORTH, RANGE EAST OF THE THIRD PRINCIPAL MERIDIAN IN COOK COUNTY, ILLINOIS.

Commonly referred to as 6755 North Cicero Avenue.

P.I.N.: 10-34-300-006-0000, 10-34-300-007-0000, 10-34-300-008-0000, 10-34-300-009-0000,
and 10-34-300-010-0000

Request For Board Action

REFERRED TO BOARD: October 7, 2014

AGENDA ITEM NO: 3

ORIGINATING DEPARTMENT: Community Development

SUBJECT: Approval of a Recommendation by the Zoning Board of Appeals to Approve a Variation from Section 4.11 of the Zoning Code to Permit a One-Story Addition to Encroach Into the Required Side Yard Setback for the Property at 7131 North Keating Avenue

SUMMARY AND BACKGROUND OF SUBJECT MATTER:

Josh Delpierre of Champion Windows, on behalf of Laurence Dy, the property owner of 7131 North Keating Avenue ("Petitioner"), initially requested two variations to permit a one-story addition to the existing single-family home: 1) a side yard setback variation; and 2) a variation concerning masonry design standards. The request for a masonry variation was eliminated because a modified design, compliant with the Village's residential masonry standards was submitted prior to the start of the Public Hearing. The proposed addition is for the purpose of adding a sunroom to the rear of the existing single-family home.

A review of Village records indicates that the existing home was constructed in approximately 1948. A search of Village records resulted in no additional information regarding the subject property pertinent to the requested variation.

Variation Request

The Petitioner seeks relief from the minimum side yard setback requirements in order to construct the proposed 300-square-foot addition. The Zoning Code requires that the minimum side yard setback be 5 feet or 10% of the lot width, whichever is greater. The lot width of the subject property is 70 feet; therefore, the required side yard setback is 7 feet. The proposed addition is set back from the side lot line by 5.1 feet and so a side yard setback variation of 1.9 feet is requested. The proposed sunroom addition is designed to match the existing setback of the single-family home.

Since adoption of the 2008 Zoning Ordinance, the Village has considered several setback variations for single-family residential additions. While every project is unique, the Village has approved ten requests for relief from side yard setback requirements. These requests resulted primarily from the existing home encroaching into the required setback.

Public Hearing

The Zoning Board of Appeals (ZBA) considered the side yard setback variation request on September 17, 2014. The Petitioner stated that the design is intended to meet the spirit of the Zoning Code and they have taken additional steps to eliminate the need for a residential design variation. The property owner testified that the proposed sunroom addition is intended to help in treatment of health issues.

Commissioner Gordon stated that the existing home is located within the required setback and that it is reasonable to permit an addition to match the existing setback. Commissioner Grant added that the proposed addition does not encroach farther into the required setback than the existing single-family home. Commissioner Grant stated that because of this existing condition, the request is distinguishable from other side yard setback requests heard by the Village.

At the July 16, 2014 Public Hearing, no member of the public was present to provide any testimony, comment, or objection.

Zoning Board of Appeals Recommendation

The ZBA concluded that the side yard setback request met the standards to approve the Variation. By a unanimous 6-0 vote, the ZBA recommends approval of the requested side yard setback Variation.

FINANCIAL IMPACT:

None

DOCUMENTS ATTACHED:

1. September 17, 2014 ZBA Minutes (Draft)
2. September 17, 2014 ZBA Packet
 - a. Staff Report to ZBA
 - b. Residential Zoning Variation Application
 - c. Proof of Ownership
 - d. Zoning Calculation Sheet
 - e. Plat of Survey
 - f. Building Elevations and Floor Plans

RECOMMENDED MOTION:

Move to concur with the recommendation of the Zoning Board of Appeals to approve a request for a variation from Section 4.11, to permit the proposed one-story addition to encroach 1.9 feet into the required 7-foot side yard setback for the property located at 7131 North Keating Avenue and to direct the Village Attorney to prepare the requisite Ordinance for adoption.



DRAFT MEETING MINUTES OF THE
ZONING BOARD OF APPEALS
SEPTEMBER 17, 2014 – 7:00 P.M.

LINCOLNWOOD VILLAGE HALL
COUNCIL CHAMBERS
6900 NORTH LINCOLN AVENUE
LINCOLNWOOD, ILLINOIS 60712

MEMBERS PRESENT:

Chairman Herb Theisen
Paul Gordon
Paul Grant
Jean Ikezoe-Halevi
Martina Keller
Kathy O'Brien

MEMBERS ABSENT:

Christopher Nickell

STAFF PRESENT:

Aaron N. Cook, AICP, Community Development Manager

I. CALL TO ORDER

Chairman Theisen noted a quorum of five members and called the meeting to order at 7:00 p.m.

II. PLEDGE OF ALLEGIANCE

III. APPROVAL OF MINUTES

Motion to recommend approval of the September 17, 2014 Meeting Minutes was made by Commissioner Grant, and seconded by Commissioner Keller.

Aye: Grant, Keller, Gordon, Ikezoe-Halevi, and Theisen

Nay: None

Motion Approved: 5-0

IV. Public Hearing: 6529 North Central Park Avenue – Residential Fence Variations

Chairman Theisen swore in the Petitioners, Mr. David Hartman and Ms. Julie DeLeon.

Mr. Cook presented the site plan for 6529 North Central Park Avenue which is in the R-3 Residential Zoning District as well as the requested fence design. The two proposed fence designs for this six-foot-tall fence are considered solid fences which are not allowed in residential districts. A six-foot fence is not permitted in a side yard. The maximum size for a side yard fence is four feet. A six-foot semi-private or open fence is allowed in a rear yard. The Petitioner is requesting two Variations: 1) to allow a solid fence in the rear yard; and 2) to allow a six-foot-tall fence in the side yard.

Homeowner Mr. David Hartman spoke about the privacy and security concerns for requesting these Variations.

Chairman Theisen asked Mr. Hartman what hardships he has encountered in asking for these Variations. Mr. Hartman's concern is that there are window wells on the north side of the property and would like to make sure they are not vulnerable or available to someone coming onto their property. His main concern is the privacy factor. Chairman Theisen reminded Mr. Hartman that solid fences are not allowed and that he has not shown any hardship.

Julie DeLeon, the landscape architect, spoke about the design of the fence and indicated that there is a half-inch gap between each board. She agreed that an adjustment could be made in this regard. Mr. Hartman also agreed that he would be willing to increase the spacing between the boards.

Commissioner Keller asked if there was opposition to this motion. There was no one in the audience to oppose this matter nor did staff receive any calls regarding this matter.

There was a discussion as to whether the fence could be modified to achieve the open fence standards but still keep the six-foot height. Mr. Hartman asked about the procedure for modifying their request. Mr. Cook answered that Mr. Hartman would have to resubmit new plans for the Zoning Board to review at a future meeting. If a Variation is still needed, a specific plan would be required. The ZBA review process would be continued to the next meeting and they would not have to start the process anew.

Mr. Hartman was agreeable to an open fence option, including a wrought iron fence. Wrought iron fences can be six-feet high in a side yard. The solid rear yard fence does not meet the Zoning Code. Chairman Theisen asked them to submit revised plans and will continue this to another meeting.

Motion to continue this matter to the October 15, 2014 meeting of the Zoning Board of Appeals was made by Commission O'Brien, and seconded by Commissioner Gordon.

Aye: O'Brien, Gordon, Grant, Ikezoe-Halevi, Keller, and Theisen

Nay: None

Motion Approved: 6-0

V. Public Hearing: 7131 North Keating Avenue – Residential Variations

Chairman Theisen swore in the Petitioners, Mr. Laurence Dy and Mr. Josh Delpierre. Mr. Cook presented the Variation request for 7131 North Keating Avenue, which is the R-3 Residential Zoning District. The Variation is permit an approximate 300-square-foot first-floor addition to allow the interior side yard setback to be reduced from the required seven feet to five feet. The plat of survey and site plan was presented for the addition of a sunroom which will be in the same location as the current concrete patio. The revised building elevations were presented that comply with current design requirements and was withdrawn.

The homeowner, Mr. Laurence Dy, is requesting this Variation due to health reasons. Mr. Dy is awaiting a heart transplant and his doctors recommended that sunlight would be beneficial to his health, especially during the winter months..

Motion to recommend approval of a Residential Variation for 7131 North Keating Avenue was made by Commissioner Gordon, and seconded by Commissioner Grant.

Aye: Gordon, Grant, Ikezoe-Halevi, Keller, O'Brien, and Theisen

Nay: None

Motion Approved: 6-0

Mr. Cook announced that this matter will be heard at either the October 7, 2014 or October 21, 2014 meeting of the Village Board. The Petitioners will have an opportunity to address the Board at that time.

Commissioner Gordon made a statement regarding the Village's fence code standards. He believes there are three flaws in the current fence code. The first flaw he mentioned was that lateral wind force is not named in the code. Secondly, the orientation regarding the cross section demands of the rails are not specified. The third flaw is in reference to the spacing of the boards adjacent to each other on each side of the fence. The current code recommends .85 times the width of the board, leaving only a half-inch between boards. He believe that this is essentially a solid fence. There was discussion as to how to change the current fence code to not permit these types of fences.

Motion to recommend to the Village Board to initiate the Plan Commission's review of the fence code language was made by Commissioner Keller, and seconded by Commissioner O'Brien.

Aye: Keller, O'Brien, Gordon, Grant, Ikezoe-Halevi, and Theisen

Nay: None

Motion Approved: 6-0

Chairman Theisen asked if anyone in the audience would like to address this Commission. Let the record state that no one came forward.

VI. ADJOURNMENT

Hearing no further business, **motion to adjourn** was made by Commissioner O'Brien, and seconded by Commissioner Gordon. Meeting adjourned at 7:45 p.m.

Aye: O'Brien, Gordon, Grant, Ikezoe-Halevi, Keller, and Theisen

Nay: None

Motion Approved: 6-0

Respectfully Submitted,

Kathryn M. Kasprzyk
Community Development Coordinator

Related Village Action

The Village in the past four years has considered several like requests for single-family residential additions. While every project is unique all but one project requested multiple variations. The Village has considered and approved ten requests for relief from side yard setback requirements, which resulted primarily from the existing home encroaching into the required setback. It is worth noting that the majority of the variations approved permitted a second story addition to match the existing non-conforming first floor setback.

Pertinent Property Information – The existing single-family home was constructed in or around 1948. A search of Village records resulted in no additional records or information regarding the subject property pertinent to this request.

Conclusion

The property owner is seeking a variation in order to allow a sunroom addition to the first floor of the existing single-family home. The requested variation is to permit the addition to match the existing nonconforming interior side yard setback of approximately 5.1 feet rather than the required seven foot setback.

Documents Attached

1. Residential Zoning Variation Application
2. Proof of Ownership
3. Zoning Calculations Sheet
4. Plat of Survey
5. Building Elevations and Floor Plans



VILLAGE OF LINCOLNWOOD
Community Development Department

Public Hearing Application
Variations

PROPERTY

Property Address: 7131 N. KEATING AVE

Permanent Real Estate Index Number(s): _____

Zoning District R-3 Lot Area: 8,672

List all existing structures on the property. Include fencing, sheds, garages, pools, etc.

10'x12' SHED , FENCING

Are there existing development restrictions affecting the property? Yes No
(Examples: previous variations, conditions, easements, covenants)

If yes, describe: CURRENT SIDEYARD SETBACKS ARE 5'

REQUESTED ACTION

- Variation - Residential
- Variation - Non-Residential
- Variation - Off-Street Parking
- Variation - Design Standards
- Variation - Signs/Special Signs
- Minor Variation
- Other

DESCRIPTION OF REQUEST

Describe the Request and Project: REDUCE SIDEYARD SETBACK FROM 7' TO 5' FOR INSTALLATION OF SCURROOM ADDITION

PROPERTY OWNER INFORMATION

Property Owner(s):

Name: (List all beneficiaries if Trust): LAURENCE DY

Address: 7131 N. KEATING AVE

Telephone: (224) 392-7008; () _____ E-mail Address: _____

Petitioner (if different from owner):

Name: JOSH DELPIERRE Relationship to Property: CONTRACTOR

Address: 310 COUNTY LINE RD. BENSENVILLE, IL 60016

Telephone: (630) 279-0909 Fax: (630) 279-0955 E-mail Address: _____

JDELPIERRE@GETCHAMPION.COM

REQUIRED ATTACHMENTS

Check all documents that are attached:

- Plat of Survey _____
- Site Plan _____
- Proof of Ownership _____
- Floor Plans _____
- Elevations _____
- Applicable Zoning Worksheet _____
- Photos of the property _____
- PDF files of all drawings _____

For Office Use Only

Fee: _____ Deposit: _____

Date Received: _____

Checked By: _____

The article(s), section(s) and paragraph(s) of the Village of Lincolnwood Zoning Ordinance from which the Action is being sought:

*The above documents are required for all applications. The Zoning Officer may release an applicant from specific required documents or may require additional documents as deemed necessary.

COST REIMBURSEMENT REQUIREMENT

The Village requires reimbursement of certain out-of-pocket costs incurred by the Village in connection with applications for zoning approvals and relief. These costs include, but are not limited to, mailing costs, attorney and engineer costs, and other out-of-pocket costs incurred by the Village in connection with this application. In accordance with Section 5.02 of the Village of Lincolnwood Zoning Ordinance, both the petitioner and the property owner shall be jointly and severally liable for the payment of such out-of-pocket costs. Out-of-pocket costs incurred shall be first applied against any hearing deposit held by the Village, with any additional sums incurred, to be billed at the conclusion of the hearing process.

Invoices in connection with this application shall be directed to:

Name: JOSH DELPIERRE / CHAMPION WINDOW CO

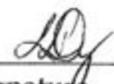
Address: 310 COUNTY LINE RD.

City, State: BENSENVILLE, IL 60016

TESTIMONY AND SIGNATURE

I hereby state that I have read and understand the Village cost reimbursement requirement, as well as the requirements and procedures outlined in Article V of the Village Zoning Ordinance, and I agree to reimburse the Village within 30 days after receipt of an invoice therefor. I further attest that all statements and information provided in this application are true and correct to the best of my knowledge and that I have vested in me the authority to execute this application.

PROPERTY OWNER

 _____

Signature Date

LAURENCE DY

PRINT NAME

PETITIONER (If different than property owner)

 8/13/14

Signature Date

JOSH DELPIERRE

PRINT NAME



VILLAGE OF LINCOLNWOOD

COMMUNITY DEVELOPMENT DEPARTMENT

VARIATION STANDARDS

To be approved, each variation request must meet certain specific standards. These eight standards are listed below. After each listed standard, explain how your variation request satisfies the listed standard. Use additional paper if necessary.

1. The requested variation is consistent with the stated intent and purposes of the Zoning Ordinance and the Comprehensive Plan.

THE REQUESTED VARIATION FOR THE SUNROOM ADDITION IN MY OPINION IS WITHIN THE PURPOSE & INTENT OF THE ZONING ORDINANCE. THE SUNROOM WILL HAVE LITTLE TO NO EFFECT ON THE PROPERTY & SURROUNDING PROPERTIES

2. The particular physical surroundings, shape or topographical conditions of the subject property would bring a particular hardship upon the owner, as distinguished from a mere inconvenience, if the strict letter of this Zoning Ordinance is enforced.

THE EXISTING HOUSE IS ONLY 5' FROM THE PROPERTY LINE MAKING IT A LEGAL NON-CONFORMING STRUCTURE. THE PROPOSED SUNROOM WILL NOT BE ANY CLOSER TO THE PROPERTY LINE THAN THE HOUSE IS.

3. The conditions upon which the petition for the variation is based would not be applicable generally to other property within the same Zoning District.

GENERALLY OTHER PROPERTIES WITHIN THE SAME ZONING DISTRICT ARE LEGAL CONFORMING STRUCTURES THAT MEET THE ZONING SETBACKS

4. The variation is not solely and exclusively for the purpose of enhancing the value of or increasing the revenue from the property.

THE PURPOSE IS TO GAIN REASONABLE USE OF THE BACKYARD & HAVE A PLACE TO SIT FREE OF BUGS & THE SUN.

5. The alleged difficulty or hardship has not been created by any person presently having an interest in the property.

THE HARDSHIP HAS BEEN CREATED BY THE CURRENT ZONING AND NOT BY ANY PERSON HAVING AN INTEREST IN THE PROPERTY.

6. The granting of the variation will not be detrimental to the public welfare or injurious to other property or improvements in the neighborhood in which the property is located.

THE SUNROOM WILL NOT BE ENCRACHING ONTO ANY NEIGHBORING PROPERTIES ANY MORE THAN THE EXISTING HOUSE.

7. The variation granted is the minimum change to the Zoning Ordinance standards necessary to alleviate the practical hardship on the subject property.

THE TWO WALLED SUNROOM WILL FOLLOW THE SIDE & BACK WALLS OF THE HOUSE SQUARING OFF THE BACK CORNER. IT WILL NOT BE ANY FARTHER INTO THE SETBACK THAN THE EXISTING HOUSE.

8. The proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the danger of fire, or otherwise endanger the public safety, or substantially diminish or impair property values within the neighborhood.

THE BEATIFULL SUNROOM ADDITION WILL BE BUILT TO CODE & WILL NOT IMPAIR LIGHT & AIR SUPPLY, INCREASE DANGER, ENDANGER PUBLIC SAFETY, OR DIMINISH PROPERTY VALUES.

WARRANTY DEED

THIS IS A CERTIFIED
TRUE AND EXACT COPY
OF THE ORIGINAL
FIRST AMERICAN TITLE

BY 

THE GRANTOR(S), **Fahad Arshad, Single, Never Married**, of the Municipality of Lincolnwood, County of Cook, State of Illinois, for and in consideration of Ten and 00/100 DOLLARS, in hand paid, CONVEY(S) and WARRANT(S) to:
~~_____~~, ^{Angelo} ~~_____~~ ^{which} **Laurence Dy & Marc Dy ***
9233 Parkside Ave.
Morton Grove, IL 60053 

(Strike Inapplicable) ~~each as to an undivided 50% interest~~
a) As Tenants in Common
~~b) Not in Tenancy in Common, but in Joint Tenancy~~
~~c) Husband and Wife, Not as Joint Tenants, Not as Tenants in Common, but as Tenants by the Entireties~~
~~d) Statutory (Individual to Individual)~~ 

The following described Real Estate situated in the County of Cook, in the State of Illinois, to wit:

LEGAL: SEE ATTACHED

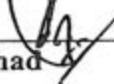
Subject to: Covenants, conditions, and restrictions of record; public utility easements; A general real estate taxes not yet due.

Hereby releasing and waiving all rights under and by virtue of the Homestead Exemption Laws of the State of Illinois. TO HAVE AND TO HOLD said premises forever.

Permanent Real Estate Index Number: **10-34-101-014-0000/10-34-101-045-0000**

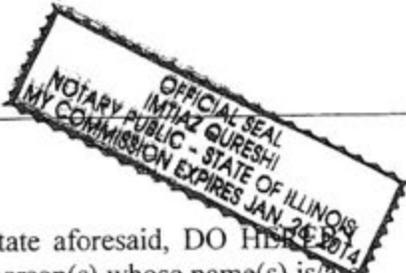
Address of Real Estate: **7131 N. Keating Ave. Lincolnwood IL 60712**

Dated **07/30/2010**

 (SEAL) _____ (SEAL)

_____ (SEAL) _____ (SEAL)

First American Title
Order # **2074660**



State of Illinois) SS.
County of Cook)

I, the undersigned, a Notary Public in and for said County, in the State aforesaid, DO HEREBY CERTIFY that **Fahad Arshad**, personally known to me to be the same person(s) whose name(s) is/are subscribed to the foregoing instrument, appeared before me this day in person, and acknowledged that he/she/they signed, sealed and delivered the said instrument as his/her/their free and voluntary act(s), for the uses and purposes therein set forth, including the release and waiver of the right of homestead.

Given under my hand and official seal, this 24th July 2010

Imtiaz Qureshi

This instrument was prepared by:
RAMSEN ISAAC & ASSOCIATES LLC, 7636 N. ROGERS AVENUE, CHICAGO, IL 60626

MAIL TO:
Dale Daemicke, *Att. at Law*
646 Raleigh Rd.
Glenview, IL 60025

SEND TAX BILL TO:
Ariane Mantner, Laurence Dy & Marc Dy
~~9223 Parkside Ave.~~
~~Morton Grove, IL 60053~~
7131 N. Keating Ave.
Lincolnwood, IL 60712



VILLAGE OF LINCOLNWOOD

Community Development Dept.

Zoning Calculations Single-Family Home Additions

Please fill out this form for all permits involving Single-Family Home Additions

ADDRESS 7131 N. KEATING AVE

ZONING DISTRICT R-3

TOTAL LOT AREA = WIDTH 70 X LENGTH 123.91 = 8,673 Sq.Ft.

ZONING SETBACKS

FRONT YARD SETBACK

Required	Existing	Proposed
<u>15</u> Ft.	<u>24</u> Ft.	<u>24</u> Ft.

INTERIOR SIDE YARD SETBACK

<u>7</u> Ft.	<u>5</u> Ft.	<u>5</u> Ft.
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INTERIOR/CORNER SIDE YARD SETBACK (Please circle one)

<u>7</u> Ft.	<u>5</u> Ft.	<u>5</u> Ft.
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REAR YARD SETBACK

	<u>52</u> Ft.	<u>52</u> Ft.
--	---------------	---------------

DISTANCE BETWEEN HOUSE AND ACCESSORY BLDG.

Min. 15 Ft.		<u>38</u> Ft.
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ZONING CALCULATION INSTRUCTIONS

60% TOTAL LOT COVERAGE (Lot area)

Permitted	Existing	Proposed Total
<u>8,673</u> X .60 = <u>5,203</u> Sq.Ft.	<u>3334</u> Sq.Ft.	<u>3334</u> Sq.Ft.

35% BUILDING COVERAGE (Lot area)

<u>8,673</u> X .35 = <u>3,036</u> Sq.Ft.	<u>2320</u> Sq.Ft.	<u>2621</u> Sq.Ft.
--	--------------------	--------------------

50% FRONT YARD COVERAGE (Front yard)

<u>1920</u> X .50 = <u>960</u> Sq.Ft.		
---------------------------------------	--	--

30% REAR YARD BLDG. COV. (Rear yard)

--	--	--

60% or 66% F.A.R. (Lot area)

<u>8,673</u> X .60 = <u>5,203</u> Sq.Ft.		
(X .66)		

AREA CALCULATIONS

A. CELLAR OR BASEMENT

Existing	Proposed Changes	Proposed Total
<u>1985</u> Sq.Ft.		

B. 1ST FLOOR

<u>2320</u> Sq.Ft.	<u>+301</u> Sq.Ft.	<u>2621</u> Sq.Ft.
--------------------	--------------------	--------------------

C. 2ND FLOOR

--	--	--

D. ACCESSORY BUILDINGS

<u>126</u> Sq.Ft.		<u>126</u> Sq.Ft.
-------------------	--	-------------------

E. DRIVEWAY

<u>411</u> Sq.Ft.		<u>411</u> Sq.Ft.
-------------------	--	-------------------

F. SIDEWALKS

<u>176</u> Sq.Ft.		<u>176</u> Sq.Ft.
-------------------	--	-------------------

G. PATIO/OTHER IMPERVIOUS SURFACE

<u>301</u> Sq.Ft.	<u>-301</u> Sq.Ft.	<u>0</u> Sq.Ft.
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TOTAL SQUARE FOOTAGE FOR PERMIT FEE CALCULATION

BASEMENT OR CELLAR + B + C + D + HABITABLE ATTIC = 4431 Sq.Ft.

HOUSE HEIGHT (Measured from top of street curb)

Permitted	Proposed
Max. 35 Ft.	<u>LESS THAN 35</u> Ft.

FINISHED FIRST FLOOR HEIGHT (Measured from top of street curb)

Max. 3 Ft.	<u>LESS THAN</u> Ft.
------------	----------------------

VARIATION REQUIRED

No

Yes

For

The undersigned hereby acknowledges the accuracy of the above information and accepts full responsibility for any discrepancies or inaccuracies.



Licensed Architect Signature & Stamp

COMMUNITY DEVELOPMENT DEPT. • 6900 N. LINCOLN AVE. • LINCOLNWOOD, IL 60712 •

PHONE: 847-673-7402 • FAX: 847-673-7456

Date

TO DETERMINE MAXIMUM PERMISSIBLE SQUARE FOOTAGE AMOUNTS

- Total Lot Coverage *Area of the Lot x Maximum Permissible Amount*
- Total Building Coverage *Area of the Lot x Maximum Permissible Amount*
- Front Yard Coverage *Area of the Front Yard x Maximum Permissible Amount*
- Rear Yard Building Coverage *Area of the Rear Yard x Maximum Permissible Amount*

ZONING CALCULATION INSTRUCTIONS

60% TOTAL LOT COVERAGE > Add B + D + E + F + G =	<u>3334</u> Sq.Ft.
35% BUILDING COVERAGE > Add B + D =	<u>2747</u> Sq.Ft.
50% FRONT YARD COVERAGE > Add E + Front Yard Portion of F =	<u>504</u> Sq.Ft.
30% REAR YARD BUILDING COVERAGE > D =	<u>126</u> Sq.Ft.
60% OR 66% F.A.R. > Add B + C + D =	<u>2747</u> Sq.Ft.

TO CALCULATE REQUIRED FRONT YARD SETBACK

The required minimum front yard setback is 25 feet, or in line with the average of 40% of the existing homes on the block adjacent to the subject property, whichever is greater.

IMPERVIOUS SURFACE

This is the area of the lot which does not allow water to be absorbed so it may percolate into deeper ground. Impervious surfaces include the footprint of a home, Portland cement concrete, bituminous concrete, composed of stone or gravel, or any other surface that allows little or no water penetration.

TOTAL LOT COVERAGE

This is the part or percent of the lot occupied by buildings or structures, including accessory buildings or structures, and any other impervious surface.

Total Lot Coverage Allowance: the numerical value obtained by dividing the Lot Coverage by the Area of the Lot.

Maximum Permissible Total Lot Coverage: 60%

BUILDING COVERAGE

This is the area of the lot that is covered by the footprint of a principal building and any structure under a roof, including accessory buildings or structures, measured at grade, from the exterior faces of the exterior walls, but excluding open decks, and terraces.

Building Coverage Allowance: the numerical value obtained by dividing the Building Coverage by the Area of the Lot.

Maximum Permissible Building Coverage: 35%



FRONT YARD COVERAGE

This is the area of the front yard that is covered with impervious surface.

Front Yard Coverage Allowance: the numerical value obtained by dividing the Front Yard Coverage by the area of the front yard.

Maximum Permissible Front Yard Coverage: 50%

REAR YARD BUILDING COVERAGE

This is the area of the rear yard that is covered by an accessory building(s).

Rear Yard Building Coverage Allowance: the numerical value obtained by dividing the Rear Yard Coverage by the area of the rear yard.

Maximum Permissible Rear Yard Coverage: 30%

TOTAL FLOOR AREA – For Determining Floor Area Ratio

This is the sum of the gross horizontal areas of all floors of a building or of such an area devoted to a specific use, measured from the exterior face of exterior walls.

Floor Area will include:

Interior features such as hallways, stairs, closets, elevator shafts, interior balconies, mezzanines, and enclosed porches. In all residential buildings, any space that has a floor to ceiling height of more than 14 feet, and any fraction thereof, shall be treated as a separate floor and will be counted twice when calculating floor area.

Floor Area will not include:

The first 250 square feet or 50% of the floor area of a garage, whichever is less.
The first 100 square feet of the area of a shed.
Total cellar square footage.
Total attic space square footage.
Chimneys that extend from the building exterior by two (2) feet or less.

FLOOR AREA RATIO (F.A.R.)

This is the numerical value obtained by dividing the "floor area" within a building or buildings on a lot by the area of such lot. The floor area ratio requirement as designated for each district when multiplied by the lot area in square feet shall determine the maximum permissible "floor area" for the building or buildings on the lot.

Maximum Permissible F.A.R.

For properties equal to or greater than 6,000 Sq.Ft. .60
For properties less than 6,000 Sq.Ft. .66 or 3,600 Sq.Ft.
(Whichever is less)

FINISHED FIRST FLOOR HEIGHT

This is the height of the first finished floor measured from the top of the street curb.

Maximum Permissible Finished Floor Height: 3 feet above top of curb



PLAT OF SURVEY

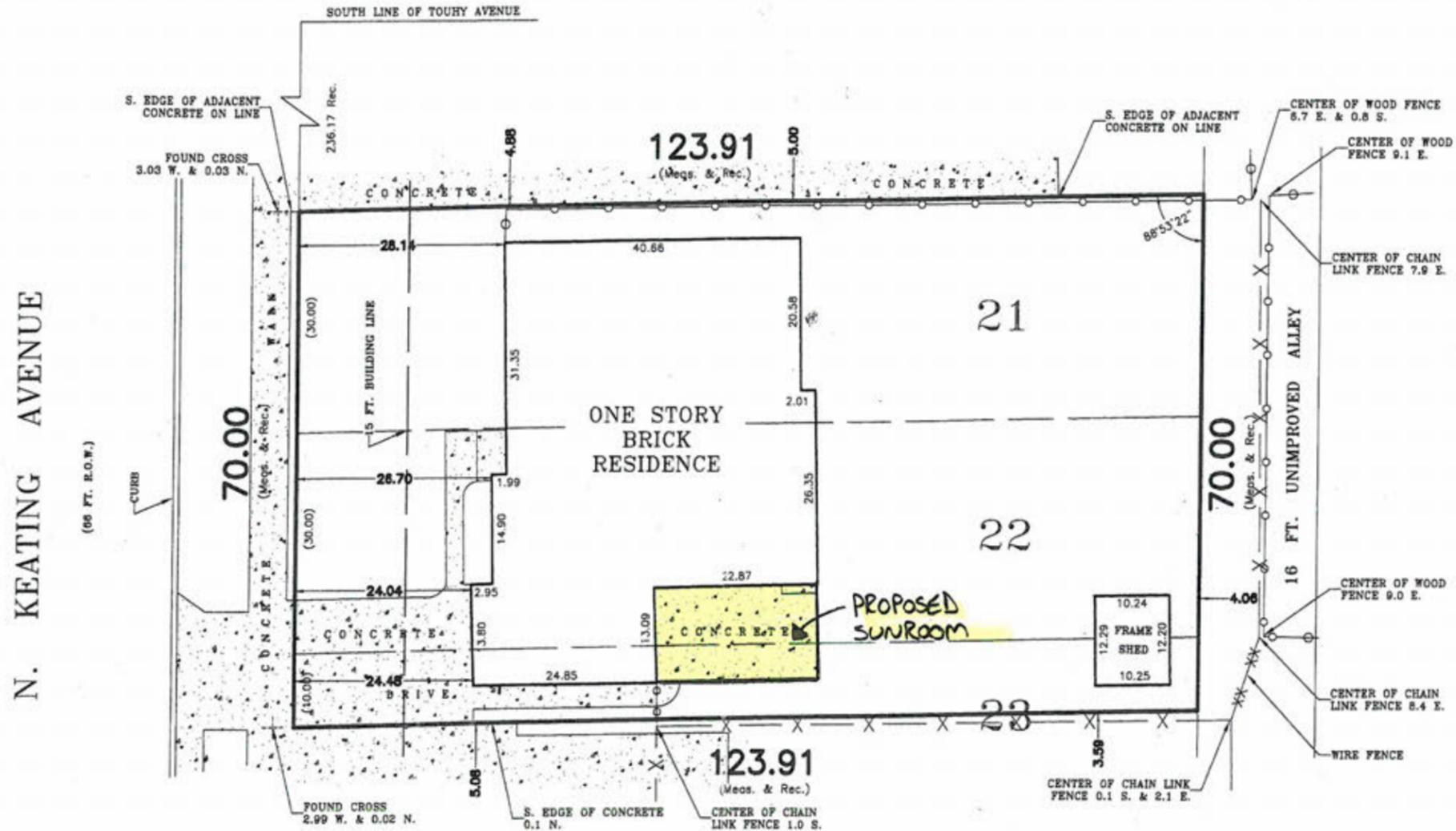
of

LOTS 21, 22 AND THE NORTH 10 FEET OF LOT 23 IN WITTBOLD'S ADDITION TO WEST RODGER'S PARK IN THE NORTHWEST QUARTER OF SECTION 34, TOWNSHIP 41 NORTH, RANGE 13 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED JUNE 20, 1927 AS DOCUMENT NUMBER 9691849, IN COOK COUNTY, ILLINOIS.

ADDRESS: 7131 N. KEATING AVENUE, LINCOLNWOOD, ILLINOIS



SCALE: 1"=20'



GENERAL NOTES:

- 1) THE LEGAL DESCRIPTION HAS BEEN PROVIDED BY THE CLIENT OR THEIR AGENT.
- 2) THIS SURVEY SHOWS THE BUILDING LINES AND EASEMENTS AS INDICATED BY THE RECORDED PLAT. THIS PLAT DOES NOT SHOW ANY RESTRICTIONS ESTABLISHED BY LOCAL ORDINANCES UNLESS SUPPLIED BY THE CLIENT.
- 3) BASIS OF BEARING FOR THIS SURVEY IS AS THE NORTH ARROW INDICATES, AND IS SHOWN TO INDICATE THE ANGULAR RELATIONSHIP OF THE BOUNDARY LINES.
- 4) MONUMENTS, IF SET, DURING THIS SURVEY, REPRESENT THE TRUE CORNERS OF THIS DESCRIPTION AS SURVEYED.
- 5) LOCATION OF SOME FEATURES MAY BE EXAGGERATED FOR CLARITY. NO EXTRAPOLATIONS MAY BE MADE FROM THE INFORMATION SHOWN HEREON.
- 6) ONLY COPIES WITH AN ORIGINAL SIGNATURE AND SEAL ARE OFFICIAL LEGAL DOCUMENTS. ALL SURVEYS ARE COPYRIGHTED MATERIALS WITH ALL RIGHTS RESERVED.

STATE OF ILLINOIS)
S.S.
COUNTY OF COOK)

SURVEY ORDERED BY: RAMSEN ISSAC

I, JOSEPH P. MAIKISCH, AS AN EMPLOYEE OF PREFERRED SURVEY INC., DO HEREBY STATE THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARD FOR A BOUNDARY SURVEY. PROPERTY CORNERS HAVE BEEN SET OR NOT IN ACCORDANCE WITH CLIENT AGREEMENT. DIMENSIONS ARE SHOWN IN FEET AND DECIMAL PARTS THEREOF AND ARE CORRECTED TO A TEMPERATURE OF 68 DEGREES FAHRENHEIT.

GIVEN UNDER MY HAND AND SEAL THIS
5TH DAY OF AUGUST



MY LICENSE EXPIRES ON 11/30/10

P.S.I. NO. 1091756

Professional Design Registration #184-002795

PREFERRED SURVEY, INC.

7845 W. 79TH STREET, BRIDGEVIEW, IL, 80455
Phone 708-458-7845 / Fax 708-458-7855
www.psisurvey.com

Field Work Completed	08/04/10	FLD CREW:	AM/RS
Land Area Surveyed	8,672.1 Sq. Ft.	CAD:	JPM
Drawing Revised			

ELECTRIC TO BE RAN IN 3/8" GREENFIELD WITH SEPERATE GROUNDING WIRE (GREEN #14) RAN THROUGHOUT THE ENTIRE CIRCUIT & BONDED INTO THE GROUND BUSS IN THE MAIN PANEL

Sunroom to have (1) dedicated 110V circuit in existing panel

Outlets to be spaced so that no point along the perimeter is greater than 6 ft. from a receptacle per article 210.52A NEC

SUNROOM ROOF DESIGNED TO WITHSTAND 90 M.P.H. WIND LOAD TABLE R 301.2(1)

SUNROOM ROOF DESIGNED TO WITHSTAND A 30 P.S.F. GROUND SNOW LOAD TABLE R 301.2(1)

ALL WOOD POSTS IN CONTACT WITH CONCRETE SHALL BE PRESSURE PRESERVATIVE TREATED (SEC. 319, IRC)

Sunroom to be unheated, unconditioned space

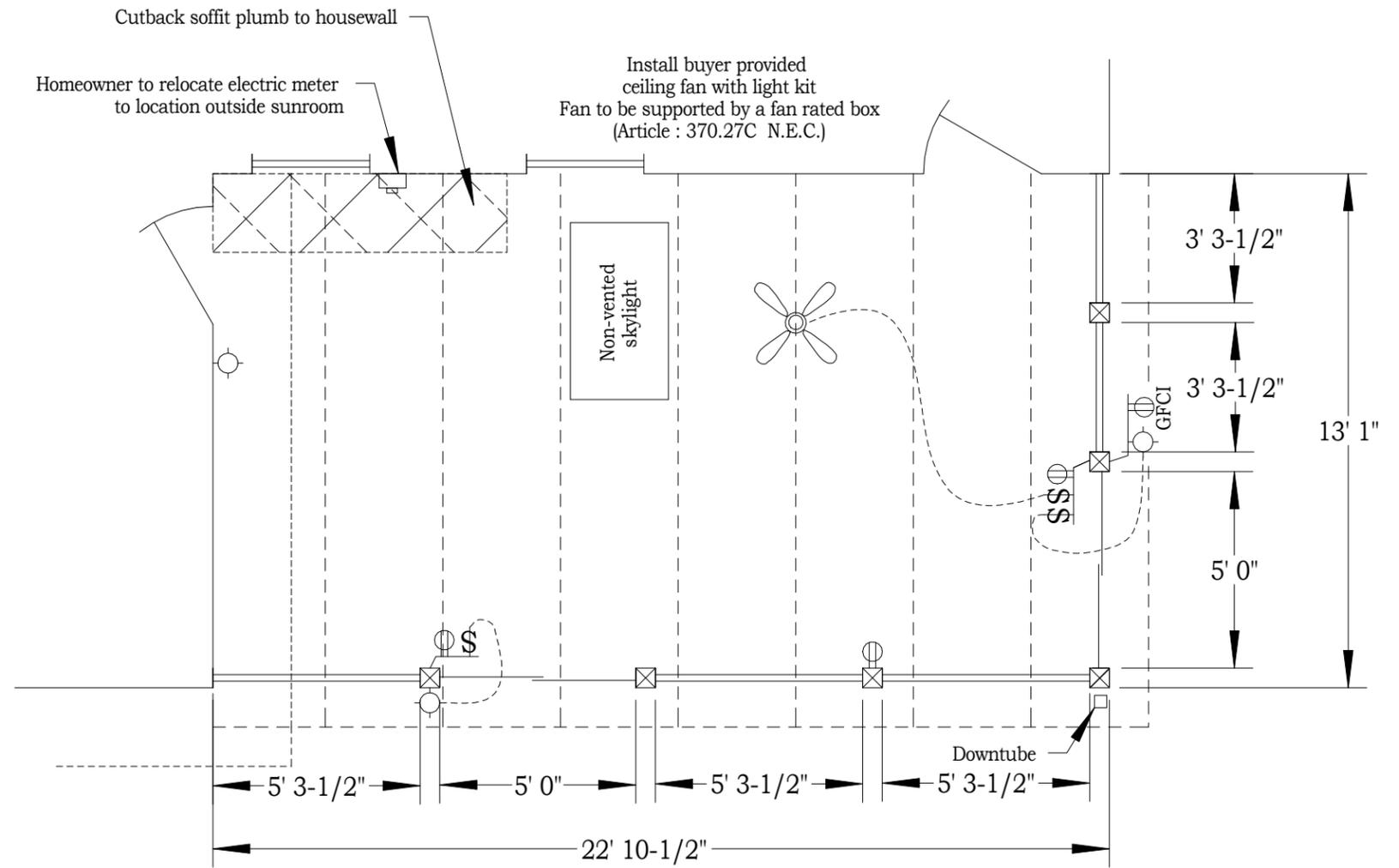
CHAMPION
Modular aluminum construction.
Seasonal, unheated, non-habitable space.
3-SEASON window room with 365 glass
Framing to be white aluminum.
Exterior panel to be white.
Interior panel to be white.
All sashes & door glass to be tempered.

CODES

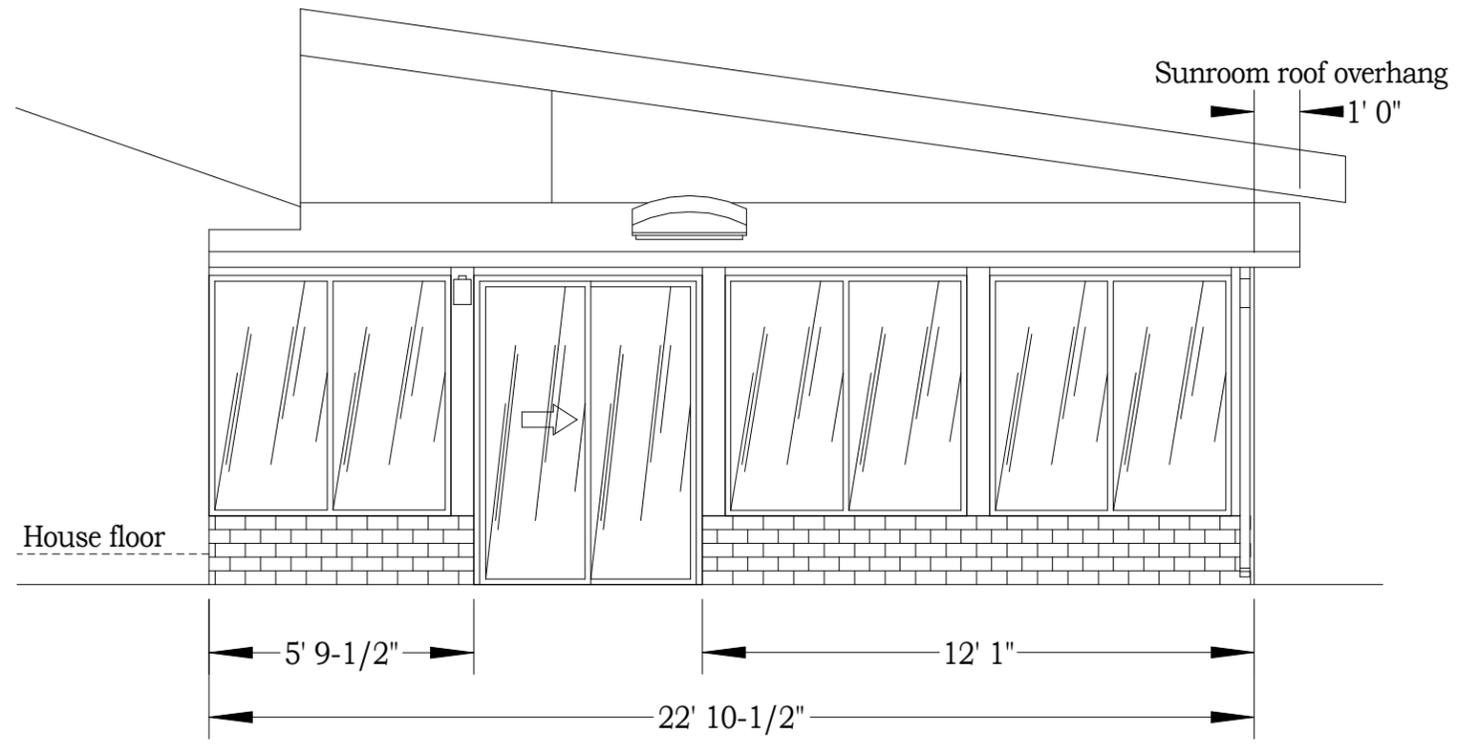
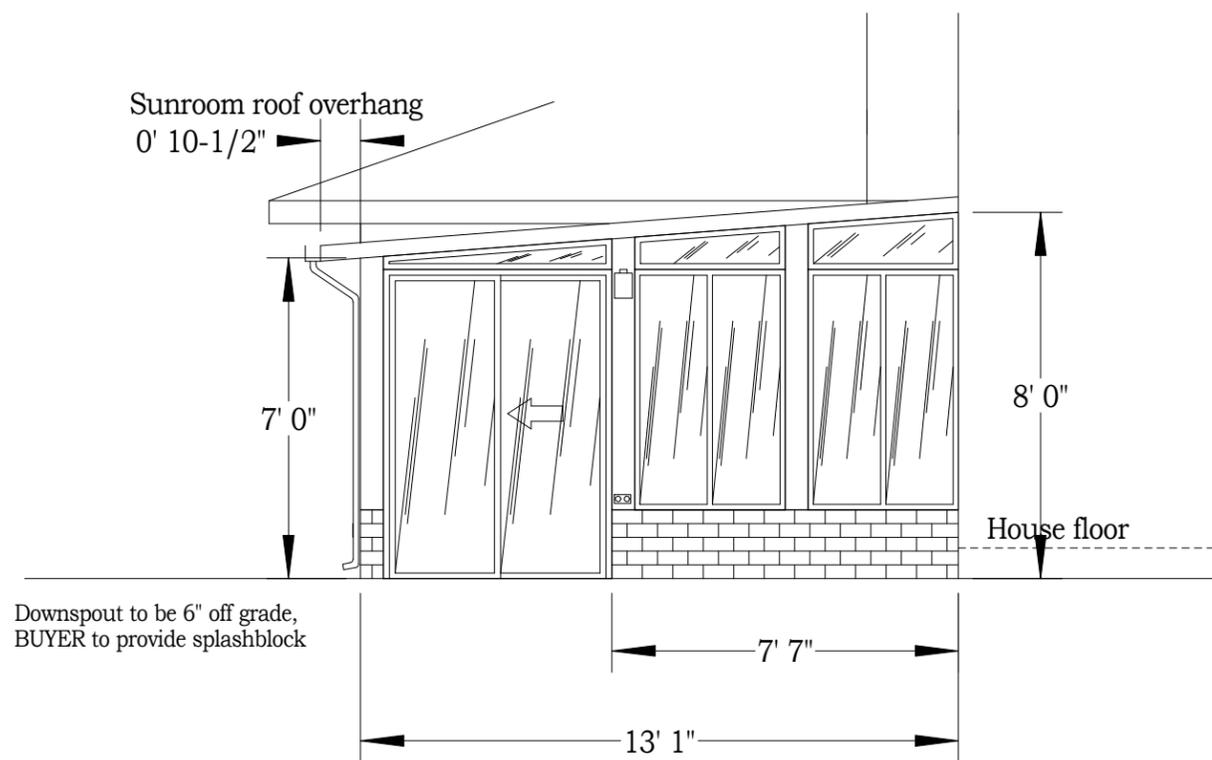
2009 International Building Code
2009 International Residential Code
2009 International Mechanical Code
2011 National Electric Code
2012 International Energy Code

Jeffery J. Heaney Architect A.I.A.
911 Washington St. Glenview, IL 60025
847 729-4190

I HEREBY CERTIFY THAT THESE DRAWINGS WERE PREPARED UNDER MY HAND AND SUPERVISION, AND THAT THEY, TO THE BEST OF MY KNOWLEDGE CONFORM TO THE CHAMPION ENGINEERED DRAWINGS OF MARTIN GOSS LICENSED ILLINOIS ENGINEER. JOSH DELPIERRE, PATIO ROOM DESIGNER	CHAMPION PATIO ROOMS 310 County Line Rd. Bensenville, IL 60106 (630) 279-0909	RESIDENCE Laurence Dy 7131 N. Keating Ave. Lincolnwood, IL 60712 224-392-7008	DATE 08/22/14	SCALE : 1/4" = 1'
	PATIO ROOM MANAGER JOSH DELPIERRE	CUSTOMER ID NO. 16296	SHEET NO. 1 OF 3	NOTES



Distance from house floor to room floor not to exceed 7-3/4"
Distance from room floor to grade not to exceed 7-3/4"



Distance from house floor to room floor not to exceed 7-3/4"
 Distance from room floor to grade not to exceed 7-3/4"

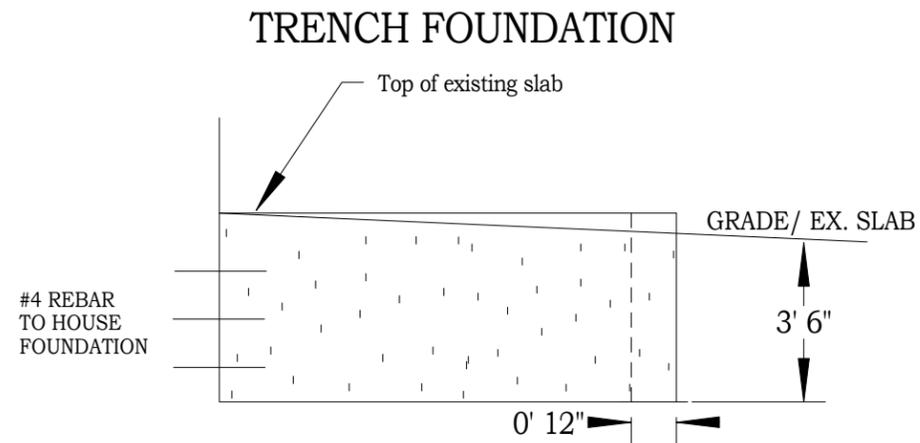
Sunroom to be unheated, unconditioned space

CODES

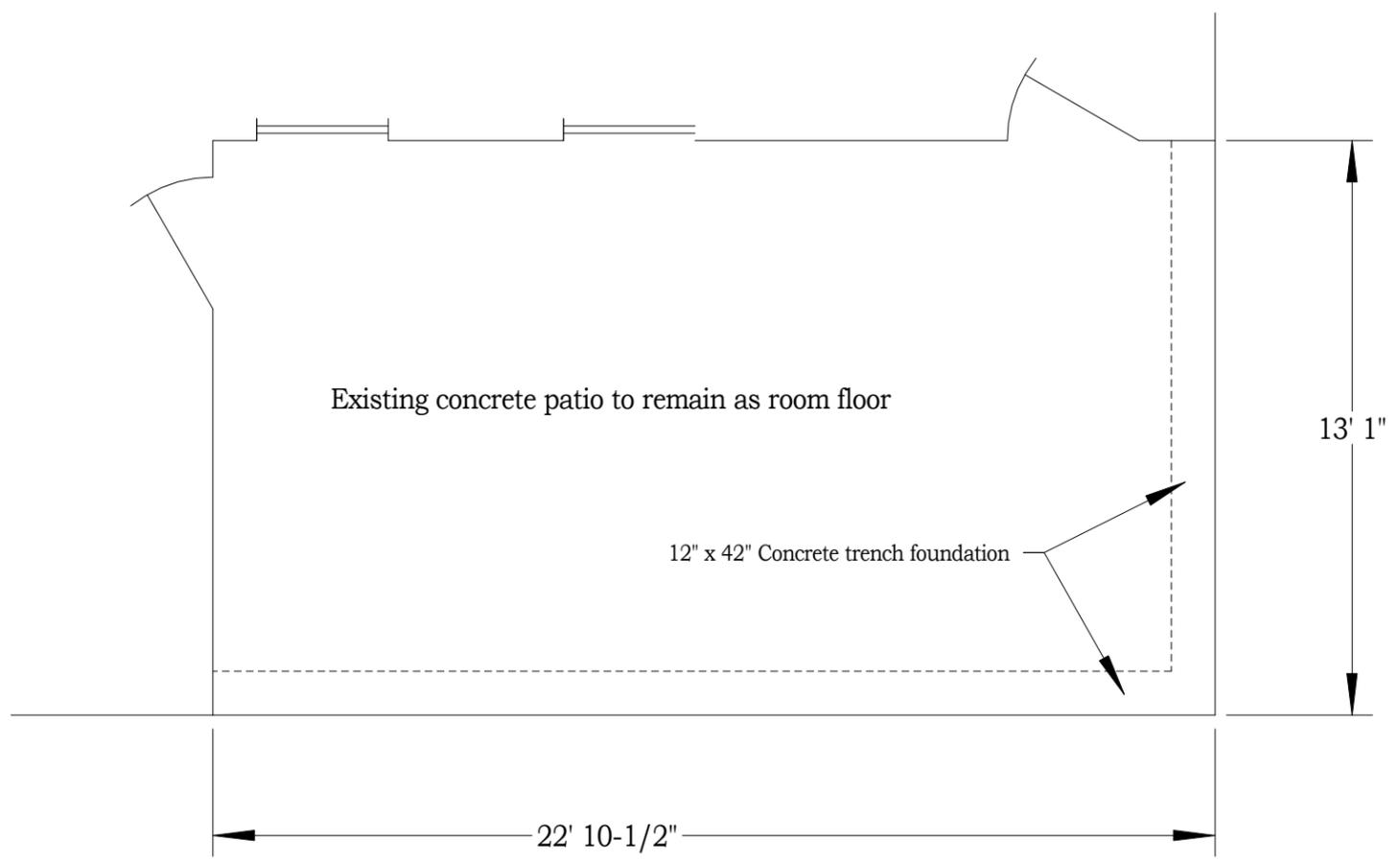
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	PATIO ROOM MANAGER JOSH DELPIERRE	CUSTOMER ID NO. 16296	SHEET NO. 2 OF 3	



- Concrete Check List:
- saw cut
 - pour over slab
 - found. underpin
 - anchor bolts
 - remove slab
 - visquine & gravel
 - foundation
 - remove concrete stoops



Distance from house floor to room floor not to exceed 7-3/4"
 Distance from room floor to grade not to exceed 7-3/4"

CODES

2009 International Building Code
 2009 International Residential Code
 2009 International Mechanical Code
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	PATIO ROOM MANAGER JOSH DELPIERRE	CUSTOMER ID NO. 16296	SHEET NO. 3 OF 3	

Request For Board Action

REFERRED TO BOARD: October 7, 2014

AGENDA ITEM NO: 4

ORIGINATING DEPARTMENT: Community Development

SUBJECT: Consideration of a Recommendation by the Zoning Board of Appeals to Approve a Variation from Section 4.11 of the Zoning Code to Permit a One-Story Addition to Encroach Into the Required Side Yard Setback for the Property at 5080 West North Shore Avenue

SUMMARY AND BACKGROUND OF SUBJECT MATTER:

At the September 16, 2014 Village Board meeting, the Village Board tabled this matter to the October 7, 2014 Village Board meeting. The Village Board, therefore, should first remove this matter from the table in order to consider it.

Also, since the September 16, 2014 meeting, the Petitioner has submitted a revised drawing reducing the area of the requested variation to only a proposed shower area. Attached is this revised drawing showing a reduced variation. Should the Village Board find in favor of this revised request, an amended motion would be in order. The material provided below was first presented to the Village Board for its September 16, 2014 meeting.

Thomas Dallas, as Trustee of the Thomas N. Dallas Trust and Evangelia A. Dallas, as Trustee of the Evangelia A. Dallas Trust, property owners of 5080 West North Shore Avenue ("Petitioner"), requested two variations to permit a one-story addition to the existing single-family home. These variations are concerning side yard setback and maximum lot coverage (the request to exceed maximum lot coverage was withdrawn during the Public Hearing). The proposed addition is for the purpose of adding a bedroom on the first floor of the existing single-family home.

The Petitioner recently experienced a fire damaging much of the existing home. The Petitioner is moving forward with restoration plans but also seeks approval of an addition to the home which triggers the requested variations. In submitting the request for variations, the Petitioner has indicated that the need for the first floor bedroom is to improve accessibility and mobility for the homeowner.

A review of Village records indicates that the existing home was constructed in approximately 1969. A permit was issued in 1981 for an addition; however, no drawings or plans are available for review to determine the extent of that addition.

Variation Requests

The Petitioner seeks relief from the minimum side yard setback requirements and, as initially requested, maximum lot coverage requirements, in order to construct the proposed 348-square-foot addition. The

Zoning Code requires that the minimum side yard setback be 5 feet or 10% of the lot width whichever is greater. The lot width of the subject property is 81 feet; therefore, the required side yard setback is 8 feet 1 3/16th inches. The proposed addition is set back from the side lot line by 6 feet 3 5/8th inches and so a side yard setback variation of 1 foot 9 9/16th inches is requested.

The Petitioner also initially sought a variation to exceed the maximum lot coverage. The Zoning Code establishes a maximum 60% lot coverage requirement. Pursuant to this regulation, the subject property may have a maximum lot coverage up to 6,418-square feet. The existing lot coverage of the subject property is 6,825.8-square feet and, therefore, the subject property currently exceeds the limitation on maximum lot coverage by 407.8-square feet. The requested variation would increase this existing nonconforming condition by an additional 342.7-square feet, for a total lot coverage variation of 750.5-square feet. As requested, the proposed addition would increase the total lot coverage to 7,168.5-square feet.

Since adoption of the 2008 Zoning Ordinance, the Village has considered several setback variations for single-family residential additions. While every project is unique, the Village has approved ten requests for relief from side yard setback requirements. These requests resulted primarily from the existing home encroaching into the required setback.

In fall of 2013, the property owner of 6643 Tower Circle Drive sought a side yard setback variation to construct a two-story addition for the purpose of installing an elevator in the existing single-family home. The property owner of 6643 Tower Circle Drive indicated the elevator was necessary in order to gain access to the entirety of the two-story home. The owner indicated accessibility and mobility issues as the primary reason for the elevator/addition.

Public Hearing

The Zoning Board of Appeals (ZBA) considered the variation requests on August 20, 2014. The ZBA first deliberated the request to exceed the maximum lot coverage. The ZBA expressed concern with permitting an increase to an existing nonconforming lot coverage condition. The Petitioner indicated that if the side yard setback request is approved, they would be willing to remove a portion of, or the entire patio, in order to not increase lot coverage. The ZBA concluded that expanding the nonconforming lot coverage is not appropriate. In response, the Petitioner withdrew their request for relief from the maximum lot coverage requirement.

The ZBA next discussed the request to permit the addition to encroach in to the required side yard. The Petitioner's architect, George Prosiliakos, indicated that the proposed addition has been designed with ADA accessibility standards in mind. Accordingly, the bedroom addition includes a bathroom which includes wider pathways and, thus, more area. Mr. Prosiliakos explained that the proposed bedroom and bathroom is a combination of the proposed 348-square-foot addition and a conversion of an existing spa room. Mr. Prosiliakos indicated that for this reason he believed the addition was as least obtrusive as possible.

At the Public Hearing, testimony was received from Mr. and Mrs. Hollander, 6715 North Navajo Avenue. The Hollanders opposed the addition indicating a concern that the addition will impede light into their home. They also indicated concern that existing landscaping on their property will die as a result of the addition. Mr. Hollander stated that he believes there are alternatives to the addition that have not been explored by the Petitioner. Mr. Hollander indicated more interior modifications should be explored rather than seeking approval for an addition that encroaches into the required setback.

Commissioner O'Brien asked the Petitioner the hardship necessitating the setback variation. Mr. Prosiliakos stated that the desire is to minimally impact the existing home and floor plan while providing a first floor bedroom and handicap accessible bathroom which resulted in the proposed addition. Commissioner Gordon asked the Petitioner if not for the fire would the Dallas' need to move due to the accessibility issues. Mr. Dallas indicated that no formal plans had been decided upon prior to the fire but they were reviewing their options. Mr. Dallas stated that after the fire the preferred option to add a bedroom on the first floor became clear.

Zoning Board of Appeals Recommendation

The ZBA noted the Petitioner's withdrawal of their variation request for lot coverage and proceeded to deliberate the requested side yard setback variation and the testimony received. A majority of ZBA members concluded that the side yard setback request met the standards to approve variations. The ZBA, by a 4-3 vote, recommends approval of the requested side yard setback variation. Those members of the ZBA dissenting from the majority position did not believe that the proposed addition was the only manner to address the Petitioner's accessibility and mobility concern and accordingly, they saw no hardship was present to grant relief from the required side yard setback requirements.

FINANCIAL IMPACT:

None

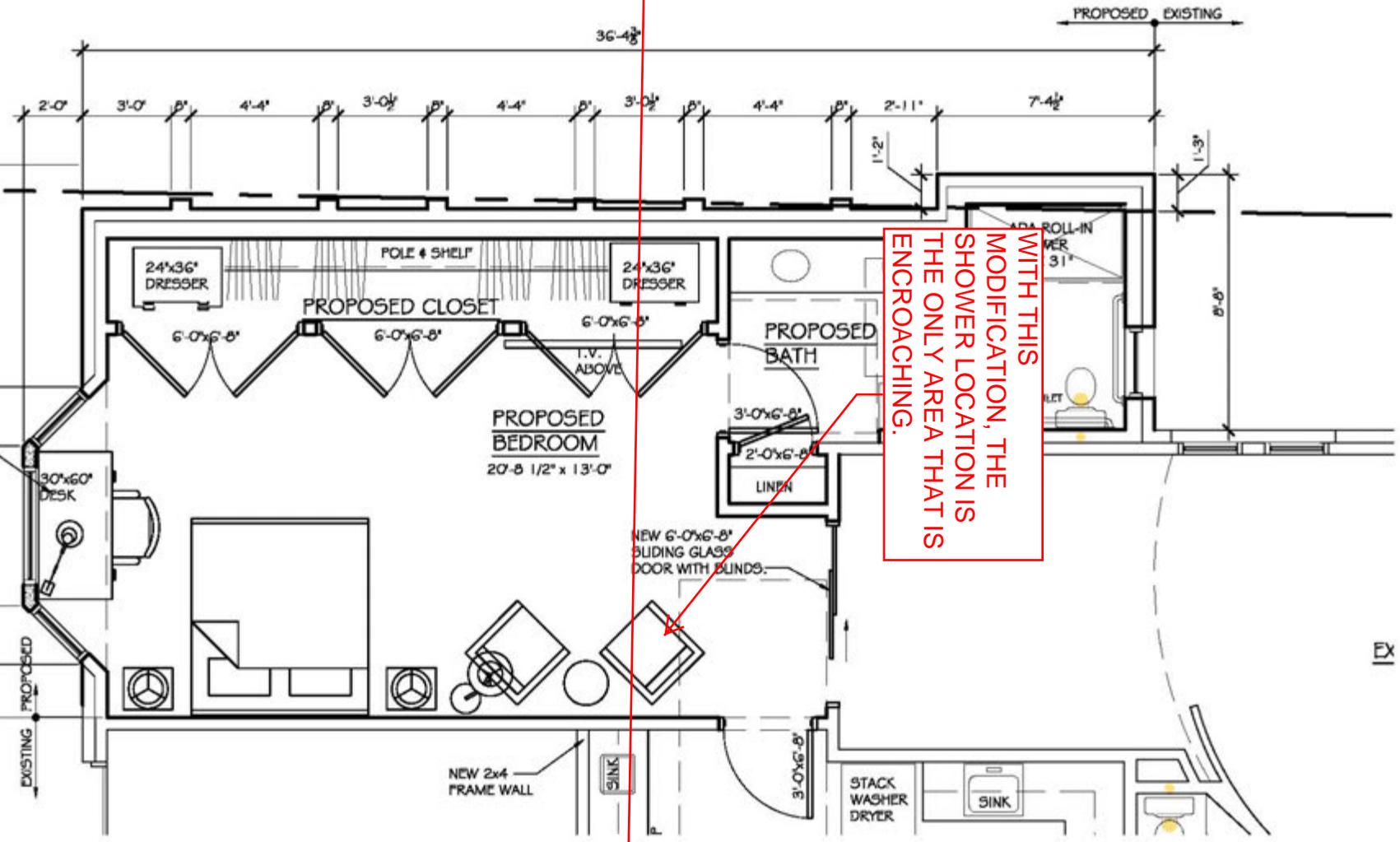
DOCUMENTS ATTACHED:

1. Revised Variation Drawing
2. August 20, 2014 ZBA Minutes
3. August 20, 2014 ZBA Packet
 - a. Staff Report to ZBA
 - b. Residential Zoning Variation Application
 - c. Zoning Calculations
 - d. Applicant Submitted Photographs of Property
 - e. Plat of Survey
 - f. Construction Drawings

RECOMMENDED MOTION:

Move to concur with the recommendation of the Zoning Board of Appeals to approve a request for a variation from Section 4.11 to permit the one-story addition to encroach into the required 8.1-foot side yard setback for the property located at 5080 West North Shore Avenue and to direct the Village Attorney to prepare an Ordinance for adoption consistent with this concurrence.

5080 W. NORTSHORE
FIRST FLOOR MASTER SUITE
REVISION
09-17-14



WITH THIS
MODIFICATION, THE
SHOWER LOCATION IS
THE ONLY AREA THAT IS
ENCROACHING.



**MEETING MINUTES OF THE
ZONING BOARD OF APPEALS
AUGUST 20, 2014 – 7:00 P.M.**

**LINCOLNWOOD VILLAGE HALL
COUNCIL CHAMBERS
6900 NORTH LINCOLN AVENUE
LINCOLNWOOD, ILLINOIS 60712**

MEMBERS PRESENT:

Chairman Herb Theisen
Paul Gordon
Paul Grant
Jean Ikezoe-Halevi
Martina Keller
Christopher Nickell
Kathy O'Brien

MEMBERS ABSENT:

None

STAFF PRESENT:

Aaron N. Cook, AICP, Community Development Manager

I. CALL TO ORDER

Chairman Theisen noted a quorum of seven members and called the meeting to order at 7:00 p.m.

II. PLEDGE OF ALLEGIANCE

III. APPROVAL OF MINUTES

Motion to recommend approval of the July 16, 2014 Meeting Minutes was made by Commissioner Grant, and seconded by Commissioner O'Brien.

Aye: Grant, O'Brien, Gordon, Ikezoe-Halevi, Keller, Nickell, and Theisen

Nay: None

Motion Approved: 7-0

IV. Public Hearing: 5080 West North Shore Avenue – Residential Variations

Chairman Theisen swore in the Petitioners and other audience members wishing to address the Zoning Board.

Mr. Cook presented the two requested residential variations for the first floor addition. The variations are to permit a first floor addition to the existing single-family home, including relief from the minimum side yard setback, and relief from the maximum lot coverage. The property is on a corner lot in the R-1 Residential Zoning District. There has been ten setback variations approved since 2010; however, those were approval of additions to match existing setbacks.

The 350-square-foot addition is on the north elevation of the single-family home for the purpose of installing a first floor bedroom. The property experienced a fire, and this variation has nothing to do with the repair and restoration of the existing home; this is above and beyond the scope of that work. The project is a remodeling of an existing spa and addition. The addition extends into the required eight-foot one-inch required side yard setback by one foot nine inches resulting in an increase to the required lot coverage requirements.

The applicant's request is to permit the interior side yard setback be changed from the required 8 foot 1 3/16 inches to 6 feet 3 5/8 inches and to permit the addition to exceed the 60% maximum lot coverage.

Mr. Fred Chapekis spoke on behalf of Mr. and Mrs. Dallas. Mr. Chapekis explained the necessity for Tom Dallas to require a first floor bedroom due to his disability and advancing age. There was a fire at the residence and they believed that if Mr. Dallas was at home when the fire started, there was a good chance that he may have not been able to exit the home safely in time. Another hardship is the plan to build the addition to meet ADA compliance standards. Without the variation, the bathroom entryway would not be able to accommodate a wheelchair if needed.

Mr. George Prosiliakos, the architect for Mr. and Mrs. Dallas, spoke of the intent to design and emulate the addition to match as close as possible to the existing residence. While they are aware that single-family homes do not need to be ADA compliant, they would like to build an addition that is barrier free if the need does arise in the future.

Commissioner O'Brien wanted to state for the record that she does not know the Dallas family directly, but does work with one of the family members on the School Board. Commissioner O'Brien also stated that she does not have a conflict of interest in deciding the matter before them.

Commissioner Keller also stated for the record that she is a next door neighbor of Mr. and Mrs. Dallas and she was the one of the neighbors who called 911 the night of the fire. She also stated that tonight's decision will have no direct impact on her and that she would be impartial.

Chairman Theisen asked if anyone in the audience had an issue with either Commissioner O'Brien or Commissioner Keller to proceed in this matter. Let the record state that no one in the audience had any objection.

Mr. and Mrs. David and Sylvia Hollander, 6715 North Navajo Avenue, live north of the Dallas residence and stated that they oppose the 1.9-foot increase. The structure already exceeds the maximum lot coverage and the fact that the present proposal is to go significantly greater than the 60% lot coverage with non-impervious surfaces is troublesome to them. The Hollanders also

oppose this addition because the proposed addition will completely cut off all natural light to their family room, living room, and office as well as jeopardizing their existing landscaping. They feel that this addition will negatively affect their property value.

A discussion ensued as to different building scenarios should their variation request be denied. Mr. Chapekis recommended that perhaps the 900-square-foot patio could be eliminated and replaced with a deck or sod which would then fall within the 60% lot coverage requirement. The addition could also be built without the 1.9-foot increase, but feel that would be a definite hardship with Mr. Dallas' disability.

Mr. Prosiliakos addressed the comments made by Mr. and Mrs. Hollander. He does not believe that this structure will decrease the natural light into their home to the degree that they mentioned. The proposed roof is not any higher than the existing structure and should not block out any more light than what occurs now. The peak of the roof is only 17 feet high. He also does not believe that this structure will damage any of the Hollander's landscaping.

Mr. Dallas addressed the Zoning Board that he would be willing to eliminate the patio to comply with the current Zoning Code in regards to lot coverage. They have made every effort for this addition to be less intrusive to their neighbors.

Chairman Theisen asked if anyone in the audience would like to speak regarding this Public Hearing. Let the record state that Mr. and Mrs. Hollander came forward with some additional comments.

Mr. and Mrs. Hollander vehemently disagree with the testimony presented. They do believe they will suffer if this addition is built. The Hollanders stated that they built an addition without a variance and would prefer that the building plan be modified.

Mr. Cook addressed the Hollanders that they did receive a side yard setback variation in 2008 for their addition. Mr. Cook wanted clarification to the Hollander's statement that they did not receive a variation for their rear yard addition.

Mr. Dallas then addressed the Zoning Board that there will be a distance of 15 feet between the wall of the proposed addition and the Hollander's home.

Commissioner Keller asked the Hollanders if they sought any professional consultation regarding their lighting concerns. They answered that they did not.

Commissioner O'Brien questioned the architect about rotating the bathroom space. Mr. Prosiliakos answered that by taking out some of the elements, it could be compliant.

Commissioner Gordon asked the architect if the existing house was ADA compliant and if the fire did not occur, would they have to move. Mr. Prosiliakos answered that the house was not ADA compliant and that without a first floor bedroom/bathroom, the house would have to be significantly reconfigured.

Commissioner Nickell asked Mr. Dallas if mobility concerns were addressed before the fire occurred. Mr. Dallas answered that over the last 18 months, his health has deteriorated and they discussed different options, including the addition of installing an elevator.

Motion to recommend approval for a Variation to reduce the side yard setback request for 5080 North Shore Avenue was made by Commissioner Nickell, and seconded by Commissioner Keller.

Aye: Nickell, Keller, Ikezoe-Halevi, and Theisen

Nay: Gordon, Grant, and O'Brien

Motion Approved: 4-3

Mr. Cook announced that this matter will be heard at the September 16, 2014 meeting of the Village Board. The Petitioners will have an opportunity to address the Board at that time.

V. ADJOURNMENT

Hearing no further business, **motion to adjourn** was made by Commissioner O'Brien, and seconded by Commissioner Nickell. Meeting adjourned at 8:10 p.m.

Aye: O'Brien, Nickell, Gordon, Grant, Ikezoe-Halevi, Keller, and Theisen

Nay: None

Motion Approved: 7-0

Respectfully Submitted,



Kathryn M. Kasprzyk
Community Development Coordinator



Staff Report

Zoning Board of Appeals

August 20, 2014

Subject Property:
5080 North Shore Avenue

Zoning District: R1 Residential

Petitioner: Thomas N. Dallas, as Trustee of the Thomas N. Dallas Trust and Evangelia A. Dallas, As Trustee of the Evangelia A. Dallas Trust - Property Owner

Requested Action: Variation sought to Article IV, Part C, Section 4.11 of the Zoning Code to reduce the required side yard setback and Article IV, Part C, Section 4.11 to exceed the maximum 60% permitted lot coverage.



Nature of Request: The property owner is seeking variations in order to allow an addition for the purpose of adding a first floor bedroom to the north side of the existing single-family home.

Notification: Notice in Lincolnwood Review dated July 31, 2014, Public Hearing Sign Installed at 5080 North Shore Avenue, and Mailed Legal Notices Dated July 30, 2014 to Properties within 250 Feet.

Summary of Request

The property owner of 5080 North Shore Avenue seeks approval to construct a one-story addition for the purpose of adding a bedroom on the first floor of the existing single-family home. The property owner recently experienced a fire damaging much of the existing home. The owner is moving forward with restoration plans and also seeks approval of the requested variations. The property owner has indicated in the responses to the variation standards that the need for the first floor bedroom is created by accessibility and mobility issues of the home owner.

The addition is 9' x 36' 4 3/8" (approx. 348.8 square feet) and is proposed at the north elevation. The proposed addition includes the conversion of an existing spa room. The property has a required side yard setback of 8 feet 1 3/16 inches (10% of lot width). The

proposed addition encroaches approximately 1 foot 9 9/16 inches into the required side yard. If approved, the addition would be setback from the north lot line by 6 feet 3 5/8 inches.

The proposed addition also requires a variation to exceed the maximum lot coverage. The Zoning Ordinance includes a maximum 60% lot coverage requirement. The subject property has a maximum lot coverage of 6,418.1 square feet (Lot Area: 10,696.86 square feet x .60 = 6,418.1 square feet). The addition increases an existing nonconforming condition relative to lot coverage. The existing lot coverage is 6,825.8 square feet. The proposed addition increases the lot coverage to 7,168.5. The property owner will be able to, without variation, restore the damaged home. The increase in lot coverage as a result of the proposed addition will require action by the Village Board.

The proposed addition complies with all other bulk regulations including but not limited to floor area ratio and building height.

Related Village Action

In fall of 2013 the property owner of 6643 Tower Circle Drive sought approval to construct a two-story addition for the purpose of installing an elevator in the existing single-family home. The property owner of 6643 Tower Circle Drive indicated the elevator was necessary in order to gain access to the entirety of the two-story home. The owner indicated accessibility and mobility issues as the primary reason for the elevator/addition.

Also, the Village in the past four years has considered several setback variations for single-family residential additions. While every project is unique the Village has approved ten requests for relief from side yard setback requirements. These requests resulted primarily from the existing home encroaching into the required setback.

Pertinent Property Information – The existing single-family home was constructed in or around 1969. The Village issued a permit for an addition in 1981 however no drawings or plans were available for review. A search of Village records resulted in no additional records or information regarding the subject property pertinent to this request.

Conclusion

The property owner is seeking a variation in order to allow a 348.8 square foot one-story addition at the north elevation of the existing single-family home. The addition is requested in order to add a bedroom on the first floor of the residence. As a result, variations to permit the addition within the required side yard setback and to exceed the maximum lot coverage are required.

Documents Attached

1. Residential Zoning Variation Application
 - a. Zoning Calculations
 - b. Applicant Submitted Photographs of Property
 - c. Plat of Survey
 - d. Construction Drawings



SUBJECT PROPERTY

Property Address: 5080 W. NORTSHORE

Permanent Real Estate Index Number(s): 10-33-402-058-0000

Zoning District R-1 Lot Area: 10,696.86

List all existing structures on the property. Include fencing, sheds, garages, pools, etc.

EXISTING 2-STORY MASONRY SINGLE FAMILY RESIDENCE TO REMAIN. EXISTING STRUCTURE TO BE RESTORED DUE TO FIRE DAMAGE.

Are there existing development restrictions affecting the property? ___ Yes X No
(Examples: previous variations, conditions, easements, covenants)

If yes, describe: _____

REQUESTED ACTION

- Variation - Residential
- Variation - Non-Residential
- Variation - Off-Street Parking
- Variation - Design Standards
- Variation - Signs/Special Signs
- Minor Variation
- Other

PROJECT DESCRIPTION

Describe the Request and Project: _____

THE PROJECT INCLUDES THE ELIMINATION OF THE EXISTING SOLAIRUM ON THE FIRST FLOOR. THE FOOTPRINT OF THE OLD SOLARIUM SHALL BE REPURPOSED FOR A NEW ONE STORY BEDROOM ADDITION WHICH WILL INCLUDE AN ADA COMPLIANT BEDROOM AND BATHROOM.

PROPERTY OWNER/PETITIONER INFORMATION

Property Owner(s): **THOMAS N. DALLAS, AS TRUSTEE OF THE THOMAS N. DALLAS TRUST DATED 01/26/06 and EVANGELIA A. DALLAS, AS TRUSTEE OF THE EVANGELIA A. DALLAS TRUST DATED 01/26/06**
Name: (List all beneficiaries if Trust): _____

MAILING Address: 6722 N. MINNEHAHA, LINCOLNWOOD, IL 60712

Telephone: (773 363-0385) Fax: (_____) E-mail Address: DIMITRA@ALAWPC.COM

Petitioner (if different from owner):

Name: _____ Relationship to Property: _____

Address: _____

Telephone: (_____) Fax: (_____) E-mail Address: _____

REQUIRED ATTACHMENTS *

Check all documents that are attached:

- Plat of Survey X
- Site Plan X
- Proof of Ownership X
- Floor Plans X
- Elevations X
- Applicable Zoning Worksheet X
- Photos of the property X
- PDF files of all drawings X

For Office Use Only

Fee: _____ Deposit: _____

Date Received: _____

Checked By: _____

The article(s), section(s) and paragraph(s) of the Village of Lincolnwood Zoning Ordinance from which the Action is being sought:

**The above documents are required for all applications. The Zoning Officer may release an applicant from specific required documents or may require additional documents as deemed necessary.*

COST REIMBURSEMENT REQUIREMENT

The Village requires reimbursement of certain out-of-pocket costs incurred by the Village in connection with applications for zoning approvals and relief. These costs include, but are not limited to, mailing costs, attorney and engineer costs, and other out-of-pocket costs incurred by the Village in connection with this application. In accordance with Section 5.02 of the Village of Lincolnwood Zoning Ordinance, both the petitioner and the property owner shall be jointly and severally liable for the payment of such out-of-pocket costs. Out-of pocket costs incurred shall be first applied against any hearing deposit held by the Village, with any additional sums incurred, to be billed at the conclusion of the hearing process.

Invoices in connection with this application shall be directed to:

Name: MR. THOMAS & EVANGELIA DALLAS

MAILING Address: 6722 N. MINNEHAHA

City, State: LINCOLNWOOD, IL 60712

ATTESTMENT AND SIGNATURE

I hereby state that I have read and understand the Village cost reimbursement requirement, as well as the requirements and procedures outlined in Article V of the Village Zoning Ordinance, and I agree to reimburse the Village within 30 days after receipt of an invoice therefor. I further attest that all statements and information provided in this application are true and correct to the best of my knowledge and that I have vested in me the authority to execute this application.

PROPERTY OWNER

PETITIONER *(If different than property owner)*

JULY 21, 2014

Signature Date

Signature Date

MR. THOMAS DALLAS

PRINT NAME

PRINT NAME



VILLAGE OF LINCOLNWOOD

COMMUNITY DEVELOPMENT DEPARTMENT

VARIATION STANDARDS

To be approved, each variation request must meet certain specific standards. These eight standards are listed below. After each listed standard, explain how your variation request satisfies the listed standard. Use additional paper if necessary.

1. The requested variation is consistent with the stated intent and purposes of the Zoning Ordinance and the Comprehensive Plan.

A. THE PUBLIC HEALTH, SAFETY, COMFORT, MORALS, CONVENIENCE, AND GENERAL WELFARE WILL NOT BE AFFECTED BY THE ONE STORY BEDROOM ADDITION. THE ADDITION WILL BE OF THE SAME CONSTRUCTION AND DESIGN AS THE EXISTING STRUCTURE.
B. SECURING NATURAL LIGHT, PURE AIR, AND SAFTY FROM FIRE AND OTHER DANGERS WILL NOT BE AFFECTED BY THE ONE STORY BEDROOM ADDITION. THE ROOF OF THE PROPOSED ADDITION WILL BE LOWER THAN THE EXISTING RESIDENCE.
C. THE TAXABLE VALUE OF THE LAND AND BUILDING WILL BE PRESERVED BY THE ONE STORY BEDROOM ADDITION. THE ADDITION WILL ENHANCE THE LAND AND BUILDING VALUE.
D. CONGESTION ON THE PUBLIC STREETS WILL NOT BE AFFECTED BY ONE STORY BEDROOM ADDITION. THERE WILL NOT BE ANY ADDITIONAL TRAFFIC AS A RESULT OF THE ADDITION.
E. THE ONE STORY BEDROOM ADDITION IS ONLY 348,8 SQUARE FEET IN SIZE. THUS THE ADDITION WILL BE A NOMINAL INCREASE IN THE TOTAL SQUARE FOOTAGE OF BUILDING AREA AND TOTAL LOT COVERATE. AN ACCUMULATION OR RUNOFF OF STORM OR FLOODWATERS IN NOT ANTICIPATED FROM THIS ADDTION.

2. The particular physical surroundings, shape or topographical conditions of the subject property would bring a particular hardship upon the owner, as distinguished from a mere inconvenience, if the strict letter of this Zoning Ordinance is enforced.

THE SUBJECT PROPERTY IS A CORNER LOT THUS HAS INHERENT DIFFICULTIES GIVEN THE SIDE YARD SETBACK REQUIREMENTS. WHEN THE EXISTING RESIDENCE WAS DEVELOPED BY THE PREVIOUS OWNERS, THE RESIDENCE WAS ERECTED CLOSE TO THE NORTH PROPERTY LINE LEAVING MORE ROOM ALONG THE SOUTH PROPERTY LINE. THERE IS OVER 20' OF SETBACK ALONG NORTHSORE AND MUCH LESS ALONG THE NORTH PROPERTY LINE. THE EXISTING DRIVEWAYS AND REAR YARD PATIO AS CONSTRUCTED BY THE PREVIOUS OWNERS ADD QUITE A BIT TO THE TOTAL LOT COVERAGE. THIS ADDITIONAL LOT COVERAGE MAKES THE EXISTING PROPERTY EXISTING NON-CONFORMING EVEN BEFORE THE ADDITION. THIS WAS NOT CREATED BY MR. AND MRS. DALLAS.

3. The conditions upon which the petition for the variation is based would not be applicable generally to other property within the same Zoning District.

THE EXISTING SOLARIUM WILL BE ELIMINATED TO ACCOMODATE THE NEW ONE STORY BEDROOM ADDITION. PART OF THE PROPOSED BEDROOM ADDITION WILL BE WHERE THE SOLARIUM IS CURRENTLY LOCATED THUS WITHIN THE EXISTING FOOTPRINT OF THE BUILDING. REPURPOSING THE AREA WHERE THE SOLARIUM IS LOCATED WILL ALLLOW THE PROPOSED BEDROOM ADDITION WILL ALLOW FOR MINIMUM ENCROACHMENT ONTO THE SIDE YARD. THE ONE STORY BEDROOM ADDITION THAT IS PROPOSED IS SPECIFIC TO THIS PARTICULAR PROPERTY, BUILDING, AND OWNER. IT IS NOT PROBABLE THAT THE SAME SET OF CIRCUMSTANCES WOULD BE GENERALLY APPLICABLE TO OTHER PROPERTY WITHIN THE SAME ZONING DISTRICT. SINCE 1967, MR. DALLAS HAS BEEN CHALLENGED WITH A PERMANENT PHYSICAL DISABILITY. HE HAS BEEN ABLE TO MANUEVER IN A 2-STORY RESIDENCE FOR THE PAST 25 YEARS. HOWEVER AS HIS AGE HAS ADVANCED, GOING UP AND DOWN STAIRS HAS BECOME A MORE DIFFICULT TASK. HIS ABILITY TO GO UP AND DOWN STAIRS WILL ULTIMATELY BECOME IMPOSSIBLE FOR HIM. THE NEED TO CREATE AN ADA COMPLIANT BEDROOM AND BATHROOM FACILITY FOR HIS FUTURE YEARS HAS BECOME A REQUIREMENT FOR HIM TO REMAIN IN HIS HOME.

4. The variation is not solely and exclusively for the purpose of enhancing the value of or increasing the revenue from the property.

THE PURPOSE OF THE BEDROOM AND BATHROOM ADDITION IS SOLELY FOR THE PURPOSES OF HANDICAPPED ACCESSIBILITY FOR THE OWNERS OF THE PROPERTY. THE ADDITION IS NOT FOR THE PURPOSES OF ENHANCING THE VALUE OF THE PROPERTY OR INCREASING THE REVENUE FROM THE PROPERTY.

VARIATION STANDARDS (continued)

5. The alleged difficulty or hardship has not been created by any person presently having an interest in the property.

MR. AND MRS. DALLAS HAVE ADVANCED IN AGE AND NOW FIND IT MORE DIFFICULT TO GO UP AND DOWN THE CURVED STAIRCASE TO ACCESS THE BEDROOMS UP STAIRS. IN ADDITION, MR. DALLAS HAS A PERMANENT PHYSICAL DISABILITY WHICH HAS BECOME EVEN MORE DIFFICULT RECENTLY TO CLIMB STAIRS. MR. AND MRS. DALLAS HAVE LIVED IN THIS HOUSE FOR OVER 25 YEARS, AND THEY INTEND ON LIVING THE DURATION OF THEIR LIVES IN THIS RESIDENCE.

AS A SIDE NOTE, TWO OF MR. DALLAS' THREE CHILDREN AS WELL AS THE BROTHER OF MR. DALLAS LIVE IN LINCOLNWOOD ONLY BLOCKS AWAY FROM THEIR OWN RESIDENCE. THE REASON FOR MENTIONING THIS IS TO POINT OUT ANOTHER REASON AS TO WHY MR. AND MRS. DALLAS WANT TO REMAIN IN THEIR HOME.

6. The granting of the variation will not be detrimental to the public welfare or injurious to other property or improvements in the neighborhood in which the property is located.

THE ONE STORY ADDITION IS ONLY ROUGHLY 348.8 ADDITIONAL SQUARE FEET AND WILL BE LOCATED ON AN INTERIOR NORTH LOT LINE. THE EXISTING AND CURRENT PUBLIC WELFARE THAT IS ENJOYED BY THE NEIGHBORING RESIDENTS AND PEDESTRIANS WILL NOT BE ALTERED OR AFFECTED BY THE PROPOSED BEDROOM ADDITION.

7. The variation granted is the minimum change to the Zoning Ordinance standards necessary to alleviate the practical hardship on the subject property.

AS A REQUEST OF THE OWNERS, THE ADDITION HAS BEEN DESIGNED WITH THE SPECIFIC INTENT TO BE ADA COMPLIANT; BARRIER FREE. THE PRIMARY REASON FOR THE ENCROACHMENT ONTO THE SIDE YARD SETBACK IS THAT THE NEW ADA COMPLIANT RESTROOM NEEDS TO BE LARGE ENOUGH TO ACCOMMODATE A FUTURE WHEEL CHAIR THAT IS BEING ANTICIPATED BY MR. DALLAS. MAKING THE PROPOSED RESTROOM SMALLER WOULD NOT ALLOW FOR ACCESSIBILITY OF A WHEEL CHAIR. THE ZONING VARIANCE REQUESTED IS THE MINIMUM REQUIRED TO ALLOW FOR THE RESTROOM TO BECOME ADA COMPLIANT.

8. The proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the danger of fire, or otherwise endanger the public safety, or substantially diminish or impair property values within the neighborhood.

THE PROPOSED ADDITION IS A ONE STORY STRUCTURE. THE PROPOSED ROOF LINE WILL BE LOWER THAN THE EXISTING ROOF LINE. THE ADEQUATE SUPPLY OF LIGHT AND AIR TO THE ADJACENT PROPERTY WILL NOT BE AFFECTED AS THE PROPOSED ROOF IS LOWER THAN THE EXISTING ROOF.

THE MATERIALS TO BE USED IN THE ERECTION OF THE ADDITION ARE CONSISTENT WITH THE MATERIALS IN THE EXISTING RESIDENCE. THE EXTERIOR OF THE ADDITION WILL BE BRICK TO MATCH THE EXISTING RESIDENCE. THE ADDITION HAS BEEN DESIGNED TO BLEND IN WITH THE EXISTING STRUCTURE INCLUDING MATCHING THE ROOF LINES, MATCHING BRICK AND BRICK DETAILS.

SIGN VARIATION STANDARDS

For all Sign Variation and/or Special Sign requests the Applicant shall also complete Questions 9-12.

9. The proposed variation is consistent with the statement of purpose set forth in Section 11.01 of the Zoning Ordinance.

NOT APPLICABLE

10. The proposed sign complies with any additional standards or conditions set forth in Article XI of the Zoning Ordinance.

NOT APPLICABLE

11. The proposed sign will substantially enhance the architectural integrity of the building or other structure to which it will be attached, if any.

NOT APPLICABLE

12. The proposed sign conforms with the design and appearance of nearby structures and signs.

NOT APPLICABLE



VILLAGE OF LINCOLNWOOD
BUILDING DEPARTMENT

Zoning Calculations
New Single-Family House

Please fill out this form for all permits involving New Single-Family Dwellings

ADDRESS 5080 W. NORTHSORE ZONING DISTRICT R-1

TOTAL LOT AREA = WIDTH 81' X LENGTH 132.06' = 10,696.86 Sq.Ft.

ZONING SETBACKS

FRONT YARD SETBACK

Required

Proposed

25 Ft.

30'- 5 3/8" Ft.

EXIST. NO CHANGE

VAR. INTERIOR SIDE YARD SETBACK

8'-1 3/16" Ft.

6'-3 5/8" Ft.

INTERIOR/CORNER SIDE YARD SETBACK (Please circle one)

10'-0" Ft.

21'-10 5/8" Ft.

EXIST. NO CHANGE

REAR YARD SETBACK

30' Ft.

30'-3 1/2" Ft.

EXIST. NO CHANGE

DISTANCE BETWEEN HOUSE AND ACCESSORY BLDG.

Minimum 15 Ft.

N / A Ft.

ZONING CALCULATION INSTRUCTIONS

VAR. 60% TOTAL LOT COVERAGE (Lot area)

10,696.86 X .60 = 6,418.1 Sq.Ft.

7,168.5 Sq.Ft. EXIST. = 6,825.8

35% BUILDING COVERAGE (Lot area)

10,696.86 X .35 = 3,743.9 Sq.Ft.

3,387.0 Sq.Ft. EXIST. = 3,043.2

50% FRONT YARD COVERAGE (Front yard)

2,487.6 X .50 = 1,243.8 Sq.Ft.

1,271.6 Sq.Ft. EXIST. NO CHANGE

30% REAR YARD BLDG. COV. (Rear yard)

2,480.9 X .30 = 744.27 Sq.Ft.

1,218.9 Sq.Ft. EXIST. NO CHANGE

60% or 66% F.A.R. (Lot area)

10,696.86 X .60 = 6,418.1 Sq.Ft.
(X .66)

5,543.2 Sq.Ft. EXIST. NO CHANGE

AREA CALCULATIONS

A. CELLAR OR BASEMENT

Proposed

1,938.2 Sq.Ft.

E. DRIVEWAY/APRON

Proposed

2,873.7 Sq.Ft.

B. 1ST FLOOR

3,392.0 Sq.Ft.

F. SIDEWALK

NONE Sq.Ft.

C. 2ND FLOOR

2,151.2 Sq.Ft.

G. PATIO/OTHER

902.8 Sq.Ft.

D. ACCESSORY BUILDINGS

N / A Sq.Ft.

IMPERVIOUS SURF.

TOTAL SQUARE FOOTAGE FOR PERMIT FEE CALCULATION

BASEMENT OR CELLAR + B + C + D + HABITABLE ATTIC = 5,543.2 Sq.Ft.

HOUSE HEIGHT (Measured from top of street curb)

Permitted

Maximum 35 Ft.

Proposed

27'-0 3/4" Ft.

FINISHED FIRST FLOOR HEIGHT (Measured from top of street curb)

Maximum 3 Ft.

EXIST. NO CHANGE Ft.

VARIATION REQUIRED

No

Yes

For INT. SIDE YARD & TOTAL LOT COVERAGE

The undersigned hereby acknowledges the accuracy of the above information and accepts full responsibility for any discrepancies or inaccuracies.

JULY 21, 2014

Licensed Architect Signature & Stamp

Date



TO DETERMINE MAXIMUM PERMISSIBLE SQUARE FOOTAGE AMOUNTS

- Total Lot Coverage *Area of the Lot x Maximum Permissible Amount*
- Total Building Coverage *Area of the Lot x Maximum Permissible Amount*
- Front Yard Coverage *Area of the Front Yard x Maximum Permissible Amount*
- Rear Yard Building Coverage *Area of the Rear Yard x Maximum Permissible Amount*

ZONING CALCULATION INSTRUCTIONS

60% TOTAL LOT COVERAGE > Add B + D + E + F +G =	<u>7,168.5</u> Sq.Ft.	EXIST. = 6,825.8
35% BUILDING COVERAGE > Add B + D =	<u>3,392.0</u> Sq.Ft.	EXIST. = 3,043.2
50% FRONT YARD COVERAGE > Add E + Front Yard Portion of F =	<u>1,271.6</u> Sq.Ft.	EXIST. NO CHANGE
30% REAR YARD BUILDING COVERAGE > D =	<u>1,218.9</u> Sq.Ft.	EXIST. NO CHANGE
60% OR 66% F.A.R. > Add B + C + D =	<u>5,543.2</u> Sq.Ft.	EXIST. NO CHANGE

TO CALCULATE REQUIRED FRONT YARD SETBACK

The required minimum front yard setback is 25 feet, or in line with the average of 40% of the existing homes on the block adjacent to the subject property, whichever is greater.

IMPERVIOUS SURFACE

This is the area of the lot which does not allow water to be absorbed so it may percolate into deeper ground. Impervious surfaces include the footprint of a home, Portland cement concrete, bituminous concrete, composed of stone or gravel, or any other surface that allows little or no water penetration.

TOTAL LOT COVERAGE

This is the part or percent of the lot occupied by buildings or structures, including accessory buildings or structures, and any other impervious surface.

Total Lot Coverage Allowance: the numerical value obtained by dividing the Lot Coverage by the Area of the Lot.

Maximum Permissible Total Lot Coverage: 60%

BUILDING COVERAGE

This is the area of the lot that is covered by the footprint of a principal building and any structure under a roof, including accessory buildings or structures, measured at grade, from the exterior faces of the exterior walls, but excluding decks, and terraces.

Building Coverage Allowance: the numerical value obtained by dividing the Building Coverage by the Area of the Lot.

Maximum Permissible Building Coverage: 35%



FRONT YARD COVERAGE

This is the area of the front yard that is covered with impervious surface.

Front Yard Coverage Allowance: the numerical value obtained by dividing the Front Yard Coverage by the area of the front yard.

Maximum Permissible Front Yard Coverage: 50%

REAR YARD BUILDING COVERAGE

This is the area of the rear yard that is covered by an accessory building(s).

Rear Yard Building Coverage Allowance: the numerical value obtained by dividing the Rear Yard Coverage by the area of the rear yard.

Maximum Permissible Rear Yard Coverage: 30%

TOTAL FLOOR AREA – For Determining Floor Area Ratio

This is the sum of the gross horizontal areas of all floors of a building or of such an area devoted to a specific use, measured from the exterior face of exterior walls.

Floor Area will include:

Interior features such as hallways, stairs, closets, elevator shafts, interior balconies, mezzanines, and enclosed porches. In all residential buildings, any space that has a floor to ceiling height of more than 14 feet, and any fraction thereof, shall be treated as a separate floor and will be counted twice when calculating floor area.

Floor Area will not include:

The first 250 square feet or 50% of the floor area of a garage, whichever is less.

The first 100 square feet of the area of a shed.

Total cellar square footage.

Total attic space square footage.

Chimneys that extend from the building exterior by two (2) feet or less.

FLOOR AREA RATIO (F.A.R.)

This is the numerical value obtained by dividing the “floor area” within a building or buildings on a lot by the area of such lot. The floor area ratio requirement as designated for each district when multiplied by the lot area in square feet shall determine the maximum permissible “floor area” for the building or buildings on the lot.

Maximum Permissible F.A.R.

For properties equal to or greater than 6,000 Sq.Ft.

.60

For properties less than 6,000 Sq.Ft.

.66 or 3,600 Sq.Ft.

(Whichever is less)

FINISHED FIRST FLOOR HEIGHT

This is the height of the first finished floor measured from the top of the street curb.

Maximum Permissible Finished Floor Height: 3 feet above top of curb





1. VIEW FROM NAVAJO AVE. AT LOCATION OF PROPOSED ADDITION.



2. VIEW FROM NORTH SHORE AVE. SOUTH ELEVATION.



3. VIEW FROM NAVAJO AVE. AT LOCATION OF PROPOSED ADDITION.



4. VIEW OF NORTH ELEVATION AT LOCATION OF PROPOSED ADDITION.



5. VIEW FROM NORTH YARD FACING WEST AT PROPOSED ADDITION.



6. VIEW OF NORTH ELEVATION AT LOCATION OF PROPOSED ADDITION.

DALLAS RESIDENCE
5080 NORTH SHORE AVE. LINCOLNWOOD, IL 60712

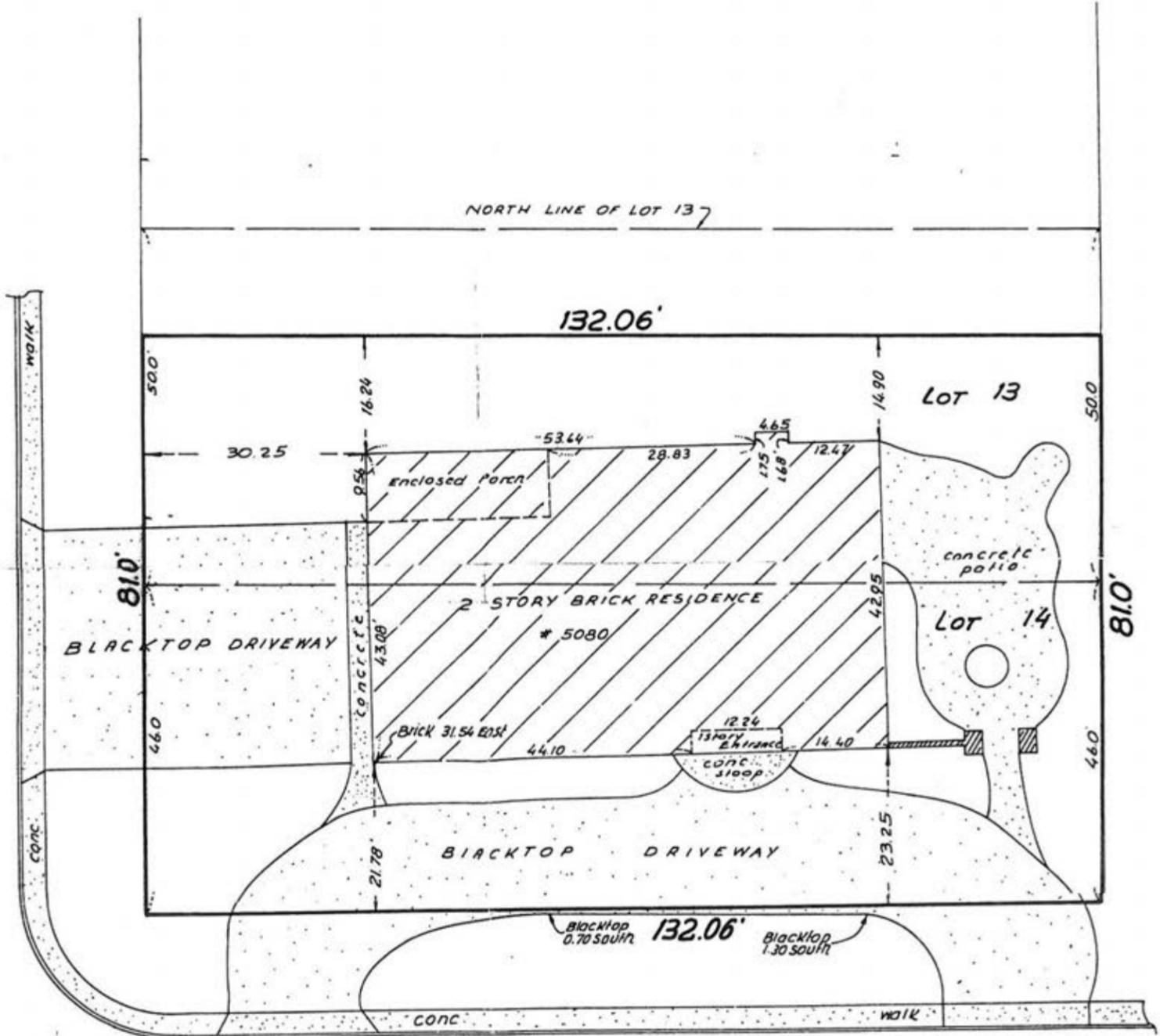
PLAT OF SURVEY



LOT 13 (EXCEPT THE NORTH 1/2 THEREOF), AND LOT 14 IN BLOCK 2 IN GUBBINS AND MCDONNELL'S SECOND EDGEBROOK GOLF ADDITION, OF LOT 7, 8 AND PART OF LOT 9 IN THE COUNTY CLERK'S DIVISION OF FRACTIONAL SECTION 33, TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS, ACCORDING TO THE PLAT THEREOF RECORDED MAY 31, 1928, AS DOUCMENT NUMBER 10041003 IN BOOK 259 OF PLATS, PAGE 27, TOGETHER WITH THE SOUTH 10 FEET OF THE NORTH 1/2 OF LOT 13 IN BLOCK 2 IN GUBBINS AND MCDONNELL'S SECOND EDGEBROOK GOLF ADDITION OF LOTS 7, 8 AND 9 (EXCEPT THE SOUTH 8.53 ACRES) IN THE COUNTY CLERK'S DIVISION OF FRACTIONAL SECTION 33, TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

AVE.

NAVAJO



NORTH SHORE

AVE.

✓ CHECK (✓) IN BOX MEANS THIS SURVEY HAS BEEN MADE FOR USE IN CONNECTION WITH A REAL ESTATE OR MORTGAGE LOAN TRANSACTION AND IS NOT TO BE USED FOR CONSTRUCTION. DIMENSIONS ARE NOT TO BE ASSUMED FROM SCALING

Order No. **890722**

Scale: feet

Date MAY 9, 1989

Ordered by KATHY PAGANI

BUILDING LINES AND EASEMENTS ARE SHOWN ONLY WHERE THEY ARE SO RECORDED IN THE MAPS, OTHERWISE REFER TO YOUR DEED OR ABSTRACT.

Decimals of a foot and their equivalent in inches and fractions thereof.

.01 = 1/8"	.07 = 7/8"	.50 = 6"
.02 = 1/4"	.08 = 1"	.58 = 7"
.03 = 3/8"	.17 = 2"	.67 = 8"
.04 = 1/2"	.25 = 3"	.75 = 9"
.05 = 5/8"	.33 = 4"	.83 = 10"
.06 = 3/4"	.42 = 5"	.92 = 11"

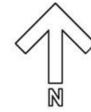
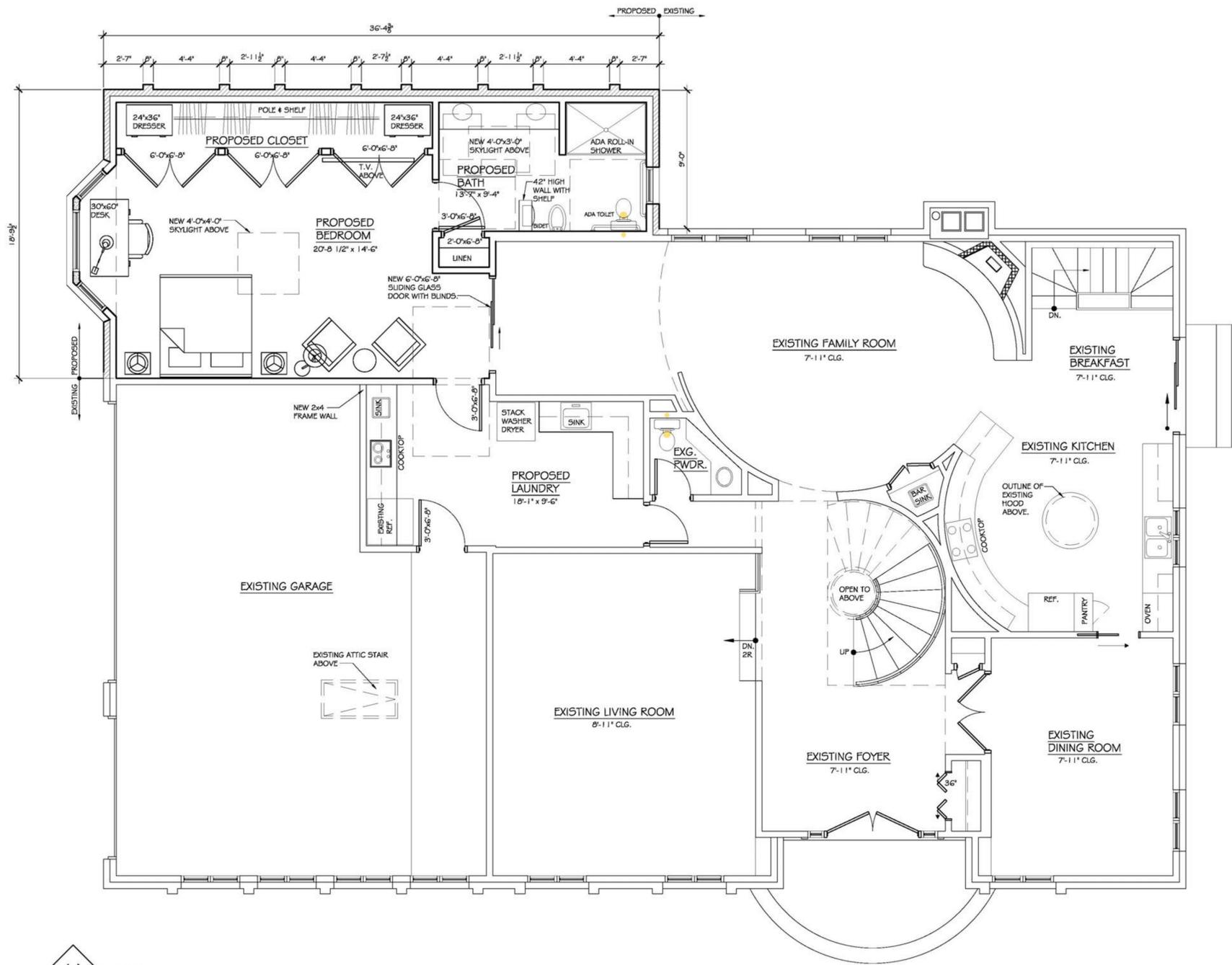


COMPARE ALL POINTS BEFORE BUILDING BY SAME AND AT ONCE REPORT ANY DIFFERENCE.

State of Illinois } ss.
County of Cook }

We, CERTIFIED SURVEY CO. do hereby certify that we have surveyed the above described property and that the plat hereon drawn is a correct representation of said survey.

Paul F. Misturak



1 PROPOSED FIRST FLOOR PLAN
1/4" = 1'-0"

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NO.	DATE	ISSUES & REVISIONS

PROSI DESIGN, INC.
ARCHITECTURE • PLANNING • INTERIOR DESIGN

7400 WALKERMAN ROAD, SUITE A-213
NILES, IL 60714
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FAX: 847.442.4200
email: Georgia@prosidep.com

PROSI DESIGN, INC. FIRM
LICENSE # 184-02748
EXP. 04/30/2015

DALLAS RESIDENCE
ADDITION

5080 W. NORTH SHORE AVE.
LINCOLNWOOD, IL 60712



DATE: _____

DRAWN BY: STAFF

CHECKED BY: GTP

A-1.2

ISSUED FOR ZONING REVIEW
07-21-2014

Request for Board Action

REFERRED TO BOARD: October 7, 2014

AGENDA ITEM NO: 5

ORIGINATING DEPARTMENT: Community Development

SUBJECT: Consideration of a Recommendation by the Plan Commission Concerning a Text Amendment to the Comprehensive Plan Concerning Residential Development within the Lincoln Avenue Corridor

SUMMARY AND BACKGROUND OF SUBJECT MATTER:

At the July 16, 2013 Committee of the Whole (COTW) meeting, the Village Board discussed residential use along the Lincoln Avenue corridor. The existing Zoning Code regulations allowing for residential development along portions of Lincoln Avenue were created as a result of the 2005 Lincoln Avenue Task Force (LATF) Report, which subsequently was made a part of the Village's Comprehensive Plan. Certain other recommendations found in the LATF Report, such as the treatment of other land uses, urban and building design requirements, and parking requirements have also been subsequently incorporated into the Zoning Code.

At the July 16, 2013 COTW meeting, discussion included a possible prohibition on all residential developments along Lincoln Avenue. At the conclusion of this discussion, the COTW directed the Plan Commission to hold a Public Hearing to fully review the Lincoln Avenue Task Force Report. As part of this referral, the Village Board requested specific consideration by the Plan Commission concerning residential development along the Lincoln Avenue corridor.

Public Hearing

At its September 11, 2013 meeting, the Plan Commission opened deliberations on the Lincoln Avenue Task Force Report. At this meeting, the Plan Commission requested that members of the Lincoln Avenue Task Force attend a future Commission meeting to discuss the intent of the Task Force recommendation relative to residential as well as its other recommendations.

At the October 23, 2013 meeting, the Plan Commission received testimony from three members of the LATF (LATF Chair James Persino, Renee Silberman, and Ken Klint). The three past members commented on the overall vision that was developed for the Lincoln Avenue Corridor including creating distinct business districts, a pedestrian friendly environment, changes needed in land uses to encourage more desirable uses, and ways to encourage private redevelopment along Lincoln Avenue.

The Commission continued its hearing on the LATF Report several times but did not again substantially discuss the LATF Report until its April 23, 2014 meeting. In deliberating this matter,

overall the Commission generally found that residential use along Lincoln Avenue could have merit and the Commission did not find a basis to outright reject the vision of the LATF for the corridor or to recommend the outright prohibition of this land use in the corridor. At this meeting, the Commission first discussed the possibility of recommending all residential uses along Lincoln Avenue be Special Uses rather than Permitted Uses. It was at this meeting where the Commission forwarded their recommendation that a moratorium on residential use in the Corridor *not* be implemented. Although this recommendation was considered by the Village Board on July 15, 2014, the Village Board imposed a 180-day moratorium on the issuance of building permits for construction of any new residential structure in the B-1 Zoning District along Lincoln Avenue. This moratorium is currently set to expire on January 12, 2015.

At the July 23rd Plan Commission meeting, the Plan Commission concluded that residential uses should be considered Special Uses within the Lincoln Avenue Corridor, rather than as Permitted Uses as is the current case for portions of the Corridor. No formal vote regarding amending the Comprehensive Plan, however, was taken. Rather, the Commission's determination to consider residential as a Special Use along Lincoln Avenue resulted in the Plan Commission directing the Village Attorney to draft new proposed text to slightly modify the LATF Report's vision concerning residential uses within the Corridor.

At the September 3, 2014 meeting, the Plan Commission reviewed this draft language prepared by the Village Attorney to slightly modify the vision of the LATF Report relative to residential uses. The Commission noted this matter concerns an amendment to the Village's Comprehensive Plan but that it calls for a change in the Zoning Code and, therefore, a referral to direct a Public Hearing for an amendment to the Zoning Code concerning residential along Lincoln Avenue should now be made.

Recommendation

At its September 3rd meeting, by a unanimous 6-0 vote, the Plan Commission recommends that text be added to the Lincoln Avenue Task Force Report portion of the Comprehensive Plan. This recommended change is to the "Modify Land Use Codes to Encourage New Uses" section. Below is the pertinent portion of this section with the recommended Commission change in bold and double-underline:

"In addition to permitting housing as part of certain mixed use developments, the plan also calls for the introduction of housing as an infill land use between the designated business hubs. Allowing housing as an in-fill land use will help demarcate the edges of the recommended business hubs and eliminate the current condition of a long, continuous strip of business properties. Moreover, development of in-fill housing between business hubs would visually extend the Village's residential neighborhoods into the corridor, thereby accentuating the residential persona of the community. In such in-fill areas, housing as a ground floor use should be permitted. Where housing is permitted, it is envisioned that residential uses in the corridor would be comprised of either multi-level townhouses or condominium buildings. **In all circumstances, residential uses should only be allowed on sites with sufficient lot size to support the impact of such development and only after site-specific review pursuant to the Village's zoning ordinance and other applicable laws.**

Recommendation: Modify the current land use code to allow for mixed use development and in-fill housing; and, to prohibit expansion of drive-thru operations and auto-oriented uses.”

To effectuate the Commission’s recommendation that residential be considered a Special Use in the corridor, the Plan Commission additionally recommends the Village Board refer for Public Hearing before the Plan Commission, a proposed Text Amendment to consider residential along Lincoln Avenue as a Special Use rather than as a Permitted Use.

FINANCIAL IMPACT:

None

DOCUMENTS ATTACHED:

1. Plan Commission Minutes
 - a. September 3, 2014 (Draft)
 - b. August 27, 2014
 - c. July 23, 2014 Excerpt
 - d. June 25, 2014 Excerpt
 - e. April 23, 2014 Excerpt
 - f. February 26, 2014 Excerpt
 - g. January 22, 2014 Excerpt
 - h. December 4, 2013 Excerpt
 - i. October 23, 2013 Excerpt
 - j. September 11, 2013 Excerpt
2. Staff Report to Plan Commission
 - a. Staff Memorandum dated September 3, 2014
 - b. Staff Memorandum dated July 23, 2014
 - c. Staff Memorandum dated April 23, 2014
 - d. Staff Memorandum dated October 23, 2013

RECOMMENDED MOTION:

Move to concur with the recommendation of the Plan Commission concerning a Comprehensive Plan Text Amendment, to direct the Village Attorney to prepare the requisite Ordinance to approve this change and to refer to the Plan Commission for Public Hearing, a proposed text amendment to the Zoning Code to make residential use along Lincoln Avenue a Special Use.



**DRAFT MEETING MINUTES OF THE
PLAN COMMISSION
SEPTEMBER 3, 2014 – 7:00 P.M.**

**LINCOLNWOOD VILLAGE HALL
COUNCIL CHAMBERS
6900 NORTH LINCOLN AVENUE
LINCOLNWOOD, ILLINOIS 60712**

MEMBERS PRESENT:

Chairman Paul Eisterhold
Irving Fishman
Patricia Goldfein (arrived at 7:04)
Steven Jakubowski (arrived at 7:06)
Anthony Pauletto
Mark Yohanna

MEMBERS ABSENT:

Don Sampen

STAFF PRESENT:

Aaron N. Cook, AICP, Community Development Manager

- I. CALL TO ORDER**
- II. PLEDGE OF ALLEGIANCE**
- III. APPROVAL OF MINUTES**

Chairman Eisterhold noted a quorum of four members and called the meeting to order at 7:03 p.m.

Motion to approve the August 6, 2014 Meeting Minutes was made by Commissioner Fishman, and seconded by Commissioner Yohanna.

Aye: Fishman, Yohanna, Pauletto, and Eisterhold

Nay: None

Motion Approved: 4-0

Motion to approve the August 22, 2014 Meeting Minutes was made by Commissioner Fishman, and seconded by Commissioner Pauletto.

Aye: Fishman, Pauletto, and Eisterhold

Abstained: Goldfein, Jakubowski, Pauletto, and Yohanna

Motion Approved: 3-0

IV. Public Hearing: Lincoln Avenue Plan – Amendment to the Comprehensive Plan

Mr. Cook presented draft language to amend the Comprehensive Plan relative to residential uses. The Village Board made a specific recommendation to the Plan Commission to make a change to the Zoning Code. This plan is to establish a long-term vision for the community along Lincoln Avenue. Mr. Cook recommended to the Plan Commission to direct a Public Hearing be held in the near future to incorporate this vision into the Zoning Code by way of a text amendment to change the current Permitted Use into a Special Use.

Staff and the Village Attorney believe the following should be added to the “Modify Land Use Codes to Encourage New Uses” section.

“In all circumstances, residential uses should only be allowed on sites with sufficient lot size to support the impact of such development and only after site-specific review pursuant to the Village’s zoning Ordinance and other applicable laws.”

Motion to approve the recommended language, as presented, to the Village Board was made by Commissioner Yohanna, and seconded by Commissioner Pauletto.

Aye: Yohanna, Pauletto, Fishman, Goldfein, Jakubowski, and Eisterhold

Nay: None

Motion Approved: 6-0

Motion to approve the Special Use language and other regulations as deemed appropriate and for the Village Board to consider a Public Hearing on this matter relative to the Zoning Code was made by Commissioner Yohanna, and seconded by Commissioner Fishman.

Aye: Yohanna, Fishman, Goldfein, Jakubowski, Pauletto, and Eisterhold

Nay: None

Motion Approved: 6-0

The next focus of discussion pertained to other recommendations relative to the Task Force’s report which encourages retail uses and no longer encourages bank or office uses along Lincoln Avenue. The Task Force recommends that these businesses be kept at a minimum or prohibited altogether as these businesses do not contribute to a pedestrian or shopping environment. Banks are currently prohibited in the B-1 District.

The Task Force also no longer encourages auto-oriented or auto-related businesses for the same reason as cited above. Auto body and repair, auto service establishments, and gas

stations are currently prohibited in the B-1 District. Auto/light truck sales and service establishments are currently a Special Use in within the B-1 District.

The Task Force recommends that the Village embraced mixed use developments, such as The Shoppes at Lincoln Pointe. North Capital Group is currently in the process of seeking approval for a mixed use development.

There was a lengthy discussion as to what uses should or should not be along Lincoln Avenue in relation to pedestrian traffic. The addition of more car dealerships was also discussed.

The next recommendation was Business Hub designation. This has been enacted by the implementation of Overlay Districts. The Task Force identified certain intersections as being unique in character. They are Lincoln and Touhy; Lincoln, Pratt, and Crawford; and Devon and Lincoln. Each intersection presents unique opportunities.

The Lincoln-Touhy Business Hub is envisioned as the downtown for the community. The Lincoln-Pratt-Crawford Business Hub is near the geographic center of the Village and the crossroads for Village residents. The Devon-Lincoln Business Hub is an entryway into the community and a major gateway to and from Chicago. Mr. Cook presented a map of the Recommended Land-Use Strategy for Lincoln Avenue Corridor Redevelopment.

Commissioner Pauletto suggested that two Commissioners each take a specific hub and make their recommendations. Commissioner Pauletto and Commissioner Yohanna volunteered to make recommendation regarding the Lincoln-Touhy Business Hub. Commissioner Fishman volunteered to make recommendations regarding the Lincoln-Pratt-Crawford Business Hub with Commissioner Sampen. Commissioner Goldfein and Commissioner Jakubowski volunteered to make recommendations regarding the Devon-Lincoln Business Hub.

Chairman Eisterhold asked if anyone in the audience would like to address this Commission. Let the record state that no one came forward.

V. NEXT MEETING

Chairman Eisterhold declared that the next Plan Commission meeting will be held on Wednesday, October 22, 2014.

Mr. Cook presented the Plan Commission's Biennial Report. This report will be reviewed by the Village Board at the Committee of the Whole Meeting scheduled for Tuesday, October 21, 2014. Chairman Eisterhold indicated that he would be present at this meeting. Chairman Eisterhold asked if any of the Commissioners had any comments or additions in regards to this report. There were none.

VI. ADJOURNMENT

Hearing no further business, **motion to adjourn** was made by Commissioner Yohanna, and seconded by Commissioner Fishman. Meeting adjourned at 8:15 p.m.

Aye: Yohanna, Fishman, Goldfein, Jakubowski, Pauletto, and Eisterhold

Nay: None

Motion Approved: 6-0

Respectfully Submitted,

Kathryn M. Kasprzyk
Community Development Coordinator

MINUTES OF AUGUST 27, 2014
MEETING OF THE LINCOLNWOOD
PLAN COMMISSION

Due to a lack of a quorum, Chairman Eisterhold announced that the August 27, 2014 regular meeting of the Plan Commission was continued to the September 3, 2014 regular meeting.

Aaron N. Cook, Development Manager

Chairman Eisterhold asked if anyone in the audience would like to speak on this matter. Nobody came forward.

Commissioner Sampen expressed his belief that there are way too many provisions attached to the draft amendments, and that he did not support the amendments.

Motion to recommend approval of the revised text amendments was made by Commissioner Jakubowski, with the added provision that minors not be allowed without being accompanied by an adult, and seconded by Commissioner Pauletto.

Aye: Jakubowski, Pauletto, Fishman, Goldfein, and Eisterhold

Nay: Sampen and Yohanna

Motion Approved: 5-2-0

V. Public Hearing: Lincoln Avenue Plan – Amendment to Comprehensive Plan

Mr. Cook highlighted the primary items for discussion having to do with the Lincoln Avenue Task Force relative to residential uses along Lincoln Avenue. The Plan Commission previously made a recommendation to the Village Board not to adopt a moratorium on residential development on Lincoln Avenue. At the July 15, 2014 Village Board meeting, Village Trustees enacted a 180-day moratorium on residential development.

Mr. Cook noted that the current Lincoln Avenue Plan encourages housing as an in-fill land use between business hubs. Mr. Cook explained that housing is not only encouraged in the Plan, but that it was found to be a development and use type that can be done on an in-fill manner between designated business hubs. The Plan further encourages mixed use development with residential uses on upper floors. The Plan includes a recommendation to “modify the current land use code to allow for mixed use development and in-fill housing and to prohibit expansion of drive-thru operations and auto-oriented uses”.

The second recommendation is to designate business district hubs at the intersections of Lincoln and Touhy; Lincoln, Pratt, and Crawford; and Devon and Lincoln. The report also encourages residential units above ground floor in the Lincoln-Touhy Business Hub and the Lincoln, Pratt, and Crawford Business Hub and recommends to “modify the Village Zoning Code to designate and regulate uses and encourage development in newly established district hubs”.

Commissioner Fishman would like to see any future development along Lincoln Avenue require a Special Use so the Plan Commission can be sure that the site is appropriate. Neighboring communities have revitalized their downtowns by implementing mixed use developments. A discussion regarding the viability of the Plan’s vision ensued, concerning whether this type of development would thrive in the current economic climate.

Mr. Cook reminded the Commissioners that the Comprehensive Plan is the Village’s long-range vision. If Zoning Code changes are desired, such as a Special Use instead of a Permitted Use, minimum lot areas can also be established for residential developments to even be able to apply for Special Use consideration.

Commissioner Yohanna questioned staff regarding the use of TIF funds. Mr. Passman stated that state law restricts the use of TIF funding for residential developments.

Staff recommended that the Commission focus on modifying the language for the Comprehensive Plan and on any initial recommended Zoning Code changes, all of which will be forwarded to the Village Board for its consideration.

Chairman Eisterhold asked if anyone in the audience would like to speak on this matter. Nobody came forward.

Motion to continue the public hearing to the Plan Commission meeting scheduled for Wednesday, August 27, 2014 was made by Commissioner Yohanna, and seconded by Commission Jakubowski.

Aye: Yohanna, Jakubowski, Fishman, Goldfein, Pauletto, Sampen, and Eisterhold

Nay: None

Motion Approved: 6-0

VI. NEXT MEETING

Chairman Eisterhold declared that the next Plan Commission meeting will be held on Wednesday, August 6, 2014.

VII. ADJOURNMENT

Hearing no further business, **motion to adjourn** was made by Commissioner Yohanna, and seconded by Commissioner Goldfein. Meeting adjourned at 9:00 p.m.

Aye: Yohanna, Goldfein, Fishman, Jakubowski, Pauletto, Sampen, and Eisterhold

Nay: None

Motion Approved: 6-0

Respectfully Submitted,

Kathryn M. Kasprzyk
Community Development Coordinator

Aye: Sampen, Jakubowski, Auerbach, Fishman, Goldfein, and Eisterhold

Nay: None

Motion Approved: 6-0

V. Public Hearing: Lincoln Avenue Plan – Amendment to Comprehensive Plan

Motion to continue the public hearing to the Plan Commission meeting scheduled for Wednesday, July 23, 2014 was made by Commissioner Sampen, and seconded by Commissioner Jakubowski.

Aye: Sampen, Jakubowski, Auerbach, Fishman, Goldfein, and Eisterhold

Nay: None

Motion Approved: 6-0

Commissioner Auerbach announced that she has tendered her resignation as a member of the Plan Commission.

VI. NEXT MEETING:

Chairman Eisterhold declared that the next Plan Commission meeting will be held on Wednesday, July 2, 2014.

VII. ADJOURNMENT:

Hearing no further business, **motion to adjourn** was made by Commissioner Sampen, and seconded by Commissioner Auerbach. Meeting adjourned at 9:30 p.m.

Aye: Sampen, Auerbach, Fishman, Goldfein, Jakubowski, and Eisterhold

Nay: None

Motion Approved: 6-0

Respectfully Submitted,



Kathryn M. Kasprzyk
Community Development Coordinator

Staff concurred that yes, they are onerous and the current standards are described only as being of a “high quality” material. Further, Staff stated that the goal of the Plan Commission is to identify higher design treatments for nonresidential buildings facing any residential areas. Chairman Eisterhold noted that the Plan Commission was unanimous in their support of applying a Special Use requirement for the use of precast materials.

Commissioner Goldfein suggested to Staff if the word aggregate could be included to the precast stone definition. Staff agreed that that will be done.

Motion to Approve Text Amendment relative to commercial design requirements as presented by Staff in the Staff Report dated April 23, 2014 plus the use of precast stone adjacent to residential areas shall be a Special Use and reconfirm previous recommendations from the March 6, 2013 meeting was made by Commissioner Fishman, and Seconded by Commissioner Goldfein.

Aye: Fishman, Goldfein, Jakubowski, Sampen, and Eisterhold

Nay: None

Motion Approved: 5-0

V. Public Hearing: Lincoln Avenue Plan – Amendment to Comprehensive Plan
*(Continued from April 2, 2014, February 26, 2014, January 22, 2014,
December 4, 2013, and October 23, 2013)*

The discussion encompassed whether there should be a moratorium on multi-family housing within the Lincoln Avenue District. Commissioner Fishman stated that he does not agree with a moratorium and that there are buildable sites along Lincoln Avenue that would be appropriate.

Commissioner Sampen agreed with Commissioner Fishman that there should be no moratorium on multi-family housing, and questioned whether or not multi-family house should require a Special Use instead.

Commissioner Jakubowski questioned if the Shoppes at Lincoln Pointe would have an impact on development of Lincoln Avenue. Commissioner Jakubowski also agreed that there should be no moratorium and concurred with the Special Use application.

Motion to Not Recommend the implementation of a moratorium relative to the Lincoln Avenue Corridor and multi-family housing made by Commissioner Fishman, and Seconded by Commissioner Jakubowski.

Aye: Fishman, Jakubowski, Goldfein, Sampen, and Eisterhold

Nay: None

Motion Approved: 5-0

Regarding the Amendment to the Comprehensive Plan, Commissioner Sampen asked Staff to provide an overview of the key items to be discussed.

Commissioner Jakubowski asked Staff if there were cost estimates regarding the implementation of the Comprehensive Plan and, if so, how does the Village plan to fund these changes. Staff explained that this is an overview of the Village's vision for the future and there are no concrete costs associated with this Plan.

Commissioner Goldfein asked if Mr. Cook could add the Special Use language to the moratorium decision. Staff agreed to this request.

Chairman Eisterhold asked if anybody in the audience would like to address the Plan Commission in any of the above matters. Let the record state that no one came forward.

Motion to Continue to the June 25, 2014 Plan Commission Meeting was made by Commissioner Sampen, and Seconded by Commissioner Fishman.

Aye: Sampen, Fishman, Goldfein, Jakubowski, and Eisterhold

Nay: None

Motion Approved: 5-0

VI. NEXT MEETING:

Chairman Eisterhold declared that the next Plan Commission Meeting will be on Wednesday, May 7, 2014.

VII. ADJOURNMENT:

Hearing no further business, **Motion to Adjourn** was made by Commissioner Sampen, and Seconded by Commissioner Goldfein. Meeting adjourned at 8:45 p.m.

Aye: Sampen, Goldfein, Fishman, Sampen, and Eisterhold

Nay: None

Motion Approved: 5-0

Respectfully Submitted,

Kathryn M. Kasprzyk
Community Development Coordinator

Aye: Yohanna, Jakubowski, Auerbach, Goldfein, Sampen, and Eisterhold
Nay: None
Motion Approved: 6-0

VI. PUBLIC HEARING: Lincoln Avenue Plan – Amendment to Comprehensive Plan
(Continued from January 22, 2014, December 4, 2013, and October 23, 2013)

Motion to Approve Fence Material Regulations was made by Commissioner Yohanna, and Seconded by Commissioner Jakubowski.

Aye: Yohanna, Jakubowski, Auerbach, Goldfein, Sampen, and Eisterhold
Nay: None
Motion Approved: 6-0

VIII. NEXT MEETING:

Chairman Eisterhold declared that the next Plan Commission Meeting will be held Wednesday, April 2, 2014.

IX. ADJOURNMENT:

Hearing no further business, **Motion to Adjourn** made by Commissioner Auerbach, and Seconded by Commissioner Yohanna. Meeting adjourned at 9:25 p.m.

Aye: Auerbach, Yohanna, Goldfein, Jakubowski, Sampen, and Eisterhold
Nay: None
Motion Approved: 6-0

Respectfully Submitted,



Kathryn M. Kasprzyk
Community Development Coordinator

Aye: Auerbach, Goldfein, Fishman, Sampen, Yohanna, and Eisterhold

Nay: None

Motion Carried: 6-0

VI. PUBLIC HEARING: Lincoln Avenue Plan – Amendment to Comprehensive Plan

(Continued from December 4, 2013 & October 23, 2013)

Request: Moratorium and Amendment to Comprehensive Plan Concerning the Lincoln Avenue Subarea Including but Not Limited to Multi-Family Housing

Motion to Continue to February 26, 2014 by Commissioner Auerbach, and Seconded by Commissioner Goldfein.

Aye: Auerbach, Goldfein, Fishman, Sampen, Yohanna, and Eisterhold

Nay: None

Motion Carried 6-0

VII. PUBLIC HEARING: Fence Material Regulations - Zoning Code Text Amendment

(Continued from December 4, 2013 & November 6, 2013)

Request: Text Amendment to Consider Unacceptable Fence Materials and Fence Design Requirements

Motion to Continue to February 5, 2014 by Commissioner Auerbach, and Seconded by Commissioner Goldfein.

Aye: Auerbach, Goldfein, Fishman, Sampen, Yohanna, and Eisterhold

Nay: None

Motion Carried 6-0

VIII. NEXT MEETING:

Chairman Eisterhold declared the next Plan Commission meeting will be held on Wednesday, February 5, 2014.

IX. ADJOURMENT:

Hearing no further business, **Motion to Adjourn** made by Commissioner Fishman, and Seconded by Commissioner Goldfein. Meeting adjourned at 9:36 p.m.

Aye: Fishman, Goldfein, Auerbach, Sampen, Yohanna, and Eisterhold

Nay: None

Motion Carried: 6-0

Respectfully Submitted,

Kathryn M. Kasprzyk
Community Development Coordinator

Motion to Continue the matter to the January 8, 2014 regularly scheduled meeting by Commissioner Auerbach and seconded by Commissioner Fishman.

Aye: Auerbach, Fishman, Goldfein, Sampen, Yohanna, and Eisterhold

Nay: None

Motion Carried 6-0.

Chairman Eisterhold requested that two Plan Commission meetings will be held in both January and February. The Commission members concurred with the request .

Chairman Eisterhold asked Staff if they could notify Mrs. Glickman regarding the date of the next Plan Commission meeting where the fencing issue will be discussed.

**IV. PUBLIC HEARING: Accessory Structures – Zoning Code Text Amendment
(Continued from September 11, 2013)**

**V. PUBLIC HEARING: Definitions – Zoning Code Text Amendment
(Continued from October 2, 2013)**

**VI. PUBLIC HEARING: Lincoln Avenue Plan – Amendment to Comprehensive
Plan (Continued from September 11, 2013)**

Chairman Eisterhold asked for consensus to continue the remaining three agenda items to the January 22, 2014 Plan Commission meeting. The Commission concurred and voted to continue the three matter by one roll call vote.

Motion to Continue the matter to the January 22, 2014 regularly scheduled meeting by Commissioner Fishman and seconded by Commissioner Auerbach.

Aye: Fishman, Auerbach, Goldfein, Sampen, Yohanna, and Eisterhold

Nay: None

Motion Carried 6-0.

IX. NEXT MEETING

Chairman Eisterhold declared the next Plan Commission meeting will be held on Wednesday, January 8, 2014.

X. PUBLIC COMMENT

None.

XI. ADJOURNMENT:

Hearing no further business, Motion to adjourn made by Commissioner Auerbach and seconded by Commissioner Fishman. Meeting adjourned at 10:05 p.m.



**MEETING MINUTES
OF THE
LINCOLNWOOD PLAN COMMISSION
October 23, 2013
7:00 P.M.**

**LINCOLNWOOD VILLAGE HALL
COUNCIL CHAMBERS
6900 NORTH LINCOLN AVENUE
LINCOLNWOOD, ILLINOIS**

MEMBERS PRESENT:

Chairman Paul Eisterhold
Irving Fishman
Sue Auerbach
Mark Yohanna
Patricia Goldfein

MEMBERS ABSENT:

Don Sampen
Steve Jakubowski

STAFF PRESENT:

Community Development Manager Aaron N. Cook, AICP
Community Development Director Timothy M. Clarke, AICP
Village Manager Timothy Wiberg

VILLAGE COUNSEL:

Hart Passman

OTHER VILLAGE REPRESENTATIVES PRESENT:

Trustee Klatzco
Trustee Patel
Village Clerk Herman
EDC Chair Persino

DEVELOPMENT TEAM REPRESENTATIVES PRESENT:

Neal Stein, North Capital Group
Ross Glickman, Urban Retail Properties
Joe Antunovich, Antunovich Architects
Greg Gorski, Antunovich Architects
Tony Gosetti, Antunovich Architects
Luay Aboona, KLOA
Jerry Callaghan, Freeborn & Peters

I. CALL TO ORDER

Chairman Eisterhold noting a quorum of five members present, called the meeting to order at 7:05 p.m. Chairman Eisterhold announced that Commissioner Sampen would not be attending tonight's proceedings due to a schedule conflict.

II. PLEDGE OF ALLEGIANCE

III. Approval of Minutes

Motion to approve the September 24, 2013 Plan Commission workshop minutes was made by Commissioner Goldfein and Seconded by Commissioner Yohanna. Motion approved 4-0-1 with Commissioner Auerbach abstaining.

IV. Public Hearing: Lincoln Avenue Plan – Amendment to Comprehensive Plan

Chairman Eisterhold announced the hearing and identified members of the Task Force who were present to participate in the discussion. Mr. Ken Klint, Ms. Renee Silberman, Mr. James Persino introduced themselves.

Mr. Clarke identified the purpose of the referral by the Village Board is to review the entire Lincoln Avenue Task Force Plan. Mr. Clarke added that the referral includes a more specific review of residential along Lincoln Avenue particularly south of Pratt Avenue. Mr. Clarke added that the Village Board also seeks Plan Commission consideration on the appropriateness of adopting a moratorium on residential development while the review of the report occurs. Mr. Clarke presented the history of the focus, process, vision, and outcome of the Task Force. Mr. Clarke reviewed several projects on Lincoln Avenue that were built after the adoption of the LATF Report.

Commissioner Fishman commented indicated that Walgreen's may not have been developed with the LATF Report because the project began prior to the adoption of the Report. Commissioner Fishman stated that has passed and disappointed that what the plan was intended to achieve has not happened. Commissioner Fishman indicated that he believes that the properties on the east side of Lincoln Avenue are different than west side. Commissioner Fishman explained that the properties on the west side have depth where the east side does not. Commissioner Fishman expressed that he does not believe a moratorium is appropriate at this time. Commissioner Fishman stated that condos have provided growth in other communities. Commissioner Fishman stated that he is not in favor of moratoriums.

Chairman Eisterhold stated that the members of the Task Force are present and that the time should be used to hear their comments on the plan.

Mr. Persino agreed that moratoriums are not positive and should not be considered at this time. Mr. Persino indicated that the commercial and residential hub concept was developed after looking at the issue of shallow and odd shaped sites along Lincoln Avenue. Mr. Persino stated that he believes that residential should still be considered. Mr. Persino expressed concern with build-to lines when coupled with building heights. Mr. Persino recommended reviewing a regulation to require buildings two stories or greater to have additional setback requirements in order to avoid "cannon effect". Mr.

Persino indicated that it may be appropriate to review the hubs and the permitted developments within those hubs. Mr. Persino stated that the economic downfall has been responsible for lack of development along Lincoln Avenue as well as odd shaped lots. Mr. Persino identified that in his opinion, lack of development is due to property owners with unrealistic property values. Mr. Persino supported the hub concept as a good idea for the community.

Ms. Silberman recalled that the plan was intended to avoid block after block of car dealerships and other automobile related businesses. Ms. Silberman stated that the Task Force wanted to promote development that is pedestrian friendly and to make crossing Lincoln safer. Ms. Silberman expressed concern that if a moratorium is not adopted that Lincolnwood would be forced into accepting the first applications made in an improving economic climate. Ms. Silberman stated that if the plan isn't reviewed there may be negatives to the community. Ms. Silberman indicated her disappointment in the proposal at Lincoln and Touhy Avenues. Ms. Silberman indicated that she doesn't want Village to be up for sale to first buyers and wants to make sure the community remains upscale. Ms. Silberman stated her general support of a moratorium.

Mr. Klint stated that there were struggles to develop along Lincoln Avenue with the most common new development proposals were car dealerships, car washes, and drive-through banks. Mr. Klint explained that the Task Force began a process to try to guide more development of more desirable uses. Mr. Klint stated that he is unsure if the Task Force envisioned the high density residential that was shown in staff's presentation. Mr. Klint supported a review of the plan and modifications as needed but the plan should not be eliminated. Mr. Klint indicated that the Task Force hoped to encourage land assemblage as well as for the review of making cul de sac streets.

The three former members of the Lincoln Avenue Task Force continued to review some of the recommendations of the plan as well as the goals. Discussion continued regarding the proposed Lincoln and Touhy Avenue development.

Chairman Eisterhold asked how mixed use developments are being perceived in the construction industry. Mr. Persino stated that smaller mixed use projects are difficult. Mr. Persino explained that is why there is an allowance for first floor residential in certain hubs. Mr. Persino agreed that there have been challenges in selling/leasing commercial space while the residential were fully occupied.

Commissioner Fishman stated that planning process and recommendations should be realistic and the property values in Lincolnwood are comparable to Skokie and Niles. Commissioner Fishman stated that planning does not bring development, the marketplace brings development. Commissioner Fishman stated that pedestrians along the entirety of Lincoln Avenue will not happen in near future. Mr. Persino stated that he believes the new development will trigger redevelopment.

Commission Yohanna asked the width of Belmont Avenue in which Mr. Persino described a "cannon effect" previously. Mr. Persino stated that Belmont Avenue is two

drive lanes with a parking lane on each side. Mr. Yohanna asked if Belmont Avenue is as wide as Lincoln Avenue. Mr. Persino stated that Lincoln Avenue is wider.

Chairman Eisterhold asked if there was anyone in the audience that wished to address the matter. There were none.

Motion to Continue the matter to the December 4, 2013 regularly scheduled meeting by Commissioner Yohanna and Seconded by Commissioner Fishman.

Aye: Yohanna, Fishman, Auerbach, Goldfein, and Eisterhold

Nay: None

Motion Carried 5-0.

The Commission went into a public meeting for the purposes of holding a workshop to discuss the concept plan for redevelopment at the former Purple Hotel site.

III. Workshop: Concept Plan for Purple Hotel Site

Mr. Callahan introduced how their presentation will proceed. Mr. Callahan indicated that there have been some modifications to the site plan. Mr. Callahan indicated that they have also attended several community meetings in addition to the Plan Commission Workshops.

Mr. Stein announced that Marriott International approved a Springhill Suites franchise license at the project. Mr. Stein indicated that green roofs were identified as desirable by those during community meetings. Mr. Stein added that traffic concerns were also raised. Mr. Stein indicated that residents along Chase Avenue wondered if blocking Chase at Lincoln Avenue is needed. Mr. Stein stated that he is not in favor of the idea of cutting street off completely because of development. Mr. Stein explained that the proposed residential building has been eliminated from the plans. Mr. Stein stated that the residential was included because the Lincoln Avenue Task Force report identified residential. Mr. Stein acknowledged that the desire for residential on Lincoln Avenue may be changing. Mr. Stein indicated that they now believe it is best to maintain office building which would be included in the PUD.

Mr. Glickman presented an update on the leasing of approximately 190,000 square feet of space. Mr. Glickman advised of discussion with grocers, health clubs, restaurants and retailers. Mr. Glickman stated that they have 10 letters of intent. Mr. Glickman stated that there is more interest from restaurants than the available restaurant space within the development.

Chairman Eisterhold asked about previous comment relative to anchor stores. Chairman Eisterhold asked for an explanation on market changes and site requirements. Mr. Glickman stated that the site does not warrant a department store anchor. Mr. Glickman explained that there are not many department stores remaining and of them few are expanding. Mr. Glickman added that there is not enough space on the site for a department store and the demographics don't warrant a department store.

Chairman Eisterhold thanked Ms. Armstrong for her assistance and for attending the meeting to discuss.

Motion to Continue the matter to the November 6, 2013 regularly scheduled meeting by Commissioner Goldfein and Seconded by Commissioner Sampen.

Aye: Goldfein, Sampen, Fishman, and Eisterhold

Nay: None

Motion Carried 4-0.

VI. PUBLIC HEARING: Health Club or Private Recreation and Firearms Shooting Ranges – Zoning Code Text Amendment (Continued from July 10, 2013, May 1, 2013, March 6, 2013 and February 6, 2013)

Motion to Continue the matter to the November 6, 2013 regularly scheduled meeting by Commissioner Sampen and Seconded by Commissioner Goldfein.

Aye: Sampen, Goldfein, Fishman, and Eisterhold

Nay: None

Motion Carried 4-0.

VII. PUBLIC HEARING: Accessory Structures – Zoning Code Text Amendment (Continued from July 10, 2013, June, 5, 2013, May 1, 2013, March 6, 2013 & February 6, 2013)

Motion to Continue the matter to the December 4, 2013 regularly scheduled meeting by Commissioner Sampen and Seconded by Commissioner Fishman.

Aye: Sampen, Fishman, Goldfein, and Eisterhold

Nay: None

Motion Carried 4-0.

Commissioner Sampen left the Plan Commission Meeting.

VIII. PUBLIC HEARING: Lincoln Avenue Plan – Amendment to Comprehensive Plan

Chairman Eisterhold stated that the Plan Commission should invite the members of the Lincoln Avenue Task Force who made recommendations in the adopted plan to attend a Plan Commission meeting. Commissioner Goldfein suggested the matter be continued to the October workshop date.

Mr. Cook clarified the staff report relative to the open item of adopting a moratorium on residential development along Lincoln Avenue. Chairman Eisterhold asked if there were any pending residential applications for residential on Lincoln Avenue. Mr. Cook stated that there are no applications in to the Village. Mr. Cook added that moratoriums should not be in place for long periods of time. Commissioner Fishman indicated that he is not in favor of imposing a moratorium. Commissioner Fishman stated a review of the various sites along Lincoln Avenue is appropriate rather than universally prohibiting residential along Lincoln Avenue.

Chairman Eisterhold stated that consensus of the Commission to not impose a moratorium.

Staff recommended the Plan Commission meet with the Task Force members at the earliest opportunity.

Motion to Continue the matter to the October 23, 2013 meeting by Commissioner Fishman and Seconded by Commissioner Goldfein.

Aye: Fishman, Goldfein, and Eisterhold

Nay: None

Motion Carried 3-0.

IX. OTHER BUSINESS:

Public Hearing Procedures will discuss at a future meeting.

X. NEXT MEETING

Staff reminded the Commission of a Workshop meeting scheduled for Tuesday September 24, 2013.

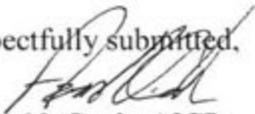
XI. PUBLIC COMMENT

None.

XII. ADJOURNMENT:

Hearing no further business, Motion to adjourn made by Commissioner Fishman and Seconded by Commissioner Goldfein. Motion approved unanimously 3-0. Meeting adjourned at 9:52 PM.

Respectfully submitted,



Aaron N. Cook, AICP
Development Manager



MEMORANDUM

TO: Chairman Eisterhold
Members of the Plan Commission

FROM: Aaron N. Cook, AICP
Development Manager

DATE: August 29, 2014

SUBJECT: **Comprehensive Plan Amendment – Review of Lincoln Avenue Task Force Report** (Continued from August 27, 2014, June 25, 2014, April 23, 2014, April 2, 2014, February 26, 2014, January 22, 2014, December 4, 2013, October 23, 2013, and September 11, 2013)

Below is the memorandum distributed for the August 27th meeting. This matter was continued due to a lack of quorum. Staff anticipates the Village Attorney will have draft text amending the Comprehensive Plan prepared prior to the September 3rd meeting. Staff will distribute this information via email for your review with the goal of discussing the draft language at the meeting on September 3rd.

At the June 25th meeting, the Commission concluded that the Lincoln Avenue Task Force (LATF) Report recommendations relative to residential uses were appropriate. However, the Plan Commission specifically commented that such uses should be considered Special Uses within the Lincoln Avenue corridor rather than permitted uses. Reclassifying residential uses will require the Village Board to direct a public hearing to be held to consider a text amendment to the Zoning Code.

Staff believes the Plan Commission should next continue with their consideration of the LATF Report Second Section of Recommendations, “Development Regulations and Design Standards”. More specifically, the Plan Commission should discuss the other recommendations made as part of the “Modify Land Use Codes to Encourage New Uses”.

Modify Land Use Codes to Encourage New Uses

The first recommendation of the “Development Regulations and Guidelines” is to modify land use codes to encourage new uses. Within this section, the report encourages the Village to significantly change land uses along Lincoln Avenue over time. Major changes to the Zoning Code were identified including encouraging retail uses which cater to Lincolnwood residents and the larger community. Banks and offices were identified as uses that should not be encouraged

as such uses were found to not contribute to cross-shopping, walk-in traffic, or a pedestrian environment. Accordingly, the LATF Report recommends keeping those uses at a minimum or prohibited altogether. This recommendation was implemented as the Zoning Code currently prohibits banks within the B-1 District.

Auto-oriented and auto-related businesses were recommended to be prohibited altogether or kept to a minimum throughout the corridor. Examples of such uses include automobile service and repair shops, oil change centers, gasoline stations, auto part stores and car washes. These uses were found by the LATF Report to not contribute to either a pedestrian or shopping environment.

The Report embraced the concept of mixed use development which at the time was not a term found in the Zoning Code. The former Purple Hotel site was specifically identified as a property envisioned for mixed use development. As the Plan Commission knows, North Capital Group is currently in the process of seeking Village approval for a mixed use development.

Designate Business District Hubs

Staff believes the Plan Commission should next continue with their consideration of the LATF Report and the recommendations made as part of the “Designate Business District Hubs”. The concept of designating three business hubs is to establish development recommendations for the three unique areas.

Lincoln-Touhy Business Hub

“This business hub is envisioned as the downtown for the community.”

Lincoln-Pratt-Crawford Business Hub

“This business hub is near the geographic center of the Village and the crossroads for Village residents...”

Devon-Lincoln Business Hub

“This business hub is an entryway into the community and a major gateway from Chicago.”

These recommendations have been incorporated into the Zoning Code by establishing overlay districts within the B-1 District and establishing the B-3 Village Center District. The Plan Commission at the August 27th meeting should provide comment on the specific recommendation of the LATF relative to designate business district hubs is: *“Modify the Village Zoning Code to designate and regulate uses and encourage development in newly established business district hubs.”*

Conclusion

The recommendations of the LATF Report have been incorporated into the Village’s Zoning Code. Upon completion of the review of the LATF Report, if recommendations are made to change the findings of the Report, it is likely that the text of the Zoning Code will need to be amended to ensure consistency between the Comprehensive Plan and the Zoning Code.

Attachments:

1. Excerpt of Lincoln Avenue Corridor Report and Recommendations of the Lincoln Avenue Task Force

II. Development Regulations and Guidelines

In addition to improving the corridor through the appearance and use of public streets and spaces, the Task Force is recommending major changes to the Village's land use codes; the creation of new "form-based" regulations for new development; and, creation of design guidelines for new buildings. Below are the Task Force recommendations as they relate to private properties and their development in the Corridor.

• Modify Land Use Codes to Encourage New Uses

Land use in the corridor is a critical component to the plan. Except for that portion of Lincoln Avenue north of the Purple Hotel site, the plan recommends significant changes to the Village's current land use policies and zoning code. These changes, over time, will enhance and encourage a pedestrian environment for the corridor. Currently, the corridor is one long, mostly undistinguished strip of business properties. The plan proposes creation of three specific business activity hubs at key intersections, with the intersection of Lincoln and Touhy Avenues identified for higher density and activity, in an effort toward creating a downtown business district for the Village (described further below).

Currently, most of Lincoln Avenue is zoned B1 Restricted Business District. This zone permits office and bank facility developments. Retail is currently allowed only through special use designation. To achieve the vision for Lincoln Avenue, a significant change in land use, over time, is warranted. This plan calls for major changes in the zoning code regarding permitted and special uses. Under this plan, retail uses which cater to both Lincolnwood residents and the larger community should be encouraged to locate in designated business hubs. In such hubs, retail uses should either be exclusive uses on the ground level or predominant uses on the ground level to help achieve a pedestrian and retail oriented nexus. Book and drug stores, food specialty shops, coffee shops, sit-down restaurants, ice cream shops, dry cleaners, furniture, clothing and boutique stores should be encouraged and permitted to operate in such hub districts, subject only to complying with the community's development standards, buffering requirements with residential properties and the Village's off street parking requirements. Because office or bank uses generally do not contribute to cross-shopping, walk-in traffic, or a pedestrian environment, such uses in designated business hubs should be kept to a minimum or prohibited altogether on the ground level.

Similarly, specifically auto-oriented or auto-related businesses should be either prohibited altogether or kept to a minimum throughout the corridor. Automobile service and repair shops, oil change centers, gasoline stations, auto part stores and car washes do not contribute to either a pedestrian or shopping environment and should either be prohibited altogether, or highly limited. Where such uses may be authorized because of significant contributions to the tax base, such as automobile dealerships, careful attention to outdoor vehicle display and its effect on the streetscape is in order. Similarly, future drive-thru operations should be highly regulated, limited or prohibited altogether.

The plan presented for Lincoln Avenue embraces the concept of mixed use development, a term not currently found in the Village's Zoning Code. The plan envisions large development parcels, such as the Purple Hotel site, as including multiple land uses within its development. Individual buildings or smaller development parcels should also have

vertical multiple uses, namely retail on the ground floor and residential or office uses on upper floors.

In addition to permitting housing as part of certain mixed use developments, the plan also calls for the introduction of housing as an infill land use between the designated business hubs. Allowing housing as an in-fill land use will help demarcate the edges of the recommended business hubs and eliminate the current condition of a long, continuous strip of business properties. Moreover, development of in-fill housing between business hubs would visually extend the Village's residential neighborhoods into the corridor, thereby accentuating the residential persona of the community. In such in-fill areas, housing as a ground floor use should be permitted. Where housing is permitted, it is envisioned that residential uses in the corridor would be comprised of either multi-level townhouses or condominium buildings.

Recommendation: Modify the current land use code to allow for mixed use development and in-fill housing; and, to prohibit expansion of drive-thru operations and auto-oriented uses.

• Designate Business District Hubs

Related to the above changes being recommended, the Village should incorporate changes into its current zoning code to allow for development of the following proposed business hubs.

Lincoln-Touhy Business Hub

This business hub, centered on the intersection of Lincoln and Touhy Avenues, would include the Purple Hotel, Bank of Lincolnwood and Lincolnwood Produce sites, as well as nearby adjoining properties. This business hub is envisioned as the downtown for the community. Building height in this area is recommended to be limited to 5 floors, exceeding the 3 floor limitation recommended for the remainder of the corridor. Ground floor retail including restaurants, some with alfresco dining, would be a predominant ground floor use, with upper floors of residential and/or office space. Uses which encourage the public to gather, as well as strong or unique retail anchors, a boutique hotel or family entertainment venues, should be incorporated into specific parcel development plans. In addition, sculptures, fountains and plazas with outdoor seating should be designed into parcel redevelopments in this hub. It is envisioned that this business hub would have the highest development intensity in the corridor and potentially would require either underground parking, parking decks or a combination of both. Any parking structure should be well designed with public safety in mind and located so as to be unobtrusive to the primary use activities. For building sides located along Touhy Avenue, we suggest an exception to the recommended corridor build-to line of 5 feet. For such building sides, we recommend a build-to line of 15 feet or greater from the public right of way to account for high traffic volume on Touhy Avenue and pedestrian safety.

Lincoln-Pratt-Crawford Business Hub

This business hub is near the geographic center of the Village and a crossroads for Village residents. Functionally, this business area is expected to serve the more

immediate area and provide shopping convenience for residents. This area is recommended to include mixed use buildings having ground level retail, including such uses as book stores, bistros, drug stores, coffee shops, restaurants, dry cleaners and the like, as well as small outdoor gathering spaces. Two floors of residential units above the ground floor are also suggested for this area. Off street parking should be located toward the rear of properties.

Devon-Lincoln Business Hub

This business hub is an entryway into the community and a major gateway from Chicago. Uses in this business hub are recommended to be primarily retail uses on the ground level, with off street parking located toward the rear. Any development of 2nd or 3rd floors should be for office uses. Good building design, site planning and quality architecture, important throughout the corridor, is paramount in this gateway area for establishing the ambiance of the corridor. New developments in this hub should include strong anchors to provide identity and a sense of place.

Recommendation: Modify the Village Zoning Code to designate and regulate uses and encourage development in newly established business district hubs.

• Create Form-Based Development Regulations

The form and physical shape of private development is just as important as the type of uses which are permitted. Much of the existing development found in the corridor today is strip center in character, meaning that buildings are set back on the property and off street parking is provided directly in front of the building, toward the street. This form of development results in a scenery of endless parking lots, cars and freestanding signs for the passerby. Such an environment challenges any sense of place, and contributes to an auto dominated rather than pedestrian oriented environment. This plan recommends a very different approach and far different form for new development along the corridor.

Prior to modern conveniences, urban form generally took the shape of buildings built close to the public right-of-way, enclosing the street with buildings along it. Shop windows were large to let in light and also to display their products to the passerby.

Our plan embraces the essence of this earlier development pattern and seeks to update this earlier form for the 21st century. Our plan embraces the relatively new planning principles of Neo Traditional Development and New Urbanism, which seek to balance the needs of auto use with people and which assert that the form of development is as important as the permitted use of the property.

We recommend that, in addition to land use changes, applicable sections of the zoning code be created to contain form-based provisions applicable to new development along Lincoln Avenue. The following form-based provisions are recommended for the corridor (additional recommendations are contained in the addendum to this report).

Build-to Line: instead of a traditional setback line, where buildings are required to be setback from the front property line by a minimum distance, we recommend institution of a “Build-to line” that would require new buildings to be built-to. We suggest that this

build-to line should be no greater than 5 feet from the public right of way line. This recommendation would result in new buildings located very near the public sidewalk. For residential-only buildings this build-to line should be no greater than 10 feet from the right of way line, with a 4 foot wrought iron style fence, located 5 feet from the right of way.

Building Orientation: Because Lincoln Avenue travels through the Village at an angle, the orientation and facades of buildings in the corridor are critical and should be parallel to the street. Only in exceptional situations should the orientation of buildings and facades be at an angle to the Lincoln Avenue right-of-way line.

Building Height: Except for the Lincoln/Touhy Avenue Hub, the maximum height in the corridor should be 3 floors or 38 feet. In the Lincoln/Touhy downtown hub, a maximum height of 5 floors or 65 feet is recommended. Any building which is permitted to exceed 3 stories in height is recommended to have a building façade setback of 10 feet for all floors above 3 stories.

Building Corner Towers: Buildings located at corners, particularly at major intersections, should incorporate a turret or tower feature in harmony with the building's architecture. This tower feature component of the building may exceed the maximum height indicated, so as to give the building a distinctive form and focal point.

Façade Windows: Building facades fronting onto Lincoln Avenue, or onto major adjoining public streets or which otherwise are highly visible from the public right-of-way, should incorporate windows in their facades, particularly at the street level. Walls facing the public right of way which lack windows should be prohibited.

Recommendation: *Create form based development regulations as outlined here and elsewhere in this report.*

• **Create Building Design Guidelines**

Our report and the work of our consultant have suggested certain building design guidelines for new buildings in the corridor. The Task Force believes further work and additional design details are appropriate to help guide developers and architects and to illustrate design elements acceptable to the Village. It is envisioned that this work would result in a separate user friendly, heavily illustrated architectural design guideline handbook. To undertake this work, the Task Force recommends the formation of an Ad Hoc Committee of Village design professionals to help prepare this document.

Recommendation: *Create an Ad Hoc Village Design Committee to develop a Building Design Guideline Handbook.*

• **Regulate Off Street Parking Locations**

For new developments, off street parking should be prohibited from the front of new buildings along Lincoln Avenue. Such parking should be located toward the rear of the property. Related to this, the Village's recently adopted requirements ensuring buffering and screening of business properties with adjoining residential properties should be continued and required in all new developments.

The location of off street parking areas and the recommendation to prohibit them from the front of new buildings along Lincoln Avenue is a critical component of this plan and this new standard should only be varied in the most unusual circumstances. For some large scale developments, parking decks may be in order and these should be placed either behind the buildings they serve or with a level of activity, such as retail, located on the ground floor of the parking structure.

This plan does not recommend relaxation of the Village's off street parking standards at this time. These standards should be reviewed to determine their current adequacy for various uses and to investigate the feasibility of shared parking arrangements among uses.

Where new uses will occupy existing buildings, existing front yard parking areas should be screened from view with landscaping or a short screen wall along the public sidewalk. Recently, the Village adopted new code standards requiring such screening treatments for off street parking lots. Strict adherence to these standards is strongly recommended with variations granted only in the most unusual cases.

For the public right-of-way, this plan recommends the creation of a designated parking lane. Although the Task Force does not recommend that off street parking requirements be specifically relaxed, variations for certain desired proposed uses should be considered depending upon the degree of available nearby on-street parking. In the reuse of existing buildings, since there is a strong interplay between providing parking lot screening and providing off street parking spaces, where on-street parking exists and is generally available in the designated Lincoln Avenue parking lane, required parking lot screening should take precedence over the required number of off street parking stalls.

Recommendation: Prohibit new off-street parking lots in front of buildings along Lincoln Avenue. Continue to require landscape screening of existing parking areas.

• Improve Business Signage

The Village updated its Sign Code in the late 1990's to incorporate new community sign standards. This code encourages (but does not require) the use of monument signs instead of freestanding pole signs. With the greater emphasis on creating a pedestrian scale along Lincoln Avenue, consideration should be given to lowering the maximum height of monument signs, possibly to 8 feet, and either creating stronger incentives to use monument signs or reducing the maximum height of freestanding signs. Another option would be to allow only monument signs and not freestanding signs along Lincoln Avenue.

Recommendation: Explore stricter requirements for signs that would improve the pedestrian scale of the corridor.



MEMORANDUM

TO: Chairman Eisterhold
Members of the Plan Commission

FROM: Aaron N. Cook, AICP
Development Manager

DATE: July 18, 2014

SUBJECT: **Comprehensive Plan Amendment – Review of Lincoln Avenue Task Force Report** (Continued from June 25, 2014, April 23, 2014, April 2, 2014, February 26, 2014, January 22, 2014, December 4, 2013, October 23, 2013, and September 11, 2013)

Note: Below is the summary provided at the June 25, 2014 Plan Commission meeting. At this meeting, the Plan Commission continued this matter without discussion to the July 23, 2014 meeting.

At the April 23rd Plan Commission meeting, staff was given direction to identify the areas of the Lincoln Avenue Task Force (LATF) Report which the Commission should focus on first. Staff believes beginning with the Second Section of Recommendations, “Development Regulations and Design Standards” is appropriate. This section contains the LATF Report recommendation relative to residential development. The LATF Report “Executive Summary” and “Vision for the New Lincoln Avenue Corridor” contain statements encouraging the development of residential along Lincoln Avenue.

As you recall, the Committee of the Whole (COTW) at their July 16, 2013 meeting directed specific review of residential development along Lincoln Avenue. Trustee Cope indicated his concern over residential developments along Lincoln Avenue include excessive building height, increased traffic generation, and decreased property values.

The COTW directed the Plan Commission to hold a Public Hearing to fully review the Lincoln Avenue Task Force Report. However, the referral originated with a discussion to possibly prohibit residential developments along Lincoln Avenue. The Plan Commission should be prepared to address fully the LATF Report recommendations relative to modify land use codes

and encourage new uses; however, given the origins of this matter, staff believes the discussion should begin with deliberation on residential uses within the corridor. Two subsections specifically address residential as an encouraged land use. Below is a brief summary and excerpt of these references.

Modify Land Use Codes to Encourage New Uses

The first recommendation of the “Development Regulations and Guidelines” is to modify land use codes to encourage new uses. Within this section, the report encourages mixed use development with residential uses on upper floors. The LATF Report encourages housing as an infill land use between designated business hubs. Below are excerpts from the LATF Report addressing the recommendation to allow housing:

“Allowing housing as an in-fill land use will help demarcate the edges of the recommended business hubs and eliminate the current condition of a long, continuous strip of business properties.”

“...development of in-fill housing between business hubs would visually extend the Village’s residential neighborhood into the corridor, thereby accentuating the residential persona of the community.”

The recommendation of the LATF relative to modify land use codes to encourage new uses is: *“Modify the current land use code to allow for mixed use development and in-fill housing; and, to prohibit expansion of drive-thru operations and auto-oriented uses.”*

Designate Business District Hubs

The second recommendation of the “Development Regulations and Guidelines” is to designate business district hubs. Within this section, the report encourages the establishment of business hubs in the areas at the intersections of Lincoln-Touhy, Lincoln-Pratt-Crawford, and Devon-Lincoln. The LATF Report encourages residential units above ground floor in the Lincoln-Touhy Business Hub and Lincoln-Pratt-Crawford Business Hub. Below are excerpts from the LATF Report addressing the recommendation to allow housing:

Lincoln-Touhy Business Hub

“Ground floor retail including restaurants, some with alfresco dining, would be a predominant ground floor use, with upper floors of residential and/or office space.”

Lincoln-Pratt-Crawford Business Hub

“Two floors of residential units above the ground floor are also suggested for this area.”

The recommendation of the LATF relative to designate business district hubs is: *“Modify the Village Zoning Code to designate and regulate uses and encourage development in newly established business district hubs.”*

Update on Moratorium: The Plan Commission forwarded to the Village Board a recommendation to not enact a moratorium on residential development along Lincoln Avenue.

At the Village Board meeting of June 17th, the Village Board directed the Village Attorney to prepare an Ordinance enacting a moratorium for a period of 180 days. It is expected that this Ordinance will be prepared and presented to the Village Board at their July 15th regular meeting.

Conclusion

While this referral from the Village Board is to review the entire LATF Report, given the pending moratorium on residential development, the Plan Commission should review and comment on the LATF Report recommendations relative to residential. The recommendations of the LATF Report have been incorporated into the Village's Zoning Code. Upon completion of the review of the LATF Report, if recommendations are made to change the findings of the Report, it is likely that the text of the Zoning Code will need to be amended to ensure consistency between the Comprehensive Plan and the Zoning Code.

Staff has attached to this memorandum, two PowerPoint presentations that were used during the LATF Report process. These will also be available at the June 25, 2014 Plan Commission meeting.

Attachments:

1. Ordinance No. 2006-2710: An Ordinance Amendment the Village of Lincolnwood's Comprehensive Plan to Incorporate the Report and Recommendations of the Lincoln Avenue Task Force
2. PowerPoint Presentation: "Lincoln Avenue Corridor - LATF Report and Recommendations"
3. PowerPoint Presentation: "Lincoln Avenue Corridor Urban Design Plan & Design Guidelines" prepared by Land Vision, Inc.

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VILLAGE OF LINCOLNWOOD

ORDINANCE NO. 2006 2710

**AN ORDINANCE AMENDING THE VILLAGE OF LINCOLNWOOD'S COMPREHENSIVE
PLAN TO INCORPORATE THE REPORT AND RECOMMENDATIONS OF THE LINCOLN
AVENUE TASK FORCE**

ADOPTED BY THE
PRESIDENT AND BOARD OF TRUSTEES
OF THE VILLAGE OF LINCOLNWOOD
THIS 18TH DAY OF MAY, 2006

ORDINANCE NO. 2006-2710

AN ORDINANCE AMENDING THE VILLAGE OF LINCOLNWOOD'S COMPREHENSIVE PLAN TO INCORPORATE THE REPORT AND RECOMMENDATIONS OF THE LINCOLN AVENUE TASK FORCE

WHEREAS, the Village of Lincolnwood (the "Village") is a home rule municipality in accordance with Article VII Section 6(a) of the Constitution of the State of Illinois of 1970;

WHEREAS, the Village has the authority to adopt ordinances and to promulgate rules and regulations that pertain to its government and affairs;

WHEREAS, on November 1, 2001, the Village Board adopted the Village of Lincolnwood Comprehensive Plan (the "Comprehensive Plan");

WHEREAS, the Comprehensive Plan includes, but is not limited to, a policy guide for future Village Board decisions concerning land use within the Village;

WHEREAS, subsequent to the adoption of the Comprehensive Plan, the Village created the Lincoln Avenue Task Force (the "Task Force") to propose a plan for retail redevelopment of properties along the Lincoln Avenue Corridor (the "Corridor") that would be attractive and image enhancing, providing residents with a safe, convenient and pedestrian-friendly environment;

WHEREAS, in November, 2005, the Task Force completed its report and recommendations, with exhibits (the "Report") for enhancement and redevelopment of the Corridor;

WHEREAS, on April 19, 2006, pursuant to a timely, legal notice of public hearing published in the *Lincolnwood Review*, the Task Force presented the Report to the Plan Commission for its review, consideration and recommendation to the Village Board regarding incorporating the Report into the Comprehensive Plan;

WHEREAS, at the April 19, 2006, public hearing all interested persons were allowed the opportunity to present their views regarding the Report, pro and con, to the Plan Commission;

WHEREAS, subsequent to the April 19, 2006, public hearing, the Plan Commission made a recommendation to the Village Board to incorporate the Report into the Comprehensive Plan;

WHEREAS, the Report is attached hereto and incorporated herein by reference as Exhibit A;

WHEREAS, the Village Board reviewed the Report and recommendation of the Plan Commission and determined that it is in the public interest and will promote the public health, safety and welfare of the Village and its residents if the Report is incorporated into the Comprehensive Plan.

NOW, THEREFORE, BE IT ORDAINED by the President and the Trustees of the Village Board as follows:

Section 1. The facts and statements contained in the preamble to this Ordinance are found to be true and correct and are hereby adopted as part of this Ordinance.

Section 2. The Report attached hereto as Exhibit A is hereby incorporated into and made a part of the Comprehensive Plan as Appendix G.

Section 3. In the event there are any inconsistencies between the Comprehensive Plan and the Report, the Report shall control.

Section 4. In all other respects, the Comprehensive Plan shall remain the same.

Section 5. If any section, paragraph or provision of this Ordinance shall be held to be invalid or unenforceable for any reason, the invalidity or unenforceability of such section, paragraph or provision shall not affect any of the remaining provisions of this Ordinance.

Section 6. This Ordinance shall be in full force and effect from and after its passage and approval according to law.

PASSED this 18th day of May, 2006.

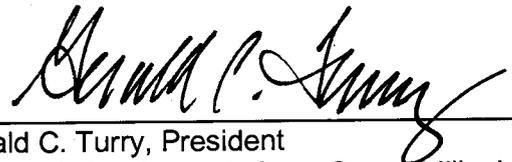
AYES: Trustees Froman, Heidtke, Lebovits, Elster, Sprogis-Marohn and Leftakes

NAYS: None

ABSENT: None

ABSTENTION: None

APPROVED by me this 18th day of May, 2006.



Gerald C. Turry, President
Village of Lincolnwood, Cook County, Illinois

ATTESTED and FILED in my office the
18th day of May, 2006.


Beryl Herman, Village Clerk
Village of Lincolnwood, Cook County, Illinois

EXHIBIT A

Copy of the Lincoln Avenue Corridor Report and Recommendations
of the Lincoln Avenue Task Force

(See Attached)

Lincoln Avenue Corridor



Report and Recommendations of the Lincoln Avenue Task Force

**Village of Lincolnwood
November 2005**

Lincoln Avenue Task Force Members

James V. Persino, Chairman

Member and Chair, Lincolnwood Economic Development Commission

Mary Couzin

Member, Lincolnwood Plan Commission and past Village Trustee

Maureen Ehrenberg

Member, Lincolnwood Economic Development Commission

Lawrence Elster

Village Trustee

Kenneth Klint

Member, Lincolnwood Plan Commission

Renee Silberman

Resident at -large

Renee Sprogis-Marohn

Village Trustee

Staff Assistance

Timothy M. Clarke, AICP
Community Development Director

Consultation Provided

Walter J. Magdziarz, AICP
Mahender Vasandani
Land Vision, Inc.

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Executive Summary

We present to you a bold and dynamic plan for Lincoln Avenue. When fully implemented, we believe this plan will result in an attractive corridor and desirable main street for our community. This report is intended to set out the necessary public actions and to guide private reinvestment and redevelopment in order to achieve the dynamic vision we have for the Corridor.

In formulating our recommendations, the Lincoln Avenue Task Force studied the redevelopment and revitalization efforts of a number of communities, including Park Ridge, Deerfield, Mount Prospect and Glenview. While our recommendations embody a bold and dynamic vision for the future of Lincoln Avenue, one which incorporates cutting-edge planning principles, these measures we recommend have been tested and proven successful elsewhere.

The central focus guiding our work has been to foster redevelopment of properties along the corridor which are both attractive and image enhancing, creating retail districts which provide residents with a safe, convenient, attractive and pedestrian friendly environment.

First, our plan calls for a dramatically different land use pattern for the corridor and we recommend significant land use changes to Village code. Specifically, we recommend creating three business districts along the Lincoln Avenue Corridor: one at Devon Avenue, another at Pratt and Crawford Avenues, and a third at Touhy Avenue. Pedestrian oriented retail is called for as the primary ground-floor use in these areas. We envision businesses such as bookstores, galleries, bistros, restaurants, coffee and ice cream shops, boutiques and other specialty retail stores enlivening these business nodes. Our plan also calls for mixed-use development in these business nodes, with upper floors of residential or office uses, to foster redevelopment and to encourage a lively, pedestrian-friendly atmosphere.

At Touhy Avenue, our plan envisions creation of a downtown center for our community, having ground floor retail, community gathering spaces and such anchor uses as a boutique hotel, sit-down restaurants, or entertainment venues. On upper floors, as well as between the three business nodes, our plan calls for introduction of residential development in the corridor.

Second, our plan calls for enhancements to the public right-of-way to improve public safety, create ambiance, and foster a strong sense of place. We recommend designating a parking lane on Lincoln Avenue; improving pedestrian crosswalks; and continuing efforts to restore parkways and to complete the median landscaping project. Most important, we call for a specific streetscape plan to be developed, which would include new streetlights and identifying street furniture and other urban design amenities and elements for the corridor. A bold but longer range component of our plan calls for creating new open spaces and focal points, such as towers or monuments in the corridor at key locations.

Third, we believe that the shape and form of new development in the corridor is critical. Therefore, our plan embraces neo-traditional planning and new urbanism concepts for new development along Lincoln Avenue. In this regard, we call for a set of new development regulations which will regulate the shape and the form of new buildings and private development in the corridor. These “form-based” recommendations for instance call for new buildings along Lincoln Avenue to be parallel to and positioned close to the street, with new off street parking located toward the rear. We recommend additional form based regulations in our report and to assist developers in meeting our vision, we also call for the creation of an illustrated architectural design handbook to guide new building development in the corridor.

Our vision for Lincoln Avenue is exciting and our work has set out many recommendations, both broad and specific. Execution will require a number of actions in order for our vision to become a reality. With Village government dedicated to the vision we present, we believe the Lincoln Avenue Corridor can truly become a main street for our community.

Report and Recommendations for a New Lincoln Avenue Corridor

Introduction

Background of Task Force

The Lincoln Avenue Task Force is an outgrowth of discussions first held in 2002 by the Village's Economic Development Commission. These discussions quickly expanded to include the Village's Plan Commission and a joint meeting of these bodies was held in the fall of 2002 to discuss Lincoln Avenue and its future. The result was a recommendation that a task force be established to consider the challenges, assets and possibilities for the Lincoln Avenue Corridor. President Peter Moy, with consent of the Village Board, subsequently established the Lincoln Avenue Task Force to undertake this work.

The seven-member appointed Task Force is comprised of two members from each of the following bodies: the Village Board, the Plan Commission, and the Economic Development Commission, with one citizen-at-large member completing the roster. Primary staff assistance to the Task Force has been provided by the Community Development Director, with technical and field support provided by key members of the Building Department. The work of the Task Force has been further assisted with an engagement of Land Vision Inc., an urban planning and design firm.

In performing its charge, the Task Force examined many elements. These ranged from conducting a windshield survey of current conditions along Lincoln Avenue; studying the redevelopment efforts of other communities such as Glenview, Deerfield, Park Ridge and Mount Prospect; seeking community input through an Image Preference Survey and conducting and soliciting additional public input in a Town Meeting setting.

The work of the Task Force has resulted in a bold and dynamic plan which will transform the Lincoln Avenue Corridor into a valuable asset for the community.

History of Corridor

Little is written about the origins of Lincoln Avenue. It is known that Lincoln Avenue began as one of the early roads linking Chicago northward to the rural farm areas of Illinois and beyond. Some of the very first businesses in the Village opened along this road, some reportedly to be out of the reach of city restrictions. As Chicago's population expanded outward and traffic increased, so did use of Lincoln Avenue and over time it became a major artery into and out of the City.

Although records are limited, most initial development along Lincoln Avenue in the Village appears to have occurred between the end of World War II and the mid 1960's. With the opening of the Edens Expressway in the 1950's however, Lincoln Avenue's role as a major thoroughfare declined. Just south of the Village, a string of 1950's-1960's era motels still can be found, testament to Lincoln Avenue's history as a major artery before the advent of the Edens.

Existing Development Pattern

The existing development pattern along Lincoln Avenue can be characterized as an auto dependent form of commercial strip. Much of the initial development of this strip occurred at a time when the automobile was beginning to exert a dominant force on land use in America. Unlike areas of Chicago, Evanston, or rail towns like Park Ridge, Lincolnwood and Lincoln Avenue developed without street cars, rail stations, mass transit or the influence of great numbers of pedestrians. With the lack of any significant mass transit, specific business hubs, or a semblance of a central gathering location, the corridor became a long, nearly continuous stretch of commercial development, dependent mainly on access by automobile.

This development pattern occurred prior to the now common phenomenon of multi-car families. Traffic and parking demands in the corridor at the time of its initial development were much less than they are today. Developments during this period often had only a limited number of off street parking spaces, generally arranged along the front of the building and sometimes evolving into a continuous curb cut onto Lincoln Avenue.

Although the Lincoln Avenue Corridor is nearly a continuous strip of businesses from Devon Avenue to Jarvis Avenue, the character of the street changes significantly, albeit gradually, from south to north. At its south end, near the border with Chicago, land uses along Lincoln Avenue tend to be more varied, including auto repair shops, older strip retail centers with continuous curb cuts, and a storage facility. Developments along this stretch of Lincoln Avenue tend to be older, more functionally obsolete in nature and contain more marginal and less intensive uses than areas north. The middle section of Lincoln Avenue, roughly from the rail crossing near Hamlin Avenue to Touhy Avenue, contains a mix of uses, including more current and intense development. This area has multi-story office and bank buildings, automobile dealerships, restaurants, renovated strip centers and community institutional uses such as Village Hall, the Library and Proesel Park. North of Touhy Avenue, to the Village's boundary with Skokie, Lincoln Avenue is primarily comprised of office buildings.

The Task Force found that, except for the office corridor north of Touhy Avenue, the existing development pattern on Lincoln Avenue was aging, many uses were marginal, and the corridor was not functioning as a vibrant main street of the community.

Challenges and Assets of Corridor

Lincoln Avenue presents a number of challenges and assets to its revitalization. First, Lincoln Avenue runs through the Village on a diagonal to the overall grid system of

streets. This results in a number of odd or irregularly shaped parcels at various corners which can inhibit typical development. Moreover, many of the parcels fronting along Lincoln have rather short depths, limiting to a degree their usefulness and redevelopment potential. This is compounded by the close proximity of single family homes to the corridor, some of which utilize shared alleyways with Lincoln Avenue properties and others which have no alley whatsoever.

Much of the existing development along the street was developed without adequate barriers or buffering measures with residential areas. This lack of buffering combined with short parcel depths and irregular parcel configurations can and often has produced land use conflicts. Recently however, the Village has strengthened its zoning code to require landscape buffering of business properties from residential areas. Where space is too constrained for landscape buffers, barrier walls are now required for new development. The implementation of these new standards for buffering should be continued and should help reduce land use conflicts within the corridor.

Lincoln Avenue itself is a relatively wide right-of-way corridor, averaging between 90 – 100 feet in width throughout its length in the Village. Existing conditions in the right-of-way, coupled with its physical width, currently inhibit safe pedestrian crossings and help aid in the dominance of the auto in the corridor. The conditions and width of this right-of-way, currently an impediment to a pedestrian friendly environment, can however be smartly designed to create a unique sense of place and a friendly pedestrian environment. With the creation of a designated parking lane for instance and a thoughtful streetscape design, this wide right-of-way can be utilized and tapped as an asset in the redevelopment of the corridor.

Another untapped asset of the corridor is the immediate market. Lincolnwood is an affluent community and with Lincoln Avenue running through its center, an attractive local market exists. Nonetheless, many marginal uses currently exist in the corridor. With changes to the Village's zoning code to encourage, for instance, restaurants and higher end retailers to locate in the corridor, this local market can be tapped to both serve the community and assist in the corridor's redevelopment.

A further asset which the Village can play upon is the recent and ongoing redevelopment and revitalization efforts occurring immediately south of the Village, along the corridor in the City of Chicago. Recently, a number of existing developments, including the Lincoln Village Shopping Center as well as the conversion of a Kmart store into a Home Depot, have resulted in significant revitalization. Moreover, the City of Chicago has recently acquired a few of the older motel properties, converting these to parks or other uses. And south of Peterson Avenue, new housing in the corridor has recently been constructed.

Vision for the New **Lincoln Avenue Corridor**

We envision the Lincoln Avenue corridor as a vibrant Main Street through our Village. We see it not only as the centerpiece and spine of the community; it is also the entryway into the many residential neighborhoods of the Village. Major Village institutions, such as Village government, the public library, community center and local schools, are located along the street or very nearby. Lincoln Avenue is central to life in the Village.

We envision that as Lincoln Avenue enters the Village from Chicago, the character of the street changes dramatically, being readily apparent to the passerby that they have left behind the big city and have entered a high quality suburb. A landscaped median, new street lights, attractive signs, public sculpture and pedestrian friendly features including easy to use crosswalks and a marked parking lane, are evident and add to the sense that Lincoln Avenue is the Main Street of an upscale community.

Instead of a long continuous commercial strip as presently exists, hubs of activity have been created along Lincoln Avenue, primarily located at major intersections. These activity hubs include ground level retail in high quality, architecturally significant buildings with adequate customer parking, tasteful business signs and attractive landscaping. Comprised of restaurants, ice cream and coffee shops and boutique retail stores, these areas serve the immediate neighboring residential areas and the community as a whole and may include second or third story residential units.

Between these hubs of retail dotting Lincoln Avenue, new housing has been added to already existing bank buildings and office complexes, to replace tired strip centers, auto garages, storage facilities or other obsolete buildings and uses. We envision this in-fill housing as no higher than three stories, with appropriate green space and comprised of attractive townhouse, 3-flat, or small condominium buildings and developments. We see this new low density in-fill housing as providing on-the-one-hand, an attractive entryway into the adjoining single family residential areas, and on-the-other hand, a buffering and transitional use from the business areas and Lincoln Avenue itself to the nearby single family neighborhoods.

While Lincoln Avenue is still a major thoroughfare through the community, through much work, it has been transformed to be more pedestrian friendly and less auto dominated. Instead of a rapid auto path akin to an expressway and a barrier separating Lincolnwood's neighborhoods, the new Lincoln Avenue links neighborhoods together, bringing the Village together at its major institutions and becoming the main street of our community.

Recommendations

The Task Force's recommendations below are divided into two major sections. The first section, Public Streets and Spaces, recommends actions and improvements in the public right-of-way which can help to create a pedestrian-friendly environment and foster a real sense of place. In this section, improved green space, wider sidewalks, new street lights, various urban design elements, as well as slower traffic speeds and eye-catching focal points are recommended to enhance the streetscape and invite residents and shoppers to the corridor. These are improvements which can be undertaken in the public right of way, by the Village in concert with the Illinois Department of Transportation (IDOT).

The second major section of our recommendations, Development Regulations and Guidelines, relates to controls on private property and new development in the corridor. The appearance, location and mix of businesses and residences along the corridor shape perceptions and use. To achieve our vision, this section recommends changes in the Village's existing land use regulations, creation of new form-based regulations and creation of design guidelines which will not only encourage private reinvestment, but will aid in shaping it. Together and in tandem, we believe these set of recommendations will help the Village create a new sense of place and achieve the vision we have for the Corridor.

I. Public Streets and Spaces

Creating an attractive, vibrant and pedestrian-oriented environment in the public right-of-way is an essential element of the Lincoln Avenue Corridor Plan. This plan reinforces many of the Village's ongoing and recently begun initiatives, such as median landscaping and parkway restoration. It also recommends a number of new initiatives, such as creating a designated parking lane and the creation of a coordinated streetscape, including installation of new street lights and street furniture. Long term, the plan calls for creating three public plazas and/or visual focal points along the corridor.

While the plan emphasizes pedestrian orientation and creating pedestrian spaces, it recognizes that Lincoln Avenue is and will remain a major and important thoroughfare in the community. By undertaking attractive pedestrian enhancements, along with the other recommendations of this plan, Lincoln Avenue can become the main street for the Village. Since it is a state controlled street, coordination with the Illinois Department of Transportation (IDOT) is essential and will be required to undertake and implement these recommendations to the right-of-way.

The following are specific recommendations regarding public streets and spaces.

- **Complete Median Landscaping**

In the late 1990's, the Village initiated landscape improvements for the medians of Lincoln Avenue. This multi-year project is continuing. Median landscaping enhances the attractiveness of the corridor and creates positive perceptions of the corridor and community.

Recommendation: Continue and complete median landscaping throughout the corridor.

- **Continue Parkway Restoration**

In 2002 the Village created standards for parkways which include the placement of trees every 25 feet in the parkway and a pervious surface of sod or vegetative ground cover. Until these standards were instituted, parkways in the Village's business areas, including Lincoln Avenue, were essentially allowed to be treated by the adjoining owner as they so desired, providing no cohesion in the corridor and producing a visually cluttered, haphazard, disjointed and unkempt appearance. Moreover, in some sections, parkway areas have been essentially confiscated and turned into long continuous driveway curb cuts that directly access off-street private parking spaces, creating large stretches of impervious surfaces devoid of street trees. This situation conflicts with pedestrian use of public sidewalks and severely affects pedestrian safety. The restoration of parkways and public sidewalks, consistent with the recently established parkway standards, is an essential element toward improving the corridor. Parkway restoration should be diligently pursued, undertaken and implemented whenever possible.

Recommendation: Diligently pursue parkway restoration whenever possible along Lincoln Avenue, including tree planting and sidewalk restoration.

- **Improve Pedestrian Crosswalks**

Currently, many of the marked crosswalks across Lincoln Avenue do not provide the pedestrian with the shortest possible crossing distance for safety. Pedestrians are in the path of traffic for longer periods of time than necessary. To enhance the pedestrian environment and safety, conscious efforts should be undertaken to create highly visible marked crosswalks utilizing the shortest distance possible. The creation of intersection bump outs (described below) will further aid in reducing crosswalk distances, improving public safety and encouraging pedestrian use.

Recommendation: Enhance crosswalk markings and designations, including signage.

- **Create a Designated Parking Lane**

Currently, parking is generally permitted along the entire length of the corridor, although it is not well marked as such. Only in a few concentrated areas, such as in the vicinity of Air Room, is street parking now commonplace. Creation of designated parking lanes on both sides of Lincoln Avenue would help slow traffic and support active retail uses.

The width of the Lincoln Avenue street corridor currently allows for creation of this parking lane. Initially, such a parking lane could be achieved relatively inexpensively through painted striping and signage. A more long term, lasting and beneficial

improvement is to create curbed “bump outs” at intersections. Such bump-out areas essentially bring the sidewalk/pedestrian area of the public right-of-way out into the parking lane, approximately 8-9 feet toward the traffic lanes. Bump outs increase public safety and enhance the pedestrian environment. Specifically, bump outs at intersections: 1) protect and better define the beginning and end of the parking lane between intersections; and, 2) physically shorten the distance for pedestrians crossing Lincoln Avenue. In some areas, additional mid-block bump outs might be desirable to visually break up long parking lane stretches. Moreover, certain urban design features could be added to these new pedestrian spaces. Items such as decorative brick pavers, lighting, benches, bicycle racks, additional landscaping, newspaper vending machines, or even public art could be incorporated into these spaces.

Recommendation: Create a designated, marked parking lane with pedestrian bump outs.

• Reduce Traffic Speed

High speed traffic is not conducive to either a pedestrian environment, or to residential or retail uses. Physical changes such as instituting a parking lane with bump outs, in and of itself, will likely reduce speed to some extent, both because driving lane widths will be perceived to be narrower and because of parking movement maneuvers slowing the speed of traffic.

Although no formal traffic study was commissioned as part of this work, traffic observations in the corridor suggest that much of existing Lincoln Avenue traffic, particularly during rush hour, is essentially “cut-through” traffic generated by Chicago residents seeking a quicker way to the Edens Expressway. This is suggested by two left turn lanes on Lincoln for westbound Touhy Avenue and two left turn lanes on Lincoln in Chicago for eastbound Peterson Avenue. Cut through traffic, particularly related to rush hour periods and home-to-work trips, typically consists of drivers intent on minimizing time. Slowing down Lincoln Avenue traffic in Lincolnwood would result in lengthened drive times for such drivers, deterring some traffic and making alternative routes more attractive.

By slowing down traffic, pedestrian safety is enhanced and greater visual observation by motorists is achieved. Businesses in the corridor will be more visible to the motorist and businesses will be better able to capitalize on impulse shopping and the customer base produced by the traffic volume. Therefore, slower traffic on Lincoln Avenue can help to contribute to the Village’s sales tax revenue.

The current posted traffic speed on Lincoln Avenue is 35 mph, although traffic appears to typically travel at speeds well in excess of the posted limit. Touhy Avenue west of Lincoln is posted at 30 mph, and a reduction in the posted speed limit on Lincoln Avenue to 30 mph should be seriously considered.

Recommendation: Take action to reduce the speed of traffic in the corridor, including physical street design changes and a reduction in the posted speed limit.

- **Reduce and Minimize Parkway Curb Cuts**

Throughout the corridor, particularly as new developments are reviewed, driveway curb cuts in the Parkway should be kept at a minimum and apron width limited to only that needed for vehicle ingress and egress. Where possible and particularly for residential development called for in this plan, driveway curb cuts should be located not on Lincoln Avenue itself, but on adjoining side streets. This would further improve pedestrian safety as well as the aesthetic appeal of the streetscape.

Recommendation: Reduce and minimize Parkway curb cuts along Lincoln and relocate as needed.

- **Create a Unified Corridor Streetscape Plan**

A specific streetscape plan should be created and implemented, specifying and delineating urban design elements that create an attractive, pedestrian scale and sense of place within the corridor. As envisioned, this streetscape plan would specify the style and identify the location of urban design and pedestrian friendly elements such as lighting, benches, bicycle racks, trash bins, and kiosks. This work could explore such other items as special signage, public art and sculptures, hanging flower containers and street banners. Use of brick pavers in sidewalks and crosswalks and specific designs for the recommended parking lane bump outs are other suggested elements that could be explored in development of this streetscape plan.

One critical component of the streetscape plan would be new streetlights for the entire corridor, to replace existing streetlights which are rapidly aging and appear to have exceeded their useful life. Our review suggests that new streetlights should not be installed in the medians as they are now, but along the sides of the street. However, this could be further investigated as the specific streetscape plan is developed. Creation of a specific streetscape plan would not only result in the identification of specific styles and designs for street furniture, sidewalks and light standards, but also would locate them in the corridor. The advantage to developing such a plan is twofold: it will help guide specific improvements in the public right-of-way at specific locations and it would allow the Village to seek Illinois Transportation Enhancement Program (ITEP) grants to undertake such improvements. We believe this is a critical component which should be budgeted within the next two fiscal years.

Recommendation: Create and implement a unified streetscape plan for the corridor.

- **Consider Vacating Portions of Intersecting Streets**

Lincoln Avenue cuts diagonally through Lincolnwood, resulting in many unusual lot configurations along the corridor. The angle of Lincoln Avenue, set against the Village's basic grid pattern of streets, also has produced a number of intersections in the corridor which vary from the typical 90 degree street intersection angle, some involving more than one intersecting street. Because such intersections are not standard or typical, they pose public safety concerns, particularly to those not familiar with the area. This situation also results in long stretches of pavement and active traffic for the pedestrian to confront.

Our plan suggests the vacation of portions of some of these intersecting side streets to eliminate such unusual intersections. Such street vacations also have the potential benefits of reducing traffic into adjoining residential neighborhoods as well as enlarging and improving the shape of development parcels in the corridor. Any vacation of a specific street, however, must be thoroughly examined as to its potential effects. This plan has not undertaken such an investigation and therefore is only suggestive in this regard.

Recommendation: Side street vacations should be explored and examined at appropriate locations.

• Create Public Space Focal Points

A more long term but bold component of this plan, is to consider creating new public space focal points at three locations in the corridor: 1) Lincoln/Touhy/Kostner; 2) Pratt/Lincoln/Crawford; and 3) Lincoln Avenue at Proesel Park.

The first two locations are triangular shaped sites surrounded on all sides by public streets. Converting these sites into public spaces would result in open vistas at these important intersections, producing natural focal points. Along with redevelopment of adjoining properties, these new public spaces would help create a unique sense of place. Based largely on results of an Image Preference Survey of residents, our plan suggests that in these spaces, tower-like monuments be constructed of similar design, potentially utilizing prairie style elements, to help bring continuity to the corridor. This is however only an initial suggestion. As the community moves forward with implementing this element of the plan, other focal point ideas could emerge or be incorporated into such designs. These spaces for instance could commemorate significant local or national events or persons, celebrate the multicultural diversity of the community, or become spaces for the remembrance of the holocaust or of war dead.

A third recommended focal point along the Lincoln Avenue corridor is at Proesel Park. Currently the park, a major asset of the community extends a relatively long distance along Lincoln Avenue. Our observation is that the current design of the park provides neither a welcoming presence nor much activity along the Lincoln Avenue corridor. Our plan suggests highlighting this community asset by creating a focal point on Lincoln Avenue at Proesel Park, similar to those proposed on the triangle parcels. Specifically, this suggestion is to widen and expand the current Lincoln Avenue median at this location and install a similar tower-like icon, visually unifying the three sites. Creating such an expanded median area would require moving and arcing the Lincoln Avenue traffic lanes somewhat, producing an oval shape. This would be another feature which would slow Lincoln Avenue traffic. This proposal obviously would require IDOT review and approval and likely would require some land acquisition.

Recommendation: Consider creating three public focal points at key corridor locations.

II. Development Regulations and Guidelines

In addition to improving the corridor through the appearance and use of public streets and spaces, the Task Force is recommending major changes to the Village's land use codes; the creation of new "form-based" regulations for new development; and, creation of design guidelines for new buildings. Below are the Task Force recommendations as they relate to private properties and their development in the Corridor.

• Modify Land Use Codes to Encourage New Uses

Land use in the corridor is a critical component to the plan. Except for that portion of Lincoln Avenue north of the Purple Hotel site, the plan recommends significant changes to the Village's current land use policies and zoning code. These changes, over time, will enhance and encourage a pedestrian environment for the corridor. Currently, the corridor is one long, mostly undistinguished strip of business properties. The plan proposes creation of three specific business activity hubs at key intersections, with the intersection of Lincoln and Touhy Avenues identified for higher density and activity, in an effort toward creating a downtown business district for the Village (described further below).

Currently, most of Lincoln Avenue is zoned B1 Restricted Business District. This zone permits office and bank facility developments. Retail is currently allowed only through special use designation. To achieve the vision for Lincoln Avenue, a significant change in land use, over time, is warranted. This plan calls for major changes in the zoning code regarding permitted and special uses. Under this plan, retail uses which cater to both Lincolnwood residents and the larger community should be encouraged to locate in designated business hubs. In such hubs, retail uses should either be exclusive uses on the ground level or predominant uses on the ground level to help achieve a pedestrian and retail oriented nexus. Book and drug stores, food specialty shops, coffee shops, sit-down restaurants, ice cream shops, dry cleaners, furniture, clothing and boutique stores should be encouraged and permitted to operate in such hub districts, subject only to complying with the community's development standards, buffering requirements with residential properties and the Village's off street parking requirements. Because office or bank uses generally do not contribute to cross-shopping, walk-in traffic, or a pedestrian environment, such uses in designated business hubs should be kept to a minimum or prohibited altogether on the ground level.

Similarly, specifically auto-oriented or auto-related businesses should be either prohibited altogether or kept to a minimum throughout the corridor. Automobile service and repair shops, oil change centers, gasoline stations, auto part stores and car washes do not contribute to either a pedestrian or shopping environment and should either be prohibited altogether, or highly limited. Where such uses may be authorized because of significant contributions to the tax base, such as automobile dealerships, careful attention to outdoor vehicle display and its effect on the streetscape is in order. Similarly, future drive-thru operations should be highly regulated, limited or prohibited altogether.

The plan presented for Lincoln Avenue embraces the concept of mixed use development, a term not currently found in the Village's Zoning Code. The plan envisions large development parcels, such as the Purple Hotel site, as including multiple land uses within its development. Individual buildings or smaller development parcels should also have

vertical multiple uses, namely retail on the ground floor and residential or office uses on upper floors.

In addition to permitting housing as part of certain mixed use developments, the plan also calls for the introduction of housing as an infill land use between the designated business hubs. Allowing housing as an in-fill land use will help demarcate the edges of the recommended business hubs and eliminate the current condition of a long, continuous strip of business properties. Moreover, development of in-fill housing between business hubs would visually extend the Village's residential neighborhoods into the corridor, thereby accentuating the residential persona of the community. In such in-fill areas, housing as a ground floor use should be permitted. Where housing is permitted, it is envisioned that residential uses in the corridor would be comprised of either multi-level townhouses or condominium buildings.

Recommendation: Modify the current land use code to allow for mixed use development and in-fill housing; and, to prohibit expansion of drive-thru operations and auto-oriented uses.

• Designate Business District Hubs

Related to the above changes being recommended, the Village should incorporate changes into its current zoning code to allow for development of the following proposed business hubs.

Lincoln-Touhy Business Hub

This business hub, centered on the intersection of Lincoln and Touhy Avenues, would include the Purple Hotel, Bank of Lincolnwood and Lincolnwood Produce sites, as well as nearby adjoining properties. This business hub is envisioned as the downtown for the community. Building height in this area is recommended to be limited to 5 floors, exceeding the 3 floor limitation recommended for the remainder of the corridor. Ground floor retail including restaurants, some with alfresco dining, would be a predominant ground floor use, with upper floors of residential and/or office space. Uses which encourage the public to gather, as well as strong or unique retail anchors, a boutique hotel or family entertainment venues, should be incorporated into specific parcel development plans. In addition, sculptures, fountains and plazas with outdoor seating should be designed into parcel redevelopments in this hub. It is envisioned that this business hub would have the highest development intensity in the corridor and potentially would require either underground parking, parking decks or a combination of both. Any parking structure should be well designed with public safety in mind and located so as to be unobtrusive to the primary use activities. For building sides located along Touhy Avenue, we suggest an exception to the recommended corridor build-to line of 5 feet. For such building sides, we recommend a build-to line of 15 feet or greater from the public right of way to account for high traffic volume on Touhy Avenue and pedestrian safety.

Lincoln-Pratt-Crawford Business Hub

This business hub is near the geographic center of the Village and a crossroads for Village residents. Functionally, this business area is expected to serve the more

immediate area and provide shopping convenience for residents. This area is recommended to include mixed use buildings having ground level retail, including such uses as book stores, bistros, drug stores, coffee shops, restaurants, dry cleaners and the like, as well as small outdoor gathering spaces. Two floors of residential units above the ground floor are also suggested for this area. Off street parking should be located toward the rear of properties.

Devon-Lincoln Business Hub

This business hub is an entryway into the community and a major gateway from Chicago. Uses in this business hub are recommended to be primarily retail uses on the ground level, with off street parking located toward the rear. Any development of 2nd or 3rd floors should be for office uses. Good building design, site planning and quality architecture, important throughout the corridor, is paramount in this gateway area for establishing the ambiance of the corridor. New developments in this hub should include strong anchors to provide identity and a sense of place.

Recommendation: Modify the Village Zoning Code to designate and regulate uses and encourage development in newly established business district hubs.

• Create Form-Based Development Regulations

The form and physical shape of private development is just as important as the type of uses which are permitted. Much of the existing development found in the corridor today is strip center in character, meaning that buildings are set back on the property and off street parking is provided directly in front of the building, toward the street. This form of development results in a scenery of endless parking lots, cars and freestanding signs for the passerby. Such an environment challenges any sense of place, and contributes to an auto dominated rather than pedestrian oriented environment. This plan recommends a very different approach and far different form for new development along the corridor.

Prior to modern conveniences, urban form generally took the shape of buildings built close to the public right-of-way, enclosing the street with buildings along it. Shop windows were large to let in light and also to display their products to the passerby.

Our plan embraces the essence of this earlier development pattern and seeks to update this earlier form for the 21st century. Our plan embraces the relatively new planning principles of Neo Traditional Development and New Urbanism, which seek to balance the needs of auto use with people and which assert that the form of development is as important as the permitted use of the property.

We recommend that, in addition to land use changes, applicable sections of the zoning code be created to contain form-based provisions applicable to new development along Lincoln Avenue. The following form-based provisions are recommended for the corridor (additional recommendations are contained in the addendum to this report).

Build-to Line: instead of a traditional setback line, where buildings are required to be setback from the front property line by a minimum distance, we recommend institution of a “Build-to line” that would require new buildings to be built-to. We suggest that this

build-to line should be no greater than 5 feet from the public right of way line. This recommendation would result in new buildings located very near the public sidewalk. For residential-only buildings this build-to line should be no greater than 10 feet from the right of way line, with a 4 foot wrought iron style fence, located 5 feet from the right of way.

Building Orientation: Because Lincoln Avenue travels through the Village at an angle, the orientation and facades of buildings in the corridor are critical and should be parallel to the street. Only in exceptional situations should the orientation of buildings and facades be at an angle to the Lincoln Avenue right-of-way line.

Building Height: Except for the Lincoln/Touhy Avenue Hub, the maximum height in the corridor should be 3 floors or 38 feet. In the Lincoln/Touhy downtown hub, a maximum height of 5 floors or 65 feet is recommended. Any building which is permitted to exceed 3 stories in height is recommended to have a building façade setback of 10 feet for all floors above 3 stories.

Building Corner Towers: Buildings located at corners, particularly at major intersections, should incorporate a turret or tower feature in harmony with the building's architecture. This tower feature component of the building may exceed the maximum height indicated, so as to give the building a distinctive form and focal point.

Façade Windows: Building facades fronting onto Lincoln Avenue, or onto major adjoining public streets or which otherwise are highly visible from the public right-of-way, should incorporate windows in their facades, particularly at the street level. Walls facing the public right of way which lack windows should be prohibited.

Recommendation: *Create form based development regulations as outlined here and elsewhere in this report.*

• **Create Building Design Guidelines**

Our report and the work of our consultant have suggested certain building design guidelines for new buildings in the corridor. The Task Force believes further work and additional design details are appropriate to help guide developers and architects and to illustrate design elements acceptable to the Village. It is envisioned that this work would result in a separate user friendly, heavily illustrated architectural design guideline handbook. To undertake this work, the Task Force recommends the formation of an Ad Hoc Committee of Village design professionals to help prepare this document.

Recommendation: *Create an Ad Hoc Village Design Committee to develop a Building Design Guideline Handbook.*

• **Regulate Off Street Parking Locations**

For new developments, off street parking should be prohibited from the front of new buildings along Lincoln Avenue. Such parking should be located toward the rear of the property. Related to this, the Village's recently adopted requirements ensuring buffering and screening of business properties with adjoining residential properties should be continued and required in all new developments.

The location of off street parking areas and the recommendation to prohibit them from the front of new buildings along Lincoln Avenue is a critical component of this plan and this new standard should only be varied in the most unusual circumstances. For some large scale developments, parking decks may be in order and these should be placed either behind the buildings they serve or with a level of activity, such as retail, located on the ground floor of the parking structure.

This plan does not recommend relaxation of the Village's off street parking standards at this time. These standards should be reviewed to determine their current adequacy for various uses and to investigate the feasibility of shared parking arrangements among uses.

Where new uses will occupy existing buildings, existing front yard parking areas should be screened from view with landscaping or a short screen wall along the public sidewalk. Recently, the Village adopted new code standards requiring such screening treatments for off street parking lots. Strict adherence to these standards is strongly recommended with variations granted only in the most unusual cases.

For the public right-of-way, this plan recommends the creation of a designated parking lane. Although the Task Force does not recommend that off street parking requirements be specifically relaxed, variations for certain desired proposed uses should be considered depending upon the degree of available nearby on-street parking. In the reuse of existing buildings, since there is a strong interplay between providing parking lot screening and providing off street parking spaces, where on-street parking exists and is generally available in the designated Lincoln Avenue parking lane, required parking lot screening should take precedence over the required number of off street parking stalls.

Recommendation: Prohibit new off-street parking lots in front of buildings along Lincoln Avenue. Continue to require landscape screening of existing parking areas.

• Improve Business Signage

The Village updated its Sign Code in the late 1990's to incorporate new community sign standards. This code encourages (but does not require) the use of monument signs instead of freestanding pole signs. With the greater emphasis on creating a pedestrian scale along Lincoln Avenue, consideration should be given to lowering the maximum height of monument signs, possibly to 8 feet, and either creating stronger incentives to use monument signs or reducing the maximum height of freestanding signs. Another option would be to allow only monument signs and not freestanding signs along Lincoln Avenue.

Recommendation: Explore stricter requirements for signs that would improve the pedestrian scale of the corridor.

Plan Implementation

To implement this plan, Village government must be focused at all levels and in all departments on achieving the enunciated goals and desired results of this plan. Each department and recommending body has a role in moving the Village forward toward achieving this plan. Below are the key elements of this plan and recommended implementation actions.

Streetscape Plan Development:

We recommend that funds be budgeted in the Village's Capital Improvement Plan for preparation of a streetscape plan for Lincoln Avenue. High priority consideration should be given for plan preparation in the next fiscal year, since this plan will help the Village seek grant funds for necessary streetscape improvements and provide reference for improvements which should take place as part of private development. In the actual development of this plan, where appropriate, input from various recommending bodies, should be sought.

Land Use and Form-Based Code Changes:

The land use changes and form based provisions recommended for Lincoln Avenue require changes to the Village's zoning code. Various options are available to achieve this result including overlay districts or separate zoning districts. We recommend that the consultant engaged to update the Village's Zoning Code also be charged with incorporating into this update, the recommendations of this report. Since this work may require further detailing and a variety of determinations, in concert with the Plan Commission, Task Force members have agreed to provide assistance in this work.

Village Departmental Actions

To achieve this plan, implementation will require the ongoing and concerted work of the entire Village organization. Coordinated through the Village Administrator's Office, departments must continue their current work such as median landscaping. These departments also must incorporate new activities called for in this report, such as improving crosswalks, into routine or scheduled work or maintenance plans. More significant improvements, such as those identified in the recommended streetscape plan, will require a strong working relationship and coordination between Village staff and IDOT for implementation.

Village Recommending Bodies

Citizen led recommending bodies to the Village Board have a very large role toward achieving and implementing this plan. Below are key Village Board recommending bodies having certain oversight, duties or expertise which can be utilized and employed to achieve the plan and vision for Lincoln Avenue.

Traffic Commission

Consider and provide input on the creation of the designated parking lane; the posted speed limit and other measures to reduce travel speeds in the corridor. It is possible that parking management in the corridor will become desirous in the future and parking restrictions and regulations for the parking lane may become appropriate. Further, the

Traffic Commission should study and provide input and recommendations regarding appropriate side street vacations, after first reviewing this report and clearly understanding its goals.

Plan Commission/Zoning Board of Appeals

Consider and include in the updated revised zoning code, the recommended use changes, the designation of new business district hubs and the addition of new form-based development regulations. Study and recommend changes to off-street parking requirements in this update. In the role as Zoning Board of Appeals, consider case-by-case variations to these new standards, only where unusual circumstances warrant.

Sculpture and Beautification Committees

Provide input and suggestions in the development of the recommended streetscape plan as well as suggestions on the selection of the specific styles of street furniture, street lights and monuments for the public space focal points.

Sign Appearance Review Board

Consider and review the existing Sign Code and consider recommending changes to the sign code that would enhance the pedestrian scale and environment of the corridor called for in this report.

Economic Development Commission

Study and provide recommendations which could assist in funding of corridor improvements including the long range component of new public open space, as well as measures which could further encourage new private reinvestment and corporate donations in the corridor.

Ad Hoc Building Design Guideline Committee

Study and create an illustrated architectural design guideline booklet which would communicate the building design elements and expectations of the community.

APPENDIX

LINCOLN AVENUE CORRIDOR URBAN DESIGN PLAN AND GUIDELINES

Lincoln Avenue Corridor Study
Lincolnwood, Illinois

**CORRIDOR URBAN DESIGN PLAN
& URBAN DESIGN GUIDELINES**



Prepared by Land Vision, Inc.
for
Lincoln Avenue Task Force
Final Report
November 16, 2005

**Lincoln Avenue Corridor Study
Corridor Urban Design Plan
& Urban Design Guidelines**

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Lincoln Avenue Corridor Study

Corridor Urban Design Plan & Urban Design Guidelines

Lincolnwood, Illinois

I. INTRODUCTION

The Village of Lincolnwood (the Village) and its Lincoln Avenue task Force (LATF) identified the Lincoln Avenue corridor as an opportunity for redevelopment. As the main street through the community, this strip could be transformed into a vibrant community centerpiece with a real sense of place. Land Vision, Incorporated (LVI) was retained in August 2004 to conduct the Lincoln Avenue Corridor Study that would assist the LATF in preparing an urban design plan and urban design guidelines to transform the corridor. Specifically, the LATF expected the Lincoln Avenue Corridor Study to focus on the following:

1. Identify activity centers and infill development opportunities in the corridor
2. Identify means of creating a unified image of the corridor
3. Apply principles of New Urbanism to identify appropriate building setbacks, heights and densities for new development
4. Identify areas for attractive gathering places
5. Recommend land uses and urban design guidelines to help achieve the corridor vision that can become the basis for updating the existing Village Comprehensive Plan and Zoning Ordinance
6. Provide development recommendations and guidelines for the hotel site at Lincoln and Touhy Avenues

To help accomplish these objectives, LVI invited input from Village citizens through an Image Preference Survey (IPS). Ninety-five citizens provided input, and the result of the survey showed strong preference for:

1. More traditional looking buildings than buildings with modern architecture
2. Two-to-four story mixed-use buildings with well-articulated architecture
3. Buildings with inviting, pedestrian-friendly, well-landscaped and safe sidewalks in front
4. Private developments with outdoor gathering places, such as sidewalk cafes, and pedestrian enclaves
5. Buildings located closer to residential areas with more complementary architecture
6. Unobtrusive signs, and
7. Parking areas that are well landscaped.

With the goals and objectives set by the Village and the design preferences identified by the Village citizens, LVI first prepared a Corridor Redevelopment Strategy, and overall Corridor Plan, and finally, a detailed set of Urban Design Guidelines that would govern the corridor redevelopment and enhance appearance of the existing properties.

LVI found that both the Village goals and residents' preferences coincided with the tenets of New Urbanism. New Urbanism is a modern planning movement that seeks to re-create a sense of place and foster walkable, human-scaled communities. It advocates for central downtowns, quality public spaces, diverse streetscapes, and location of stores, services, parks and schools within easy walking distance of homes.

LVI recommends that new buildings – regardless of their use or type – be placed at the front of the property, close to the street, with all parking located behind buildings. This is

principal tenet of New Urbanism; disallowing parking in front of buildings improves the street for pedestrians and enhances streetscape appearances.

Urban design guidelines will further direct the “form” of buildings: how they look, how they relate to the street and to one another, and how they, along with the streetscape, form a cohesive and well-designed urban environment. Such urban design guidelines, called for by New Urbanism, can be outlined in Village land use and zoning codes in what are called form-based regulations.

LVI strongly recommends that, as a follow-up to the adoption of the Corridor Plan and Urban Design Guidelines, the Village adopt a form-based code for the corridor. This will ensure that the vision for the Lincoln Avenue corridor is implemented consistently into the future.

II. CORRIDOR REDEVELOPMENT STRATEGY

As many other strip commercial corridors in the Chicago region, Lincoln Avenue has evolved into a corridor consisting primarily of retail and commercial uses. The current Village regulations permit banks to locate in the corridor more easily than retail uses such that the corridor now has a preponderance of banks. Most of the properties fronting onto Lincoln Avenue are shallow, and many of the existing retail uses and businesses in the corridor are at best nominal. In conjunction with a lack of proper maintenance on some of the corridor properties and a lack of adequate parking on other, the LATF determined that the existing form of development in the corridor is outmoded and fails to exploit the corridor’s full potential.

Given these concerns, LATF identified specific areas of the corridor that could be considered as future hubs of activity with new uses replacing the old and consisting of either mixed-uses or single uses. As opposed to a continuous set of uses spread uniformly along the corridor, LATF determined that creating one main hub and other smaller hubs in the corridor would encourage the development of a potential downtown in Lincolnwood that could be used for creating key gathering places to attract the Village residents and give the village a new identity. LATF also indicated that the site with the “Purple” hotel at the northwest corner of Lincoln Avenue and Touhy Avenue should be given consideration for creating the “downtown” hub in the Village.

In response, LVI recommended a Corridor Redevelopment Strategy that built on the key ideas of the LATF. As shown in Figure A, some of the major recommendations LVI made include:

1. To create a downtown hub in the Village, consider redevelopment of the “Purple” hotel site, the Bank of Lincolnwood site, and the site for Lincolnwood Produce. While some of these recommendations – especially the Bank of Lincolnwood site -- may be implemented in the long-run (over a period of next ten years and beyond), LVI recommended that in order to create a critical mass for the new developments to function as a downtown, the Village would have to allow the redevelopment of more than just the “Purple” hotel site. The range of mixed uses for these sites will include residential, retail and office. The “Purple” hotel site, in addition, may also include a boutique hotel, should the market conditions favor such use to be feasible.

2. LVI also recommended redeveloping the existing Tuffy site with a plaza and a focal-point monument in order to enhance the visual appearance of the major point of entry in to the Village from north. It would also serve as a key marker for the new downtown Lincolnwood
3. Directly in front of Proesel Park, along the park's frontage with Lincoln Avenue, LVI recommended installing another focal-point monument in the middle of the road with new pavements channeling around the monument in an oval form. From a transportation standpoint, this was recommended to calm the traffic along Lincoln Avenue, and as an urban design strategy this would create another key identifier that in conjunction with the monument at the Tuffy site would be the start of forming a cohesive image of the corridor. (See later discussion on Urban Design Guidelines for how these monuments should be treated architecturally).
4. At the site of existing Mobil gas station, located at the triangular site bounded by Lincoln Avenue, Crawford Avenue and Pratt Avenue, LVI recommended the installation of another focal monument, as part of a new mixed-use hub at this location. Surrounded by three major roads, the existing gas station creates adverse traffic conditions in the corridor that would be best ameliorated by its replacement with a public plaza (with the monument). From an urban design standpoint, since the Lincolnwood Public Library is located on Pratt Avenue, the triangular plaza in its front will provide an appropriate foreground for the civic building.
5. For the properties around the new triangular plaza along Lincoln Avenue, LVI recommended retail and residential uses, such that along with the existing public library, this area will become another mixed-use hub in the corridor. While the existing Grossinger Toyota (soon to become Grossinger Volvo) will remain an auto-oriented use at this hub, the rest of the uses will emphasize pedestrian access along with specific outdoor gathering places as part of the redevelopment projects.
6. For the next hub at the intersection of Devon Street and Lincoln Avenue, with the "Bunny Hutch" site providing a key redevelopment opportunity, LATF recommended that the new uses should primarily be commercial. This hub will include the site east of Lincoln Avenue that will be redeveloped into a new commercial use. This commercial hub at the southern end of the corridor will need to design consistency between individual buildings to create a strong sense of entry into the corridor.
7. For the intermediate redevelopment opportunities in the corridor, LVI recommended the primary use to be medium density residential. The key sites proposed for such residential redevelopment include the industrial area southwest of Lincoln Avenue and the railroad and the area east of Lincoln Avenue.
8. In conjunction with many of the above redevelopment projects, LVI recommended closing a number of street segments as they intersect with Lincoln Avenue. These are highlighted on the Corridor Plan for Lincoln Avenue that LVI developed after discussing with the LATF the specific types of medium-density residential and mixed-used projects that would be most appropriate for the corridor.

III. LINCOLN AVENUE CORRIDOR URBAN DESIGN PLAN

As the Corridor Redevelopment Strategy was finalized in discussion with the LATF, LVI prepared a detailed Corridor Urban Design Plan (Corridor Plan) to show how the individual properties or groups of properties may be redeveloped to consistent with the overall strategy. This Corridor Plan is depicted in Figure B (with larger versions of this plan available in poster form).

As Figure B shows, LVI has shown detailed building footprints and parking areas or parking garages that would be necessary to serve a redevelopment project. Many of the recommended projects will require consolidation of individual parcels and street closings. It will be critical for the Village to allow redevelopment of projects with all required parcels consolidated and not permit their individual, piecemeal development. Another major thrust of the recommendations is to “line” the front of the properties with buildings, .i.e. place all new buildings at a set maximum distance from the property line (see Urban Design Guidelines for more discussion). The Village should require all redevelopment projects to have such “liner” buildings with parking for the projects concealed behind the buildings.

While LVI prepared the individual site plans for redevelopment to demonstrate that the recommended projects will be feasible following the principles of New Urbanism, LVI and LATF expressly state that none of the redevelopment projects have to follow the footprints of buildings exactly as drawn by LVI. The Corridor Plan is CONCEPTUAL in nature and it is the intent of LATF to allow developers and builders who will undertake redevelopment in the corridor, a full measure of flexibility provided their projects follow the urban design guidelines. As and when the Village adopts a new form-based code that is based on the urban design guidelines recommended by LVI (see below), the project redevelopers will need to conform to the new ordinance regulations of the Village.

IV. URBAN DESIGN GUIDELINES

As stated by the LATF, the future Lincoln Avenue Corridor must have an overall image that is not haphazard but more uniform and cohesive. A mile-long corridor with an array of uses developed over many years has an image today that is disparate, outmoded (in visual terms as well, in addition to economic) and dysfunctional. The challenge of achieving the goal of cohesive redevelopment out of such conditions can be met by adopting the urban design guidelines recommended by LVI.

In order to meet the full scope of a new urban design for the corridor that also includes the citizens’ image preferences, LVI recommended urban design guidelines that cover not only the domain of the public street, but also the domain of private properties and how the buildings in the corridor relate to the street and to one another.

The public domain guidelines apply to properties that will not be redeveloped for the foreseeable future. These guidelines include the “interface” between the public and private domains, covering the area of private property directly adjacent to the right-of-way. The private domain guidelines apply to a property or a group of properties that is set for redevelopment. These guidelines will cover the portions of redevelopment project that include sidewalks, street furniture, planters and landscaping.

For the corridor as a whole, LVI also recommended that there be on-street parking – whether associated with existing uses or redevelopment projects. While the corridor already has on-street parking, this parking will need to be integrated into a new sidewalk system with “bump-outs” from the existing sidewalk line.

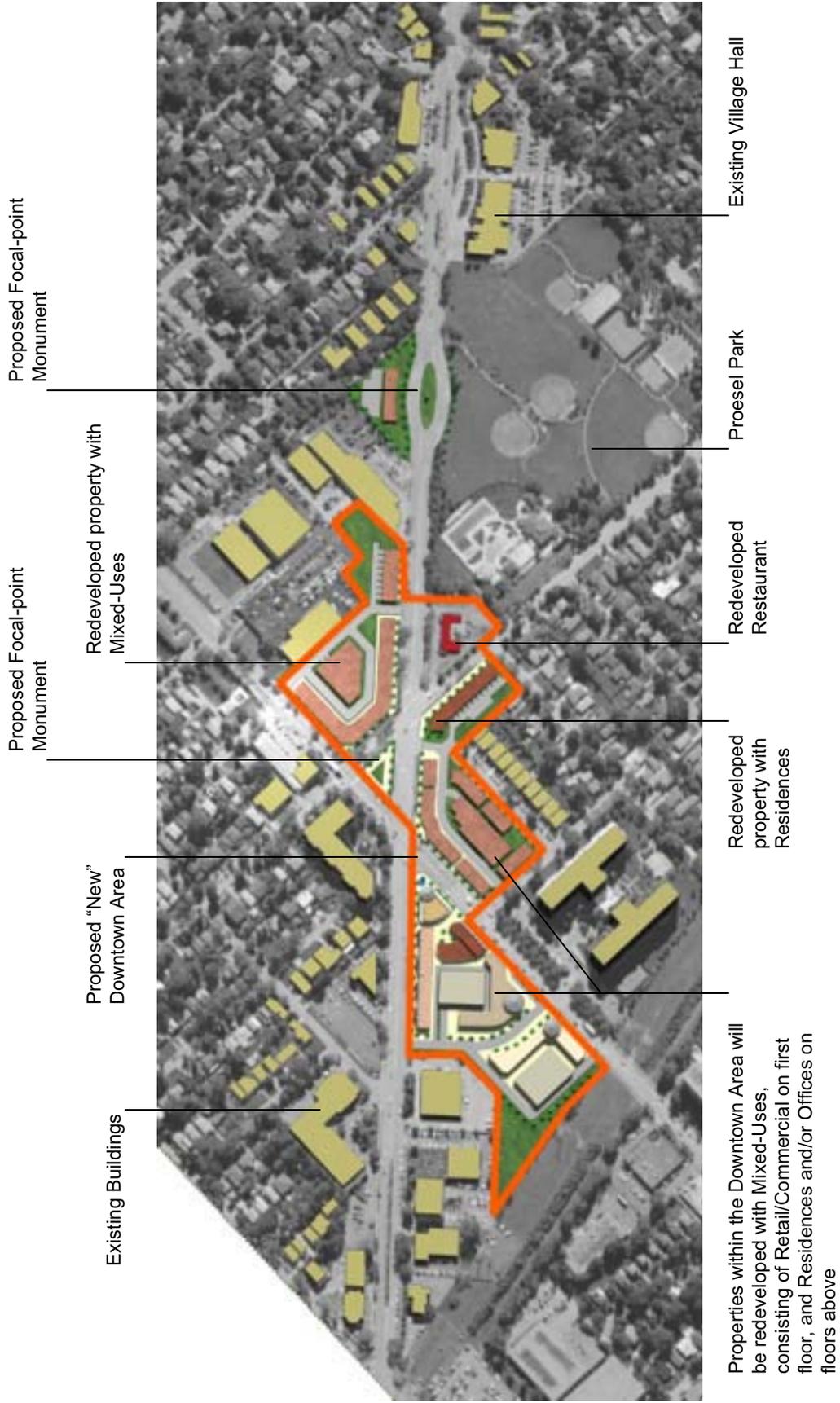


Figure B1: Illustrative Corridor Plan: North
 (Note: All site plans in this exhibit are shown for illustration only. Developers seeking to redevelop properties will follow the urban design guidelines)

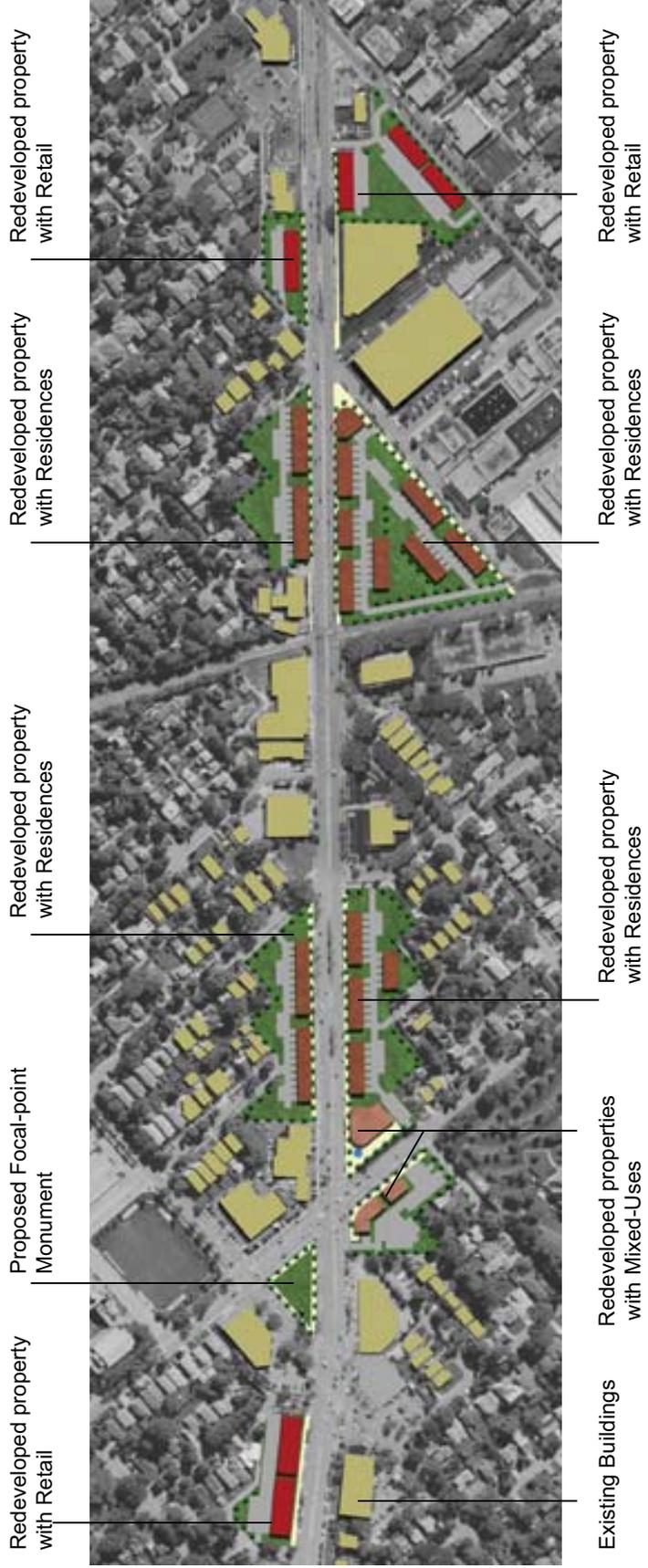


Figure B2: Illustrative Corridor Plan: South
 (Note: All site plans in this exhibit are shown for illustration only. Developers seeking to redevelop properties will follow the urban design guidelines)

The Village will need to prioritize the implementation of the urban design guidelines for the public domain based on the severity of existing conditions and availability of funding. If redevelopment is anticipated, then the urban design guidelines for the private domain (or the form-based code regulations) will be implemented at the time when new project is constructed. Regardless of the timing of when public or private improvements are implemented, in the long run, following the urban design guidelines (or form-based code regulations) will help the corridor achieve the desired cohesive image.

1. GUIDELINES FOR PUBLIC DOMAIN

Covering the area within the R.O.W., including these streetscape elements: Streets; Medians; Street curbs; Parkways; Sidewalks; Street signs; Private signs; Street lights; Median lights and landscaping

Taking into account the above streetscape elements, LVI identified several existing conditions that are typical of different sections of the corridor. For these corridor sections, LVI recommends a number of design guidelines that are illustrated in plan views and cross-sections below. LVI also recommends certain general guidelines that can be applied to any section in the corridor. The Village staff will need to determine the best fit between the existing conditions and the recommended guidelines to select the most appropriate streetscape improvements necessary for implementation in a given section. The recommended guidelines are not meant to supersede the existing, more restrictive zoning regulations that meet the design intent of the guidelines. What follows first is a description of the general urban design guidelines and then the more section-specific guidelines. The guidelines are described in no specific order of preference.

A. General Guidelines

1. **Parkway Trees:** All parkways within the Lincoln Avenue corridor with available space for landscaping should have trees planted at a minimum center-to-center distance of 25 feet (or, as per the existing Village ordinance)
2. **Median Landscaping:** Within the medians along the Lincoln Avenue Corridor, there should be adequate landscaping consisting of shrubs and other materials that are well-maintained
3. **Landscape Materials:** Selection of landscape materials to be used in the parkways, medians or sidewalks should be governed by their hardiness to road salt and with the goal of achieving consistency between one median landscaping to the next
4. **Sidewalk Materials:** Sidewalks in the corridor should have two different materials to better delineate the area used for pedestrians – especially when new sidewalks are built in conjunction with the redevelopment of properties. There should be consistency in the use of sidewalk materials from one property to another
5. **Pedestrian Buffers:** All pedestrian portions of sidewalks should be buffered from the street by landscape planters and/or row of trees
6. **Street Lights:** Selection of new light standards for the corridor should be based on how well they relate to the pedestrians without overwhelming them by their size, height or level of illumination. Examples from other communities such as Elmhurst where IDOT agreed to install special light fixtures in conjunction with a major roadway widening project, suggest that it should be possible for the Village to work with IDOT to have special light fixtures installed in the corridor. The light fixtures with special pedestrian oriented lighting should be located on both sides of the street and not in the median.

7. **Street Signs:** Street signs in the corridor should adopt a distinct new design that makes them stand out as compared to the regular street signs in the rest of the Village to create a special image of the corridor
8. **Street Furniture:** Street furniture, such as benches, information kiosks, telephone kiosks, waste receptacles and bicycle stands, should be provided along the municipal park where it meets the Lincoln Avenue corridor, mixed-use hubs and other areas of the corridor as later deemed appropriate. The design of street furniture in one area should match the street furniture design in another area to further reinforce the visual cohesiveness of the corridor
9. **Business Signs:** For free-standing signs advertising businesses located on the same property, their location, size, height and design should have a level of consistency that lends cohesiveness to the appearance of the corridor
10. **Focal-point Monuments:** The Corridor Plan recommends three focal-point monuments in the corridor: the first at the current Tuffy site (at Touhy and Lincoln Avenues); the second within the proposed bi-furcated roadway pavements of a "traffic oval" in front of Proesel Park, and the third at the current Mobil gas station site (at Crawford and Lincoln Avenue). The design of these monuments should be consistent in order to create a unified set of visual landmarks in the corridor. Of the three monuments the most significant in height and architectural treatment should be the tower in front of the Proesel Park. Because of its close proximity to the Village Hall that's built in Prairie Style architecture, the Village may consider requiring the design of this monument to be in Prairie style as well. In addition to visually tie these major identifiers together, the other two monuments may also be constructed in Prairie style architecture.

B. Section-specific Guidelines

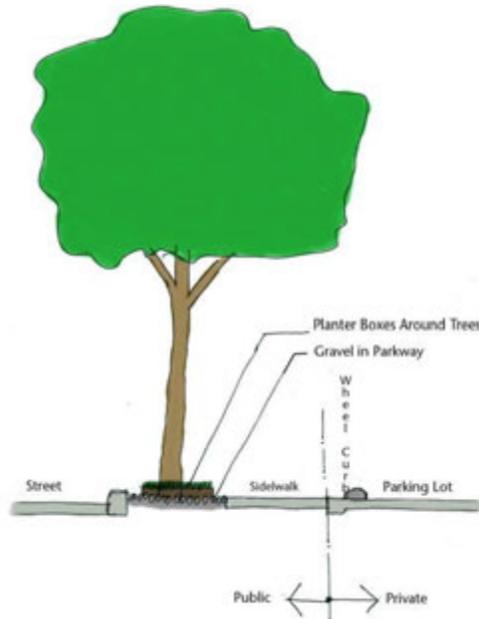
The use of these guidelines will vary from one section of the corridor to another. They are presented as illustrations that show the existing conditions needing improvement and the recommended guidelines to achieve better urban design. The guidelines also cover the interface where the private property abuts the right of way. This interface affects the visual perception of the corridor as much as the quality of the streetscape elements within the right-of-way. Until the time the buildings on a property remain the same – even if the uses in the buildings change – focusing on this interface between the public and private domains will be important. When properties redevelop, new buildings in the corridor will be located close to the street along with new sidewalks in front of the new buildings. This will create a new set of conditions that will be governed by the guidelines for Private Domain. Until change occurs on the private properties, the following design guidelines show how to treat the public domain and the edge of the private properties.

Section-specific Guidelines for Public Domain

NOTE: *These guidelines apply only to properties with existing buildings. For new or redevelopment projects, see Urban Design Guidelines for Private Domain*

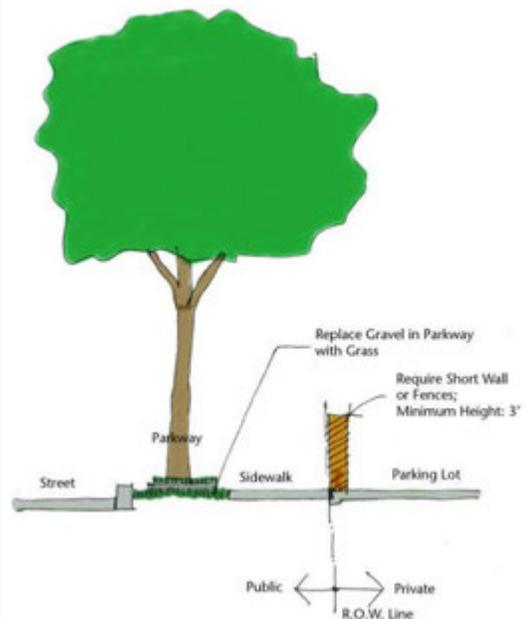
- Existing parkway has materials other than grass, and only wheel-stops exist between the sidewalk and adjoining parking lot

Figure 1: EXISTING CONDITIONS



- Appearance of gravel in the parkway is visually unappealing and inconsistent with the rest of the properties in the corridor with grass in parkway
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists. This makes the corridor more unattractive

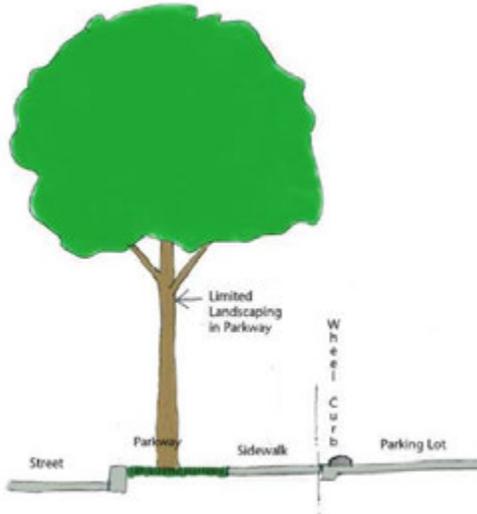
Figure 1A: RECOMMENDED GUIDELINES



- Replace gravel with grass in the parkway
- Require a short wall of no less than 3' to be installed at the property line to conceal the parked cars. Materials and color of wall should primarily match the materials and color of other walls along the corridor

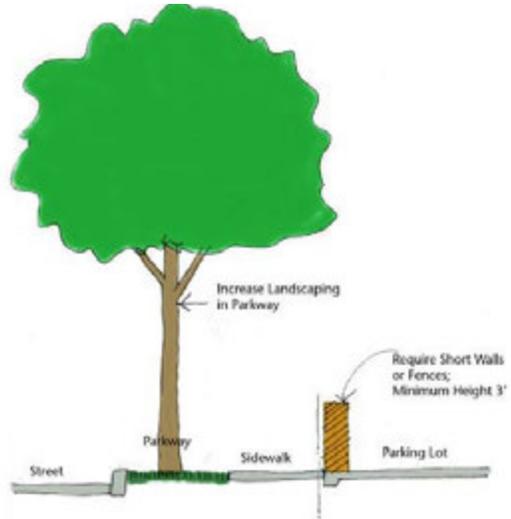
- Existing parkway has limited number of trees and like the first condition has no significant barrier between the sidewalk and the adjoining parking lot

Figure 2: EXISTING CONDITIONS



- Limited number of trees in the parkway fails to define the corridor edge effectively
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists makes the corridor more unattractive

Figure 2A: RECOMMENDED GUIDELINES



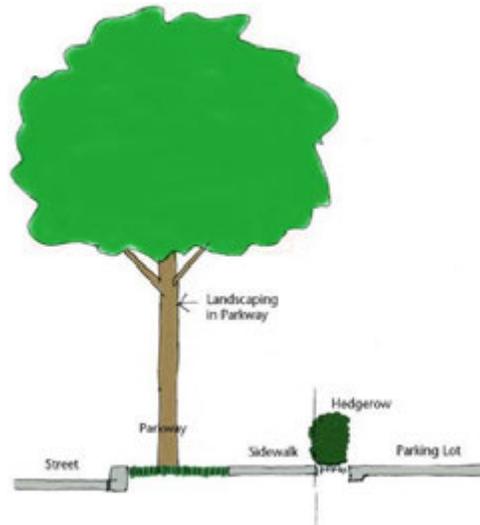
- Increase the number of trees in the parkway to be no more than 25' apart
- As in Recommendation 1, require short walls at the edge of the private property

Alternate to Short Walls (as shown in Recommendations '1' and '2')

Figure 3: EXISTING CONDITIONS

- Limited number of trees in the parkway fails to define the corridor edge effectively
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists. This makes the corridor more unattractive

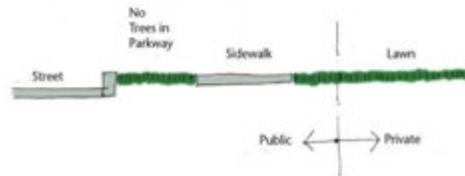
Figure 3A: RECOMMENDED GUIDELINES



- Increase the number of trees in the parkway to be no more than 25' apart
- Instead of short walls, require hedgerows at the edge of the private property in a way. This will not affect the overall parking supplied at the property. Hedgerows should be a minimum 3' tall

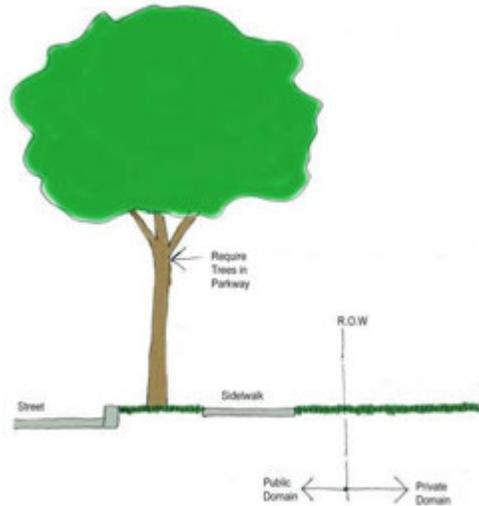
4. Existing parkway has no (or few) trees and the adjoining property has no parking lot

Figure 4: EXISTING CONDITIONS



1. Without the trees, even when the adjoining property has lawn in its front yard and no parking lot, the street lacks a defining edge.

Figure 4A: RECOMMENDED GUIDELINES



1. Require trees to be planted within the R.O.W. where there are no trees, OR when there are few trees,
2. Require trees in the parkway no less than 25' apart

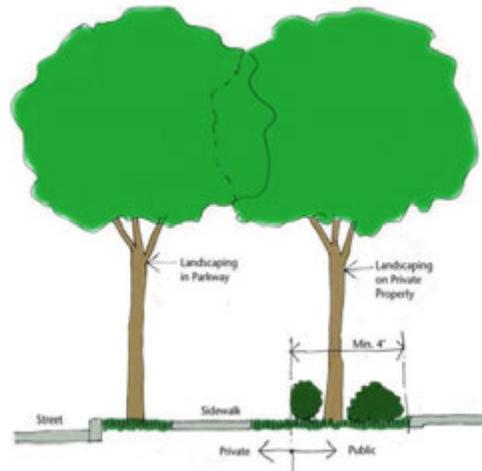
5. Ideal Condition for Landscaping on Private Property

Figure 5: Existing Conditions

When any of the following conditions exist on a property, i.e. if:

1. There are materials within the parkway that are not grass
2. There are limited number of trees in the parkway
3. There are no trees in the parkway at all, and when there are no physical elements separating the adjoining parking lots from the sidewalk

Figure 5A: Recommended Guidelines



1. Require parkway trees
2. On private property, require a landscape buffer no less than 4 feet wide for trees and shrubs. The double row of trees – one in the parkway, the other on private property – should be staggered.

(The Village of Lincolnwood already has examples of this type of streetscape along Devon Avenue near the intersection with Crawford Avenue).

2. GUIDELINES FOR PRIVATE DOMAIN

Covering the private property, including: Placement of buildings; Height of Buildings; Relation of building to street/sidewalk; Relation of building to buildings along one side of the street; Relation of buildings to buildings across the street; Treatment of building entryways; Relation of entryways of one building to the next; Treatment of building bulk; Treatment of special gathering places outside the building

The design guidelines for private domain are described in general terms and not on section-specific or site-specific basis. They are differentiated by categories of use and are based on the design preferences of the Village citizens and LATF members. The three categories of private domain guidelines are:

- A. All Buildings
- B. Residential Buildings
- C. Commercial and Mixed-use Buildings

A. Design Guidelines for All Buildings

1. ***Build-to Line for Placement of Commercial and Mixed-Use Buildings, and Residential Buildings**** : The new commercial and mixed-use buildings in the Lincoln Avenue corridor should be located close to the front of property, at 5 feet from the right-of-way line, with parking concealed from view from Lincoln Avenue and located behind buildings. Residential buildings that do not provide fences in the front yard should also be located at 5 feet from the ROW line. (See Figure 6)

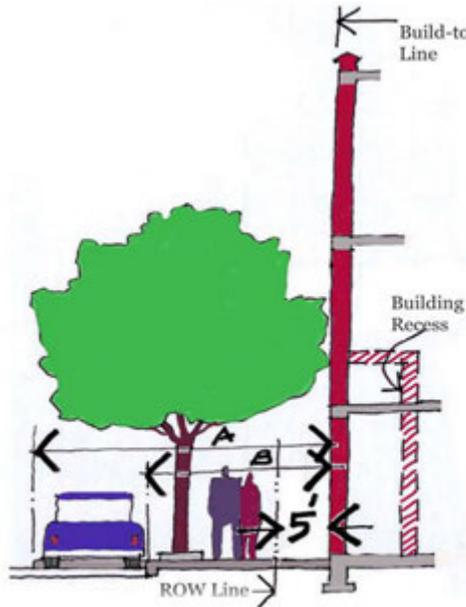


Figure 6: Illustrative Guidelines for Redevelopment of Properties

Maximum Distance from R.O.W to "Build-to" Line:	5 feet
'A': Maximum Distance from Exterior Wall to Street Curb, Including Parking Bay:	23-24 feet
'B': Maximum Distance from Exterior Wall to Street Curb, Excluding Parking Bay:	15 feet

* Applies to only Residential Buildings without fences in the front yard. For residential buildings with fences in the front yard, see guidelines for residential buildings

2. **Building Relationship to Street:** The relationship of new buildings (all commercial and mixed-use and some residential*) to the street and other street amenities is illustrated in Figure 7. The current right-of-way of Lincoln Avenue is 100 feet and the road pavement is 80 feet. The guidelines call for creating “bump-outs” from the existing sidewalk line of up to 8 feet or 9 feet to enclose parking bays within the sidewalk system. Recommended trees in the corridor will be installed at 25’ center-to-center. Buildings will be located at the “Build-to” line 5 feet from the Lincoln Avenue right-of-way. Touhy Avenue has the same right-of-way and pavement widths as Lincoln Avenue. The proposed guidelines will also apply for redevelopment of properties along Touhy Avenue within the recommended mixed-use/downtown hub.

(*Only for residential buildings without fences in the front yard)

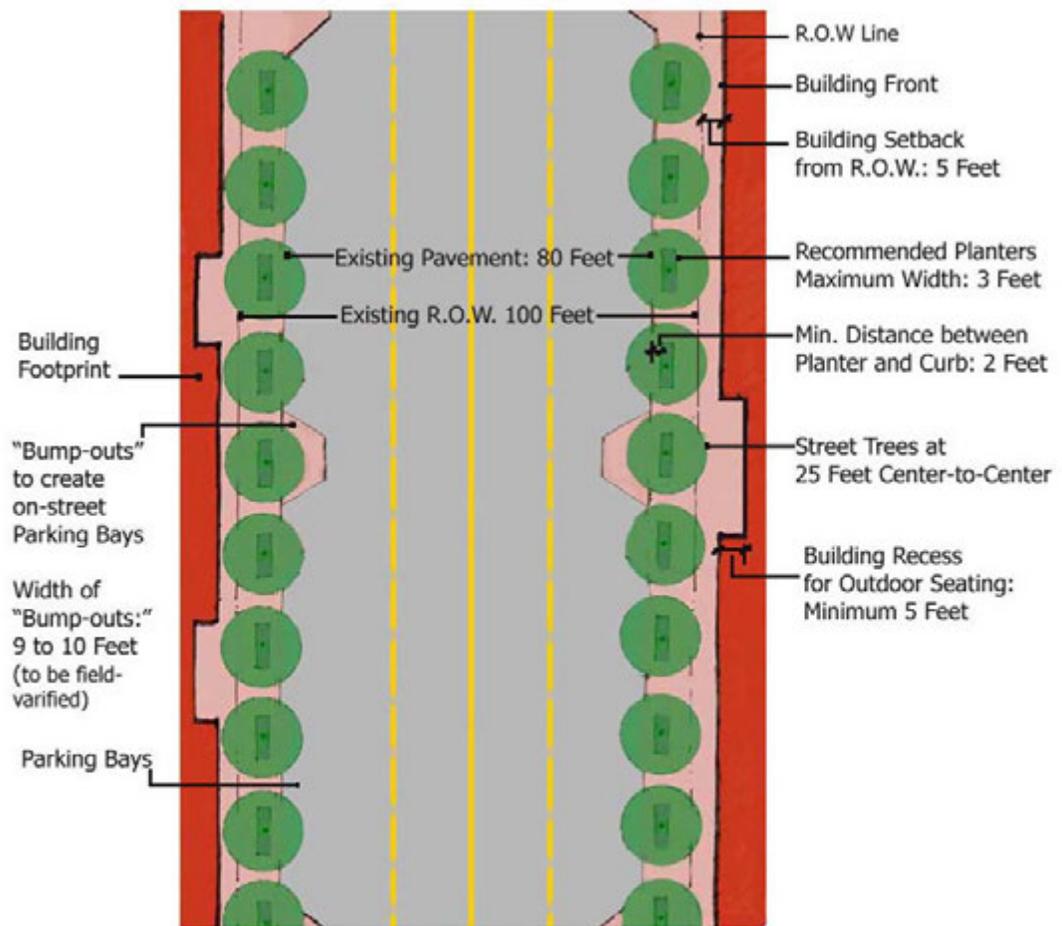


Figure 7: Lincoln Avenue Corridor Redevelopment Guidelines

(Note: These guidelines also apply to Touhy Avenue Redevelopment, in proximity to Lincoln Avenue)

3. **Building Heights:** No building in Lincoln Avenue Corridor should have more than three floors, except in the recommended mixed-use/downtown hub where the buildings may be five stories tall. As shown in Figure 8, portions of building higher than three stories, should be recessed back a minimum of 10 feet.

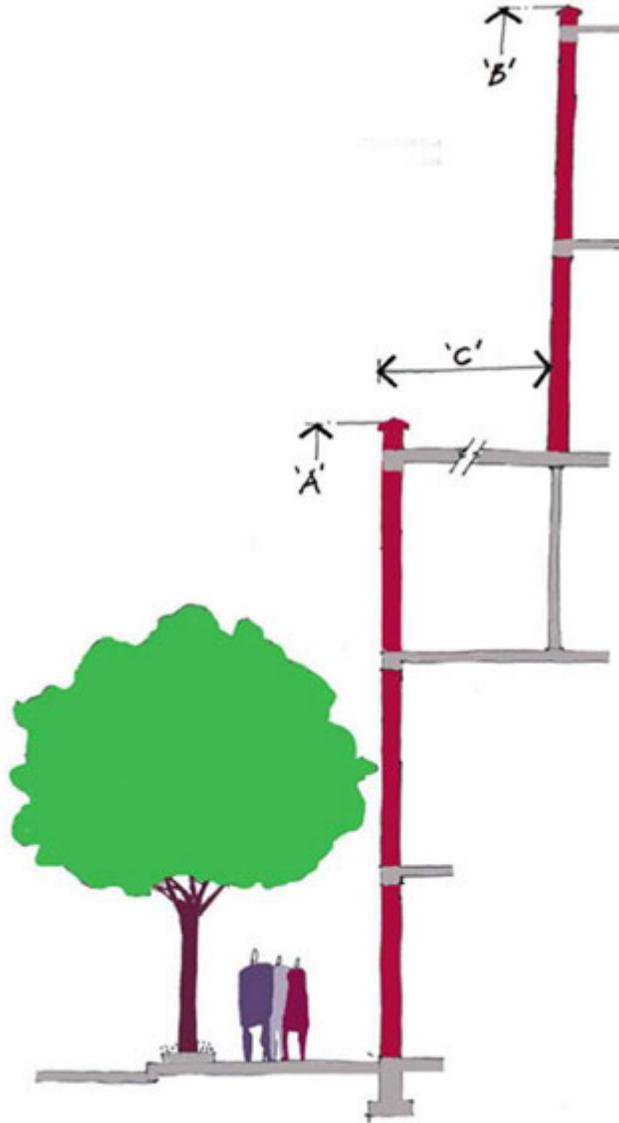


Figure 8: Illustrative Guidelines for Buildings Higher than Three Floors

- | | |
|--|------------------|
| 'A': Maximum Allowable Height Immediately Adjacent to Sidewalks: | 3 Floors/38 feet |
| (Measured to the top of eave from the top of the sidewalk) | |
| 'B': Maximum Allowable Height for Buildings: | 5 Floors/65 feet |
| (Measured to the top of the eave from the top of the sidewalk) | |
| 'C': Minimum Setback for Floors Above Three Floors: | 10 feet |

4. **Building Orientation and Elevation:** New buildings in the corridor should be located parallel to the street frontage. The mass of buildings should be broken up in a rhythmic fashion – well articulated -- so as not to create plain building facades. Balconies and other such elements may be used to create variety and interest and yet remain integral to the overall design of the building (See Image 1)



Image 1: An example of building placed parallel to Lincoln Avenue, with a well-articulated building elevation

5. **Angled Building Facades to Streets:** On an exceptional basis, provided there is a strong reason to do so, the Village may approve buildings with angled façade toward Lincoln Avenue. Image 1 is an example of angled buildings that may be considered acceptable on exceptional basis.



Image 2: An example of angled buildings facing a major street that may be acceptable

6. **Building Corner Towers:** Buildings located at a major intersection should have a tower feature at the corner that may emphasize key entrance to the building. These

corner towers should be in harmony with the building's architecture and will be allowed to exceed the maximum permitted building height so as to create the distinct appearance. (See Image 3 for illustration)



Image 3: An example of "Corner Tower" integral to the overall design of building

7. **On-street Parking:** For new buildings facing Lincoln Avenue there should be on-street parking as part of a sidewalk system that includes "bump-outs" from the existing sidewalk line. All required parking for a use should be provided behind the buildings. (See Figure 7 for parking "bump-outs.")
8. **Building to Building Relation Across or Along Lincoln Avenue:** Buildings located next to each other along the corridor, and buildings located across the street, should have a level of design consistency through either the use of similar materials and colors or distinct architectural elements such as cornices, dormers, balconies, brackets and pendants. It is not necessary for buildings to have the same architectural style to achieve design consistency, although it can help.
9. **Impacts on Adjoining Residential Uses:** Whenever the rear parking impacts the uses on adjoining properties, specific landscape buffers, fences or short walls, or a combination thereof, should be installed by the new building owners
10. **"Dead" Walls Along Streets:** While attractive landscaping can enhance the quality of streetscape, buildings with "dead" or blank walls along the sidewalks should be avoided (or minimized). Residential or other buildings with units above a terrace and parking below that create blank walls along the street should be avoided. (See Images 4 & 5 for comparison.)



Image 4: Example of attractive landscaping that enhances streetscape



Image 5 shows the same streetscape (as in Image 4) with buildings that create blank walls along a sidewalk. This is an example of building relation to street that should be discouraged.

B. Additional Design Guidelines for Residential Buildings

- 1. **Placement of Residential Buildings with Fences in Front Yard:** Residential buildings with fences in front yard should not be placed more than 10 feet from the right-of-way line. In such cases, the fence should be placed at 5 feet from the right-of-way line, with a hedgerow behind the fence toward the building side. The fences in the front yards should be wrought iron that are no more than 4' high. (See Image 6)



Image 6: An acceptable example of fence along residential buildings facing Lincoln Avenue

- 2. **Pedestrian Access to Residential Buildings:** Residential buildings should have direct access to units from the front street side, with strong entries to individual units emphasized by distinct architectural elements such as porticos. (See Image 7)



Image 7: Residential building along street with strong statement for entries to units

3. ***Vehicular Access to Residential Buildings from Lincoln Avenue:*** No residential building should be allowed to have garages facing Lincoln Avenue or driveways with direct vehicular access from Lincoln Avenue.
4. ***Garages in Residential Buildings:*** For residential buildings, the fronts of units at the rear of the property should not face the garages of the units in the building in front. (See Image 8 that is an example of what should not be allowed.)



Image 8 shows residential units facing garages. This should not be allowed in the corridor.

5. ***Pocket Parks in Residential Projects:*** For new residential projects on properties with shallow depth, pocket parks between buildings should be provided to create visual interest and on-site recreational opportunities. (See Image 9)



Image 9: Pocket parks for properties with shallow depth

C. Additional Design Guidelines for Commercial and Mixed-Use Buildings

1. **Outdoor Seating:** For commercial buildings that allow outdoor seating for their patrons, the portion of buildings that houses cafes, restaurants and other such uses should be set back 5' from the normal build-to line. (See Image 10)



Image 10: Additional building set back from build-to line to allow outdoor eating and other activities

2. **Minimum Height for Building Setbacks for Outdoor Seating:** For the outdoor seating areas created by the additional setback from the build-to line, the minimum height for “recesses” should be 10 feet. (See Figures 6 & 7 for building “recesses”)

Lincoln Avenue Corridor

**Lincoln Avenue Task Force
Report and
Recommendations**
Village of Lincolnwood

Task Force Members

James V. Persino, Chairman

Member and Chair, Lincolnwood Economic Development Commission

Mary Couzin

Member, Lincolnwood Plan Commission and past Village Trustee

Maureen Ehrenberg

Member, Lincolnwood Economic Development Commission

Lawrence Elster

Village Trustee

Kenneth Klint

Member, Lincolnwood Plan Commission

Renee Silberman

Resident at -large

Renee Sprogis-Marohn

Village Trustee

Task Force Assisted by

- Mahender Vasandani, Land Vision, Inc.
- Village Staff
- Input from Residents and Businesses

Purpose of Task Force

- To Consider Challenges, Assets and Possibilities for the Lincoln Avenue Corridor
- To Provide Recommendations

Task Force Work

- Undertook Windshield Survey - Assessment of Current Conditions
- Studied Revitalization Efforts - Other Communities
- Solicited Citizen Feedback Via Innovative Citizen Image Preference Survey
- Gathered Input through Town Meeting
- Examined Cutting Edge Planning Tools

Initial Finding of Task Force

- Except for Office Corridor North of Touhy, development pattern aging, many existing uses marginal and corridor not functioning as vibrant asset of the community

VISION for Corridor

- Centerpiece of community and vibrant main street
- Attractive and pedestrian friendly
- Sense of place created
- Lively and useful shopping and business environment

Concepts and Terms

- Business Hubs –Downtown Center
- Mixed Use Development
- Ground Floor Retail
- Residential In-Fill
- Pedestrian Friendly Environment
- Designated Parking Lane-bump outs
- Traffic Calming
- Streetscape Improvements
- Build-to lines
- Building Design Guidelines
- Building Corner-Towers
- Civic focal points
- Form-based regulations
- New Urbanism

Recommendations

Public Streets and Spaces

- Complete Median Landscaping and Parkway Restoration
- Improve Pedestrian Crosswalks
- Create Designated Parking Lane and “Bump Outs”
- Reduce Traffic Speed
- Reduce and Minimize Parkway Curb Cuts
- Create a Unified Corridor Streetscape Plan
- Consider Vacating Select Intersecting Streets
- Create Public Focal Points

Recommendations

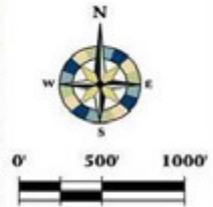
Development Regulations and Design Standards

- Modify Land Use Code to Encourage New Desired Uses
- Establish Business District Hubs
- Create “Form-Based” Regulations
- Create Building Design Guidelines
- Regulate Off-Street Parking Locations
- Improve Business Signage



RECOMMENDED LAND-USE STRATEGY FOR LINCOLN AVENUE CORRIDOR REDEVELOPMENT

LINCOLNWOOD, ILLINOIS



Next Steps

- Develop Streetscape Plan
- Implement Zoning Code Changes
- Continue Median/Parkway Enhancements
- Develop Building Design Guidelines
- Pursue Traffic Calming Measures

Implementation

- Plan Commission
- Traffic Commission
- Economic Development Commission
- Sculpture and Beautification Committees
- Sign Appearance Review Board
- Design Guideline Committee
- Village Departments

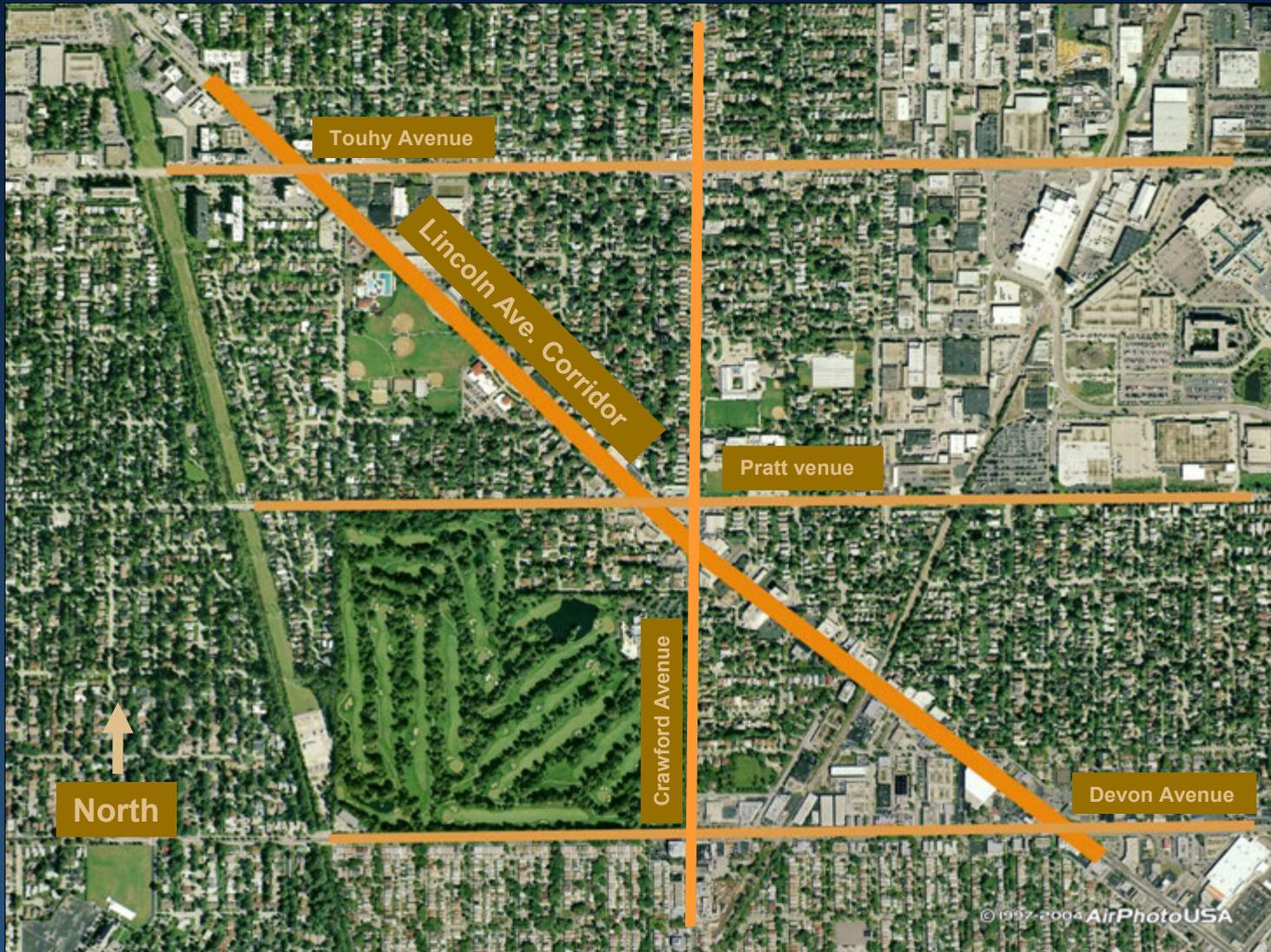
Lincoln Avenue Corridor Urban Design Plan & Design Guidelines

Prepared for the Village of Lincolnwood
Lincoln Avenue Task Force

By
Land Vision, Inc.
February 9, 2006



The Lincoln Avenue Corridor



Existing Corridor Conditions



The Corridor Issues

- **Outmoded Form of Development**
- **Can Not Exploit Full Potential**
- **No Hubs/Community Gathering Places**
- **Excessive Banks**
- **Lack of Positive Identity**
- **No Design Cohesiveness**

The Corridor Goals

- **Revitalize Corridor with New/Appropriate Form of Development**
- **Create Hubs of Activities/Gathering Places**
- **Allow In-fill Housing**
- **Create A Cohesive Image for the Corridor**
- **Add Market Value to Corridor Properties**

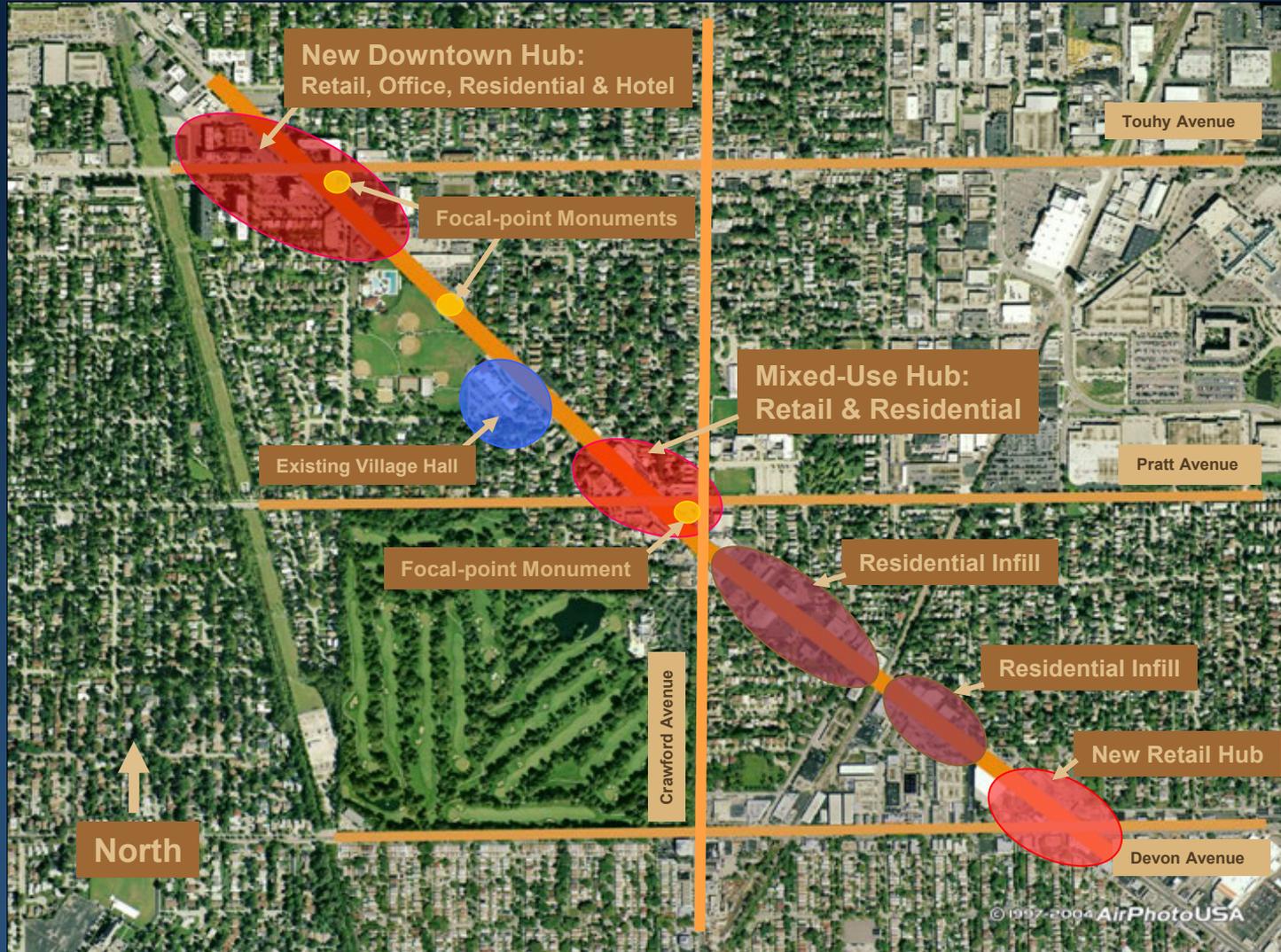
The Planning/Design Approach

- **Image Preference Survey/Development Form**
- **Existing Conditions: Land-Use; Figure/Ground Analysis**
- **Redevelopment Strategy**
 - Create Mixed-Use and Retail Hubs
 - Create A Major Hub/ A New “Downtown”
 - Create In-Fill Housing
- **New Urbanist Corridor Development Form**
- **Corridor Urban Design Plan/Design Guidelines**
- **A New Downtown Plan**
- **Recommend Adoption of Form-Based Code**

New Urbanist Development Form

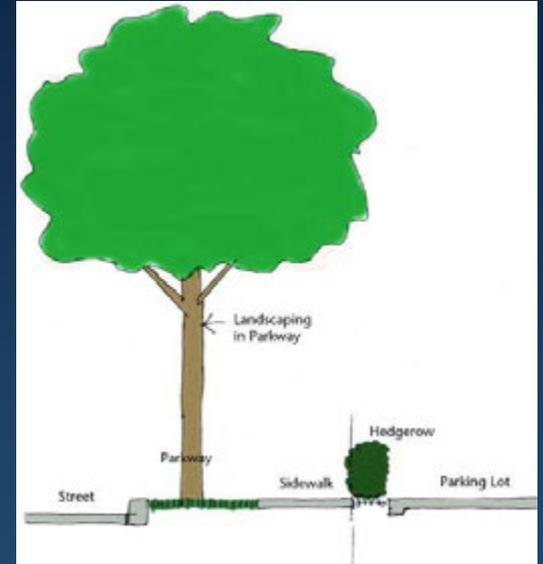
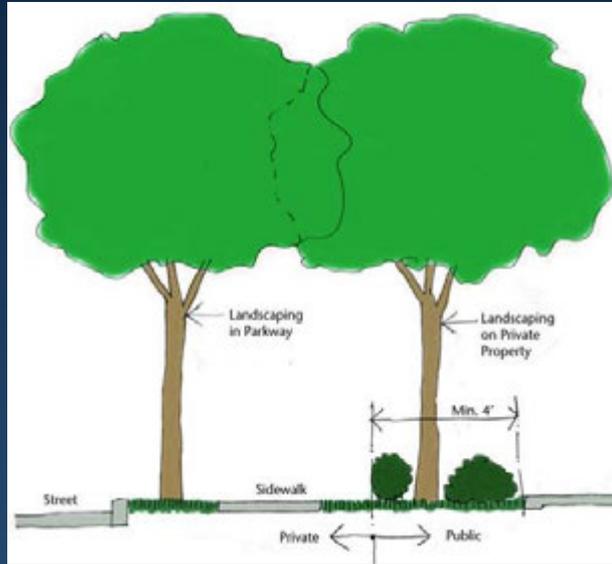
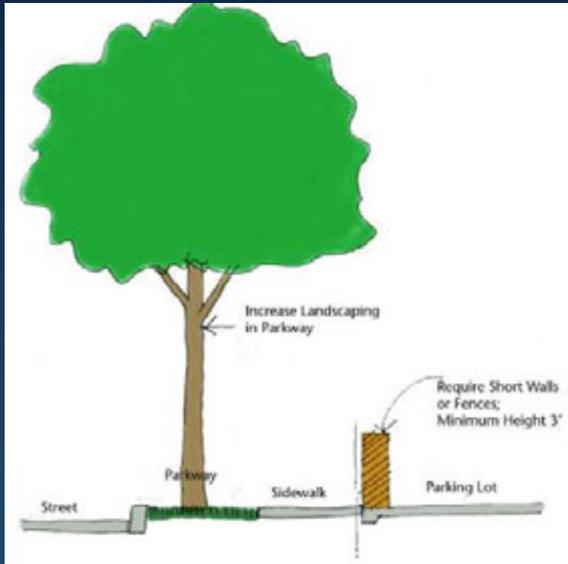


Corridor Redevelopment Strategy



Corridor Urban Design Guidelines

Public Right-of-Way



Parkway Landscaping Guidelines



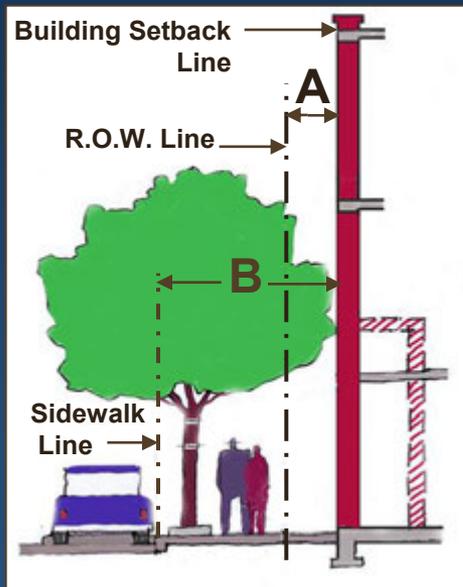
Corridor Urban Design Guidelines

Private Properties

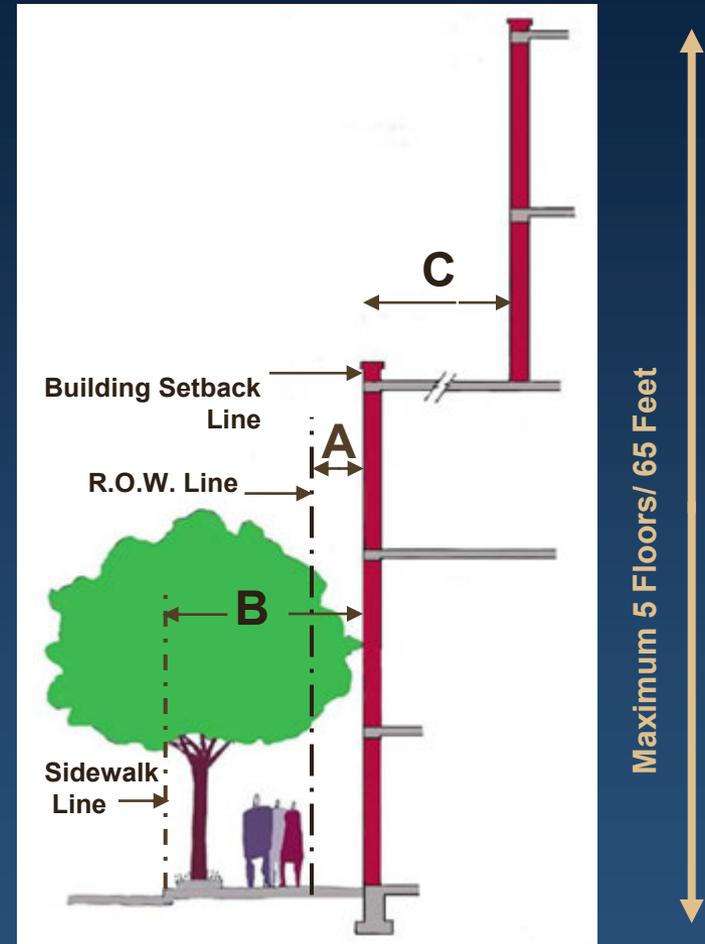
“A”: Maximum Building Setback: 5 Feet

“B”: Max. Distance to Sidewalk: 15 Feet

“C”: Min. Setback Above 3 Floors: 10 Feet

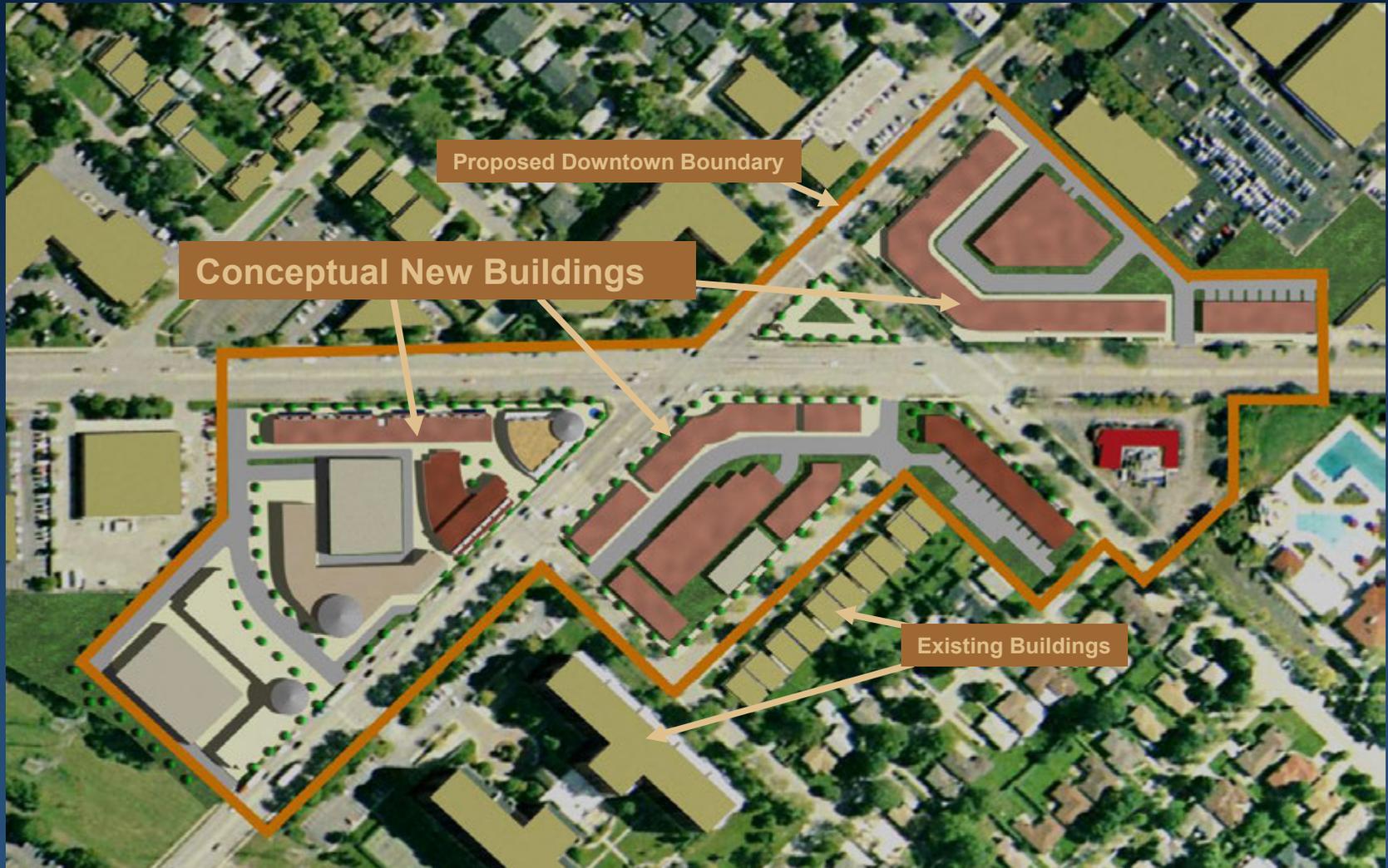


3-Story Buildings



5-Story Buildings: Downtown Area

Corridor Focus: New “Downtown”



Downtown Plan: Phase I



Conceptual Development Form: View from Touhy Avenue
Redevelopment of "Purple" Hotel Site

Downtown Plan: Phase I

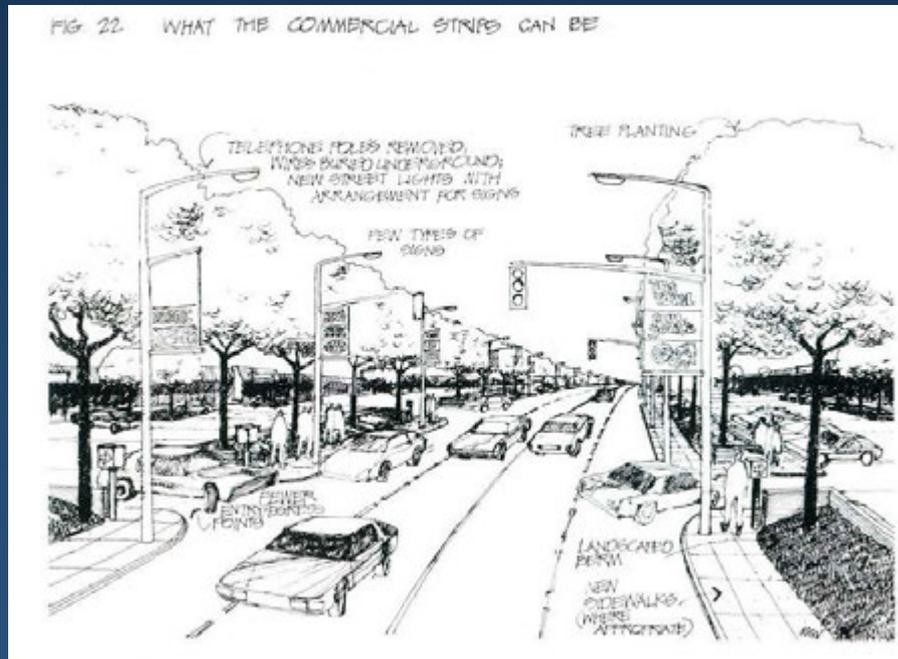


Conceptual Development Form: View from Lincoln Avenue
Redevelopment of "Purple" Hotel Site

Conventional Approach

Emphasis on Public Domain:

- Parkway Trees
- Landscaped Berms/Walls
- Coordinated Signs
- Driveways Consolidation



New Urbanist Approach

Emphasis on both Public & Private Domains:

- **Placement of Buildings Close to Street**
- **Building/Development Form**
- **Outdoor “Rooms” As Formed By Buildings**
- **RECOMMEND Adoption of Form-Based Code**





MEMORANDUM

TO: Chairman Eisterhold
Members of the Plan Commission

FROM: Aaron N. Cook, AICP
Development Manager

DATE: April 17, 2014

SUBJECT: **Comprehensive Plan Amendment – Review of Lincoln Avenue Task Force Report** *(Continued from April 2, 2014, February 26, 2014, January 22, 2014, December 4, 2013 and October 23, 2013)*

Note: Below is the summary provided at the April 2, 2014, February 26, 2014, January 22, 2014, December 4, 2013 and October 23, 2013 Plan Commission meeting. At the October 23rd meeting the Plan Commission heard from three members of the Lincoln Avenue Task Force. At the December 4th meeting, the Plan Commission continued this matter without discussion to the January 22, 2014 meeting.

At the Committee of the Whole (COTW) meeting of July 16, 2013, the Committee discussed multi-family housing in the Lincoln Avenue corridor. The Zoning Code regulations for housing along Lincoln Avenue have their origins in the recommendation and vision adopted as part of the Lincoln Avenue Task Force (LATF) Report. Certain other recommendations of the LATF Report such as land use, design requirements, parking requirements, etc. have been incorporated into the Zoning Code. The LATF Report was adopted and made part of the Comprehensive Plan in May 2006.

At the July 16th COTW meeting, discussion included a possible prohibition on residential developments along Lincoln Avenue which is not consistent with the Zoning Code or the Comprehensive Plan. Trustee Cope indicated his concern over residential developments along Lincoln Avenue include excessive building height, increased traffic generation, and decreased property values. Rather than focus on the housing recommendations only, several trustees recommended a full review of the LATF Report. At the conclusion of the July 16th discussion, the COTW directed the Plan Commission to hold a public hearing to fully review the Lincoln Avenue Task Force Report. As part of this referral, the Village Board requests consideration by the Plan Commission to place a moratorium on residential development along the Lincoln Avenue corridor while the LATF Report is under review.

At the September 11th Plan Commission meeting the members should be prepared to 1) recommend to the Village Board on adopting a moratorium on residential development along the Lincoln Avenue corridor and 2) discuss the LATF Report beginning with the recommendations relative to residential development but also the report as a whole. Upon completion of the review of the LATF Report, if recommendations are made to change the findings of the Report, it is likely that the text of the Zoning Code will need to be amended to ensure consistency between the Comprehensive Plan and the Zoning Code.

Attachments:

1. July 16, 2013 Committee of the Whole Minutes
2. Ordinance No. 2006-2710: An Ordinance Amendment the Village of Lincolnwood's Comprehensive Plan to Incorporate the Report and Recommendations of the Lincoln Avenue Task Force

indicated that NCG does not object to expanding the TIF District. Mr. Elrod informed the Village Board that his staff would prepare a Resolution for consideration at the August 20 Village Board meeting which would confirm that NCG is paying for the study and authorizing staff to proceed.

3. Discussion Concerning the Purple Hotel Site Restoration Standards

The item was presented by Mr. Cook who indicated that the Pre-Development agreement between NCG and the Village for the Purple Hotel expired on July 1, 2013. NCG is requesting reinstatement of the agreement plus additional items. Staff is recommending that NCG be required to submit for land-use relief no later than December 1, 2013, receive land-use approval no later than July 1, 2014, and be permitted to extend site restoration waivers for the adjoining properties until July 1, 2014. Mr. Firsell reported that all asbestos had been abated from the Purple Hotel and the demolition contractor is ready to begin demolishing the building. Mr. Stein indicated that the only items which are pending are the pre-development agreement and the Illinois Department of Transportation's approval. Discussion ensued. The Village Board directed that the subcommittee of Trustees Elster and Patel convene a meeting to discuss the terms of the new pre-development agreement

4. Discussion Concerning the Design Standards for Non-Residential Structures

The item was presented by Mr. Cook. On April 16, 2013, the Village Board considered a recommendation by the Plan Commission concerning proposed changes to the Village's design standards for non-residential buildings. These design standards are commonly referred to as the Village's masonry requirements. In considering this matter at the April 16, 2013 meeting, the Village Board referred this item to the Committee on Ordinances, Rules & Buildings (CORB) for further deliberation. The Plan Commission recommendation on this matter included the following:

1. Eliminating "Masonry Only" requirement from the Lincoln Avenue Overlay District
2. Establish that the exterior walls of non-residential buildings be constructed with a minimum of 75% "high quality materials". This includes a requirement that from the ground level to the top of the first floor windows must be constructed entirely of "high quality materials"
3. Establish a list of permitted "high quality materials" which includes:
 - 1) Brick with a minimum thickness of 2.25 inches
 - 2) Natural stone
 - 3) Sandstone
 - 4) Other native stone
 - 5) Pre-cast decorative stone
 - 6) Glass
4. Add definitions for Brick, Decorative Concrete Block, Masonry, and Stone; and
5. Add *Exterior Insulation Finishing Systems ("EIFS")* and *metal and vinyl lap board siding* as specifically prohibited materials.

CORB considered this matter at its June 11, 2013 meeting and now seeks to have the COTW consider its recommendations on this matter. Discussion ensued. The Village Board referred CORB's recommendation to the Plan Commission for their consideration.

5. Discussion Concerning Multi-Family Housing Zoning in the Lincoln Avenue Corridor

The item was introduced by Mr. Wiberg who indicated that Trustee Cope requested the discussion of multi-family on Lincoln Avenue be discussed at the Committee of the Whole. Mr. Cook indicated that zoning for multi-family housing was the result of the Lincoln Avenue Task Force Plan and provided further background on the matter. Trustee Cope indicated his opposition to allowing the condo development on 6535 North Lincoln Avenue with concerns relating to height, traffic, and affecting other area property values. He proposed prohibiting such developments from Lincoln Avenue in the future and for the Village to place a moratorium in the immediate term. Trustee Patel suggested that the Plan Commission review the Lincoln Avenue Task Force Plan in its entirety. Mr. Cook recommended that the Comprehensive Plan be reviewed first. The Village Board concurred. The matter of reviewing the Plan and placing a moratorium on multi-family housing on Lincoln Avenue was referred to the Plan Commission

Adjournment

VILLAGE OF LINCOLNWOOD

ORDINANCE NO. 2006 2710

**AN ORDINANCE AMENDING THE VILLAGE OF LINCOLNWOOD'S COMPREHENSIVE
PLAN TO INCORPORATE THE REPORT AND RECOMMENDATIONS OF THE LINCOLN
AVENUE TASK FORCE**

ADOPTED BY THE
PRESIDENT AND BOARD OF TRUSTEES
OF THE VILLAGE OF LINCOLNWOOD
THIS 18TH DAY OF MAY, 2006

ORDINANCE NO. 2006-2710

AN ORDINANCE AMENDING THE VILLAGE OF LINCOLNWOOD'S COMPREHENSIVE PLAN TO INCORPORATE THE REPORT AND RECOMMENDATIONS OF THE LINCOLN AVENUE TASK FORCE

WHEREAS, the Village of Lincolnwood (the "Village") is a home rule municipality in accordance with Article VII Section 6(a) of the Constitution of the State of Illinois of 1970;

WHEREAS, the Village has the authority to adopt ordinances and to promulgate rules and regulations that pertain to its government and affairs;

WHEREAS, on November 1, 2001, the Village Board adopted the Village of Lincolnwood Comprehensive Plan (the "Comprehensive Plan");

WHEREAS, the Comprehensive Plan includes, but is not limited to, a policy guide for future Village Board decisions concerning land use within the Village;

WHEREAS, subsequent to the adoption of the Comprehensive Plan, the Village created the Lincoln Avenue Task Force (the "Task Force") to propose a plan for retail redevelopment of properties along the Lincoln Avenue Corridor (the "Corridor") that would be attractive and image enhancing, providing residents with a safe, convenient and pedestrian-friendly environment;

WHEREAS, in November, 2005, the Task Force completed its report and recommendations, with exhibits (the "Report") for enhancement and redevelopment of the Corridor;

WHEREAS, on April 19, 2006, pursuant to a timely, legal notice of public hearing published in the *Lincolnwood Review*, the Task Force presented the Report to the Plan Commission for its review, consideration and recommendation to the Village Board regarding incorporating the Report into the Comprehensive Plan;

WHEREAS, at the April 19, 2006, public hearing all interested persons were allowed the opportunity to present their views regarding the Report, pro and con, to the Plan Commission;

WHEREAS, subsequent to the April 19, 2006, public hearing, the Plan Commission made a recommendation to the Village Board to incorporate the Report into the Comprehensive Plan;

WHEREAS, the Report is attached hereto and incorporated herein by reference as Exhibit A;

WHEREAS, the Village Board reviewed the Report and recommendation of the Plan Commission and determined that it is in the public interest and will promote the public health, safety and welfare of the Village and its residents if the Report is incorporated into the Comprehensive Plan.

NOW, THEREFORE, BE IT ORDAINED by the President and the Trustees of the Village Board as follows:

Section 1. The facts and statements contained in the preamble to this Ordinance are found to be true and correct and are hereby adopted as part of this Ordinance.

Section 2. The Report attached hereto as Exhibit A is hereby incorporated into and made a part of the Comprehensive Plan as Appendix G.

Section 3. In the event there are any inconsistencies between the Comprehensive Plan and the Report, the Report shall control.

Section 4. In all other respects, the Comprehensive Plan shall remain the same.

Section 5. If any section, paragraph or provision of this Ordinance shall be held to be invalid or unenforceable for any reason, the invalidity or unenforceability of such section, paragraph or provision shall not affect any of the remaining provisions of this Ordinance.

Section 6. This Ordinance shall be in full force and effect from and after its passage and approval according to law.

PASSED this 18th day of May, 2006.

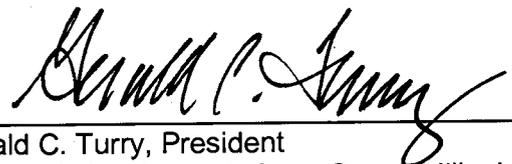
AYES: Trustees Froman, Heidtke, Lebovits, Elster, Sprogis-Marohn and Leftakes

NAYS: None

ABSENT: None

ABSTENTION: None

APPROVED by me this 18th day of May, 2006.



Gerald C. Turry, President
Village of Lincolnwood, Cook County, Illinois

ATTESTED and FILED in my office the
18th day of May, 2006.


Beryl Herman, Village Clerk
Village of Lincolnwood, Cook County, Illinois

EXHIBIT A

Copy of the Lincoln Avenue Corridor Report and Recommendations
of the Lincoln Avenue Task Force

(See Attached)

Lincoln Avenue Corridor



Report and Recommendations of the Lincoln Avenue Task Force

**Village of Lincolnwood
November 2005**

Lincoln Avenue Task Force Members

James V. Persino, Chairman

Member and Chair, Lincolnwood Economic Development Commission

Mary Couzin

Member, Lincolnwood Plan Commission and past Village Trustee

Maureen Ehrenberg

Member, Lincolnwood Economic Development Commission

Lawrence Elster

Village Trustee

Kenneth Klint

Member, Lincolnwood Plan Commission

Renee Silberman

Resident at -large

Renee Sprogis-Marohn

Village Trustee

Staff Assistance

Timothy M. Clarke, AICP
Community Development Director

Consultation Provided

Walter J. Magdziarz, AICP
Mahender Vasandani
Land Vision, Inc.

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- Reduce Traffic Speed
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- Consider Vacating Portions of Intersecting Streets
- Create Public Space Focal Points

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- Encourage Central Business Hubs
- Create Form Based Development Regulations
- Create Building Design Guidelines
- Regulate Off-Street Parking Locations
- Improve Business Signage

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APPENDIX: CORRIDOR URBAN DESIGN PLAN AND GUIDELINES

Executive Summary

We present to you a bold and dynamic plan for Lincoln Avenue. When fully implemented, we believe this plan will result in an attractive corridor and desirable main street for our community. This report is intended to set out the necessary public actions and to guide private reinvestment and redevelopment in order to achieve the dynamic vision we have for the Corridor.

In formulating our recommendations, the Lincoln Avenue Task Force studied the redevelopment and revitalization efforts of a number of communities, including Park Ridge, Deerfield, Mount Prospect and Glenview. While our recommendations embody a bold and dynamic vision for the future of Lincoln Avenue, one which incorporates cutting-edge planning principles, these measures we recommend have been tested and proven successful elsewhere.

The central focus guiding our work has been to foster redevelopment of properties along the corridor which are both attractive and image enhancing, creating retail districts which provide residents with a safe, convenient, attractive and pedestrian friendly environment.

First, our plan calls for a dramatically different land use pattern for the corridor and we recommend significant land use changes to Village code. Specifically, we recommend creating three business districts along the Lincoln Avenue Corridor: one at Devon Avenue, another at Pratt and Crawford Avenues, and a third at Touhy Avenue. Pedestrian oriented retail is called for as the primary ground-floor use in these areas. We envision businesses such as bookstores, galleries, bistros, restaurants, coffee and ice cream shops, boutiques and other specialty retail stores enlivening these business nodes. Our plan also calls for mixed-use development in these business nodes, with upper floors of residential or office uses, to foster redevelopment and to encourage a lively, pedestrian-friendly atmosphere.

At Touhy Avenue, our plan envisions creation of a downtown center for our community, having ground floor retail, community gathering spaces and such anchor uses as a boutique hotel, sit-down restaurants, or entertainment venues. On upper floors, as well as between the three business nodes, our plan calls for introduction of residential development in the corridor.

Second, our plan calls for enhancements to the public right-of-way to improve public safety, create ambiance, and foster a strong sense of place. We recommend designating a parking lane on Lincoln Avenue; improving pedestrian crosswalks; and continuing efforts to restore parkways and to complete the median landscaping project. Most important, we call for a specific streetscape plan to be developed, which would include new streetlights and identifying street furniture and other urban design amenities and elements for the corridor. A bold but longer range component of our plan calls for creating new open spaces and focal points, such as towers or monuments in the corridor at key locations.

Third, we believe that the shape and form of new development in the corridor is critical. Therefore, our plan embraces neo-traditional planning and new urbanism concepts for new development along Lincoln Avenue. In this regard, we call for a set of new development regulations which will regulate the shape and the form of new buildings and private development in the corridor. These “form-based” recommendations for instance call for new buildings along Lincoln Avenue to be parallel to and positioned close to the street, with new off street parking located toward the rear. We recommend additional form based regulations in our report and to assist developers in meeting our vision, we also call for the creation of an illustrated architectural design handbook to guide new building development in the corridor.

Our vision for Lincoln Avenue is exciting and our work has set out many recommendations, both broad and specific. Execution will require a number of actions in order for our vision to become a reality. With Village government dedicated to the vision we present, we believe the Lincoln Avenue Corridor can truly become a main street for our community.

Report and Recommendations for a New Lincoln Avenue Corridor

Introduction

Background of Task Force

The Lincoln Avenue Task Force is an outgrowth of discussions first held in 2002 by the Village's Economic Development Commission. These discussions quickly expanded to include the Village's Plan Commission and a joint meeting of these bodies was held in the fall of 2002 to discuss Lincoln Avenue and its future. The result was a recommendation that a task force be established to consider the challenges, assets and possibilities for the Lincoln Avenue Corridor. President Peter Moy, with consent of the Village Board, subsequently established the Lincoln Avenue Task Force to undertake this work.

The seven-member appointed Task Force is comprised of two members from each of the following bodies: the Village Board, the Plan Commission, and the Economic Development Commission, with one citizen-at-large member completing the roster. Primary staff assistance to the Task Force has been provided by the Community Development Director, with technical and field support provided by key members of the Building Department. The work of the Task Force has been further assisted with an engagement of Land Vision Inc., an urban planning and design firm.

In performing its charge, the Task Force examined many elements. These ranged from conducting a windshield survey of current conditions along Lincoln Avenue; studying the redevelopment efforts of other communities such as Glenview, Deerfield, Park Ridge and Mount Prospect; seeking community input through an Image Preference Survey and conducting and soliciting additional public input in a Town Meeting setting.

The work of the Task Force has resulted in a bold and dynamic plan which will transform the Lincoln Avenue Corridor into a valuable asset for the community.

History of Corridor

Little is written about the origins of Lincoln Avenue. It is known that Lincoln Avenue began as one of the early roads linking Chicago northward to the rural farm areas of Illinois and beyond. Some of the very first businesses in the Village opened along this road, some reportedly to be out of the reach of city restrictions. As Chicago's population expanded outward and traffic increased, so did use of Lincoln Avenue and over time it became a major artery into and out of the City.

Although records are limited, most initial development along Lincoln Avenue in the Village appears to have occurred between the end of World War II and the mid 1960's. With the opening of the Edens Expressway in the 1950's however, Lincoln Avenue's role as a major thoroughfare declined. Just south of the Village, a string of 1950's-1960's era motels still can be found, testament to Lincoln Avenue's history as a major artery before the advent of the Edens.

Existing Development Pattern

The existing development pattern along Lincoln Avenue can be characterized as an auto dependent form of commercial strip. Much of the initial development of this strip occurred at a time when the automobile was beginning to exert a dominant force on land use in America. Unlike areas of Chicago, Evanston, or rail towns like Park Ridge, Lincolnwood and Lincoln Avenue developed without street cars, rail stations, mass transit or the influence of great numbers of pedestrians. With the lack of any significant mass transit, specific business hubs, or a semblance of a central gathering location, the corridor became a long, nearly continuous stretch of commercial development, dependent mainly on access by automobile.

This development pattern occurred prior to the now common phenomenon of multi-car families. Traffic and parking demands in the corridor at the time of its initial development were much less than they are today. Developments during this period often had only a limited number of off street parking spaces, generally arranged along the front of the building and sometimes evolving into a continuous curb cut onto Lincoln Avenue.

Although the Lincoln Avenue Corridor is nearly a continuous strip of businesses from Devon Avenue to Jarvis Avenue, the character of the street changes significantly, albeit gradually, from south to north. At its south end, near the border with Chicago, land uses along Lincoln Avenue tend to be more varied, including auto repair shops, older strip retail centers with continuous curb cuts, and a storage facility. Developments along this stretch of Lincoln Avenue tend to be older, more functionally obsolete in nature and contain more marginal and less intensive uses than areas north. The middle section of Lincoln Avenue, roughly from the rail crossing near Hamlin Avenue to Touhy Avenue, contains a mix of uses, including more current and intense development. This area has multi-story office and bank buildings, automobile dealerships, restaurants, renovated strip centers and community institutional uses such as Village Hall, the Library and Proesel Park. North of Touhy Avenue, to the Village's boundary with Skokie, Lincoln Avenue is primarily comprised of office buildings.

The Task Force found that, except for the office corridor north of Touhy Avenue, the existing development pattern on Lincoln Avenue was aging, many uses were marginal, and the corridor was not functioning as a vibrant main street of the community.

Challenges and Assets of Corridor

Lincoln Avenue presents a number of challenges and assets to its revitalization. First, Lincoln Avenue runs through the Village on a diagonal to the overall grid system of

streets. This results in a number of odd or irregularly shaped parcels at various corners which can inhibit typical development. Moreover, many of the parcels fronting along Lincoln have rather short depths, limiting to a degree their usefulness and redevelopment potential. This is compounded by the close proximity of single family homes to the corridor, some of which utilize shared alleyways with Lincoln Avenue properties and others which have no alley whatsoever.

Much of the existing development along the street was developed without adequate barriers or buffering measures with residential areas. This lack of buffering combined with short parcel depths and irregular parcel configurations can and often has produced land use conflicts. Recently however, the Village has strengthened its zoning code to require landscape buffering of business properties from residential areas. Where space is too constrained for landscape buffers, barrier walls are now required for new development. The implementation of these new standards for buffering should be continued and should help reduce land use conflicts within the corridor.

Lincoln Avenue itself is a relatively wide right-of-way corridor, averaging between 90 – 100 feet in width throughout its length in the Village. Existing conditions in the right-of-way, coupled with its physical width, currently inhibit safe pedestrian crossings and help aid in the dominance of the auto in the corridor. The conditions and width of this right-of-way, currently an impediment to a pedestrian friendly environment, can however be smartly designed to create a unique sense of place and a friendly pedestrian environment. With the creation of a designated parking lane for instance and a thoughtful streetscape design, this wide right-of-way can be utilized and tapped as an asset in the redevelopment of the corridor.

Another untapped asset of the corridor is the immediate market. Lincolnwood is an affluent community and with Lincoln Avenue running through its center, an attractive local market exists. Nonetheless, many marginal uses currently exist in the corridor. With changes to the Village's zoning code to encourage, for instance, restaurants and higher end retailers to locate in the corridor, this local market can be tapped to both serve the community and assist in the corridor's redevelopment.

A further asset which the Village can play upon is the recent and ongoing redevelopment and revitalization efforts occurring immediately south of the Village, along the corridor in the City of Chicago. Recently, a number of existing developments, including the Lincoln Village Shopping Center as well as the conversion of a Kmart store into a Home Depot, have resulted in significant revitalization. Moreover, the City of Chicago has recently acquired a few of the older motel properties, converting these to parks or other uses. And south of Peterson Avenue, new housing in the corridor has recently been constructed.

Vision for the New **Lincoln Avenue Corridor**

We envision the Lincoln Avenue corridor as a vibrant Main Street through our Village. We see it not only as the centerpiece and spine of the community; it is also the entryway into the many residential neighborhoods of the Village. Major Village institutions, such as Village government, the public library, community center and local schools, are located along the street or very nearby. Lincoln Avenue is central to life in the Village.

We envision that as Lincoln Avenue enters the Village from Chicago, the character of the street changes dramatically, being readily apparent to the passerby that they have left behind the big city and have entered a high quality suburb. A landscaped median, new street lights, attractive signs, public sculpture and pedestrian friendly features including easy to use crosswalks and a marked parking lane, are evident and add to the sense that Lincoln Avenue is the Main Street of an upscale community.

Instead of a long continuous commercial strip as presently exists, hubs of activity have been created along Lincoln Avenue, primarily located at major intersections. These activity hubs include ground level retail in high quality, architecturally significant buildings with adequate customer parking, tasteful business signs and attractive landscaping. Comprised of restaurants, ice cream and coffee shops and boutique retail stores, these areas serve the immediate neighboring residential areas and the community as a whole and may include second or third story residential units.

Between these hubs of retail dotting Lincoln Avenue, new housing has been added to already existing bank buildings and office complexes, to replace tired strip centers, auto garages, storage facilities or other obsolete buildings and uses. We envision this in-fill housing as no higher than three stories, with appropriate green space and comprised of attractive townhouse, 3-flat, or small condominium buildings and developments. We see this new low density in-fill housing as providing on-the-one-hand, an attractive entryway into the adjoining single family residential areas, and on-the-other hand, a buffering and transitional use from the business areas and Lincoln Avenue itself to the nearby single family neighborhoods.

While Lincoln Avenue is still a major thoroughfare through the community, through much work, it has been transformed to be more pedestrian friendly and less auto dominated. Instead of a rapid auto path akin to an expressway and a barrier separating Lincolnwood's neighborhoods, the new Lincoln Avenue links neighborhoods together, bringing the Village together at its major institutions and becoming the main street of our community.

Recommendations

The Task Force's recommendations below are divided into two major sections. The first section, Public Streets and Spaces, recommends actions and improvements in the public right-of-way which can help to create a pedestrian-friendly environment and foster a real sense of place. In this section, improved green space, wider sidewalks, new street lights, various urban design elements, as well as slower traffic speeds and eye-catching focal points are recommended to enhance the streetscape and invite residents and shoppers to the corridor. These are improvements which can be undertaken in the public right of way, by the Village in concert with the Illinois Department of Transportation (IDOT).

The second major section of our recommendations, Development Regulations and Guidelines, relates to controls on private property and new development in the corridor. The appearance, location and mix of businesses and residences along the corridor shape perceptions and use. To achieve our vision, this section recommends changes in the Village's existing land use regulations, creation of new form-based regulations and creation of design guidelines which will not only encourage private reinvestment, but will aid in shaping it. Together and in tandem, we believe these set of recommendations will help the Village create a new sense of place and achieve the vision we have for the Corridor.

I. Public Streets and Spaces

Creating an attractive, vibrant and pedestrian-oriented environment in the public right-of-way is an essential element of the Lincoln Avenue Corridor Plan. This plan reinforces many of the Village's ongoing and recently begun initiatives, such as median landscaping and parkway restoration. It also recommends a number of new initiatives, such as creating a designated parking lane and the creation of a coordinated streetscape, including installation of new street lights and street furniture. Long term, the plan calls for creating three public plazas and/or visual focal points along the corridor.

While the plan emphasizes pedestrian orientation and creating pedestrian spaces, it recognizes that Lincoln Avenue is and will remain a major and important thoroughfare in the community. By undertaking attractive pedestrian enhancements, along with the other recommendations of this plan, Lincoln Avenue can become the main street for the Village. Since it is a state controlled street, coordination with the Illinois Department of Transportation (IDOT) is essential and will be required to undertake and implement these recommendations to the right-of-way.

The following are specific recommendations regarding public streets and spaces.

- **Complete Median Landscaping**

In the late 1990's, the Village initiated landscape improvements for the medians of Lincoln Avenue. This multi-year project is continuing. Median landscaping enhances the attractiveness of the corridor and creates positive perceptions of the corridor and community.

Recommendation: Continue and complete median landscaping throughout the corridor.

- **Continue Parkway Restoration**

In 2002 the Village created standards for parkways which include the placement of trees every 25 feet in the parkway and a pervious surface of sod or vegetative ground cover. Until these standards were instituted, parkways in the Village's business areas, including Lincoln Avenue, were essentially allowed to be treated by the adjoining owner as they so desired, providing no cohesion in the corridor and producing a visually cluttered, haphazard, disjointed and unkempt appearance. Moreover, in some sections, parkway areas have been essentially confiscated and turned into long continuous driveway curb cuts that directly access off-street private parking spaces, creating large stretches of impervious surfaces devoid of street trees. This situation conflicts with pedestrian use of public sidewalks and severely affects pedestrian safety. The restoration of parkways and public sidewalks, consistent with the recently established parkway standards, is an essential element toward improving the corridor. Parkway restoration should be diligently pursued, undertaken and implemented whenever possible.

Recommendation: Diligently pursue parkway restoration whenever possible along Lincoln Avenue, including tree planting and sidewalk restoration.

- **Improve Pedestrian Crosswalks**

Currently, many of the marked crosswalks across Lincoln Avenue do not provide the pedestrian with the shortest possible crossing distance for safety. Pedestrians are in the path of traffic for longer periods of time than necessary. To enhance the pedestrian environment and safety, conscious efforts should be undertaken to create highly visible marked crosswalks utilizing the shortest distance possible. The creation of intersection bump outs (described below) will further aid in reducing crosswalk distances, improving public safety and encouraging pedestrian use.

Recommendation: Enhance crosswalk markings and designations, including signage.

- **Create a Designated Parking Lane**

Currently, parking is generally permitted along the entire length of the corridor, although it is not well marked as such. Only in a few concentrated areas, such as in the vicinity of Air Room, is street parking now commonplace. Creation of designated parking lanes on both sides of Lincoln Avenue would help slow traffic and support active retail uses.

The width of the Lincoln Avenue street corridor currently allows for creation of this parking lane. Initially, such a parking lane could be achieved relatively inexpensively through painted striping and signage. A more long term, lasting and beneficial

improvement is to create curbed “bump outs” at intersections. Such bump-out areas essentially bring the sidewalk/pedestrian area of the public right-of-way out into the parking lane, approximately 8-9 feet toward the traffic lanes. Bump outs increase public safety and enhance the pedestrian environment. Specifically, bump outs at intersections: 1) protect and better define the beginning and end of the parking lane between intersections; and, 2) physically shorten the distance for pedestrians crossing Lincoln Avenue. In some areas, additional mid-block bump outs might be desirable to visually break up long parking lane stretches. Moreover, certain urban design features could be added to these new pedestrian spaces. Items such as decorative brick pavers, lighting, benches, bicycle racks, additional landscaping, newspaper vending machines, or even public art could be incorporated into these spaces.

Recommendation: Create a designated, marked parking lane with pedestrian bump outs.

• Reduce Traffic Speed

High speed traffic is not conducive to either a pedestrian environment, or to residential or retail uses. Physical changes such as instituting a parking lane with bump outs, in and of itself, will likely reduce speed to some extent, both because driving lane widths will be perceived to be narrower and because of parking movement maneuvers slowing the speed of traffic.

Although no formal traffic study was commissioned as part of this work, traffic observations in the corridor suggest that much of existing Lincoln Avenue traffic, particularly during rush hour, is essentially “cut-through” traffic generated by Chicago residents seeking a quicker way to the Edens Expressway. This is suggested by two left turn lanes on Lincoln for westbound Touhy Avenue and two left turn lanes on Lincoln in Chicago for eastbound Peterson Avenue. Cut through traffic, particularly related to rush hour periods and home-to-work trips, typically consists of drivers intent on minimizing time. Slowing down Lincoln Avenue traffic in Lincolnwood would result in lengthened drive times for such drivers, deterring some traffic and making alternative routes more attractive.

By slowing down traffic, pedestrian safety is enhanced and greater visual observation by motorists is achieved. Businesses in the corridor will be more visible to the motorist and businesses will be better able to capitalize on impulse shopping and the customer base produced by the traffic volume. Therefore, slower traffic on Lincoln Avenue can help to contribute to the Village’s sales tax revenue.

The current posted traffic speed on Lincoln Avenue is 35 mph, although traffic appears to typically travel at speeds well in excess of the posted limit. Touhy Avenue west of Lincoln is posted at 30 mph, and a reduction in the posted speed limit on Lincoln Avenue to 30 mph should be seriously considered.

Recommendation: Take action to reduce the speed of traffic in the corridor, including physical street design changes and a reduction in the posted speed limit.

- **Reduce and Minimize Parkway Curb Cuts**

Throughout the corridor, particularly as new developments are reviewed, driveway curb cuts in the Parkway should be kept at a minimum and apron width limited to only that needed for vehicle ingress and egress. Where possible and particularly for residential development called for in this plan, driveway curb cuts should be located not on Lincoln Avenue itself, but on adjoining side streets. This would further improve pedestrian safety as well as the aesthetic appeal of the streetscape.

Recommendation: Reduce and minimize Parkway curb cuts along Lincoln and relocate as needed.

- **Create a Unified Corridor Streetscape Plan**

A specific streetscape plan should be created and implemented, specifying and delineating urban design elements that create an attractive, pedestrian scale and sense of place within the corridor. As envisioned, this streetscape plan would specify the style and identify the location of urban design and pedestrian friendly elements such as lighting, benches, bicycle racks, trash bins, and kiosks. This work could explore such other items as special signage, public art and sculptures, hanging flower containers and street banners. Use of brick pavers in sidewalks and crosswalks and specific designs for the recommended parking lane bump outs are other suggested elements that could be explored in development of this streetscape plan.

One critical component of the streetscape plan would be new streetlights for the entire corridor, to replace existing streetlights which are rapidly aging and appear to have exceeded their useful life. Our review suggests that new streetlights should not be installed in the medians as they are now, but along the sides of the street. However, this could be further investigated as the specific streetscape plan is developed. Creation of a specific streetscape plan would not only result in the identification of specific styles and designs for street furniture, sidewalks and light standards, but also would locate them in the corridor. The advantage to developing such a plan is twofold: it will help guide specific improvements in the public right-of-way at specific locations and it would allow the Village to seek Illinois Transportation Enhancement Program (ITEP) grants to undertake such improvements. We believe this is a critical component which should be budgeted within the next two fiscal years.

Recommendation: Create and implement a unified streetscape plan for the corridor.

- **Consider Vacating Portions of Intersecting Streets**

Lincoln Avenue cuts diagonally through Lincolnwood, resulting in many unusual lot configurations along the corridor. The angle of Lincoln Avenue, set against the Village's basic grid pattern of streets, also has produced a number of intersections in the corridor which vary from the typical 90 degree street intersection angle, some involving more than one intersecting street. Because such intersections are not standard or typical, they pose public safety concerns, particularly to those not familiar with the area. This situation also results in long stretches of pavement and active traffic for the pedestrian to confront.

Our plan suggests the vacation of portions of some of these intersecting side streets to eliminate such unusual intersections. Such street vacations also have the potential benefits of reducing traffic into adjoining residential neighborhoods as well as enlarging and improving the shape of development parcels in the corridor. Any vacation of a specific street, however, must be thoroughly examined as to its potential effects. This plan has not undertaken such an investigation and therefore is only suggestive in this regard.

Recommendation: Side street vacations should be explored and examined at appropriate locations.

• Create Public Space Focal Points

A more long term but bold component of this plan, is to consider creating new public space focal points at three locations in the corridor: 1) Lincoln/Touhy/Kostner; 2) Pratt/Lincoln/Crawford; and 3) Lincoln Avenue at Proesel Park.

The first two locations are triangular shaped sites surrounded on all sides by public streets. Converting these sites into public spaces would result in open vistas at these important intersections, producing natural focal points. Along with redevelopment of adjoining properties, these new public spaces would help create a unique sense of place. Based largely on results of an Image Preference Survey of residents, our plan suggests that in these spaces, tower-like monuments be constructed of similar design, potentially utilizing prairie style elements, to help bring continuity to the corridor. This is however only an initial suggestion. As the community moves forward with implementing this element of the plan, other focal point ideas could emerge or be incorporated into such designs. These spaces for instance could commemorate significant local or national events or persons, celebrate the multicultural diversity of the community, or become spaces for the remembrance of the holocaust or of war dead.

A third recommended focal point along the Lincoln Avenue corridor is at Proesel Park. Currently the park, a major asset of the community extends a relatively long distance along Lincoln Avenue. Our observation is that the current design of the park provides neither a welcoming presence nor much activity along the Lincoln Avenue corridor. Our plan suggests highlighting this community asset by creating a focal point on Lincoln Avenue at Proesel Park, similar to those proposed on the triangle parcels. Specifically, this suggestion is to widen and expand the current Lincoln Avenue median at this location and install a similar tower-like icon, visually unifying the three sites. Creating such an expanded median area would require moving and arcing the Lincoln Avenue traffic lanes somewhat, producing an oval shape. This would be another feature which would slow Lincoln Avenue traffic. This proposal obviously would require IDOT review and approval and likely would require some land acquisition.

Recommendation: Consider creating three public focal points at key corridor locations.

II. Development Regulations and Guidelines

In addition to improving the corridor through the appearance and use of public streets and spaces, the Task Force is recommending major changes to the Village's land use codes; the creation of new "form-based" regulations for new development; and, creation of design guidelines for new buildings. Below are the Task Force recommendations as they relate to private properties and their development in the Corridor.

• Modify Land Use Codes to Encourage New Uses

Land use in the corridor is a critical component to the plan. Except for that portion of Lincoln Avenue north of the Purple Hotel site, the plan recommends significant changes to the Village's current land use policies and zoning code. These changes, over time, will enhance and encourage a pedestrian environment for the corridor. Currently, the corridor is one long, mostly undistinguished strip of business properties. The plan proposes creation of three specific business activity hubs at key intersections, with the intersection of Lincoln and Touhy Avenues identified for higher density and activity, in an effort toward creating a downtown business district for the Village (described further below).

Currently, most of Lincoln Avenue is zoned B1 Restricted Business District. This zone permits office and bank facility developments. Retail is currently allowed only through special use designation. To achieve the vision for Lincoln Avenue, a significant change in land use, over time, is warranted. This plan calls for major changes in the zoning code regarding permitted and special uses. Under this plan, retail uses which cater to both Lincolnwood residents and the larger community should be encouraged to locate in designated business hubs. In such hubs, retail uses should either be exclusive uses on the ground level or predominant uses on the ground level to help achieve a pedestrian and retail oriented nexus. Book and drug stores, food specialty shops, coffee shops, sit-down restaurants, ice cream shops, dry cleaners, furniture, clothing and boutique stores should be encouraged and permitted to operate in such hub districts, subject only to complying with the community's development standards, buffering requirements with residential properties and the Village's off street parking requirements. Because office or bank uses generally do not contribute to cross-shopping, walk-in traffic, or a pedestrian environment, such uses in designated business hubs should be kept to a minimum or prohibited altogether on the ground level.

Similarly, specifically auto-oriented or auto-related businesses should be either prohibited altogether or kept to a minimum throughout the corridor. Automobile service and repair shops, oil change centers, gasoline stations, auto part stores and car washes do not contribute to either a pedestrian or shopping environment and should either be prohibited altogether, or highly limited. Where such uses may be authorized because of significant contributions to the tax base, such as automobile dealerships, careful attention to outdoor vehicle display and its effect on the streetscape is in order. Similarly, future drive-thru operations should be highly regulated, limited or prohibited altogether.

The plan presented for Lincoln Avenue embraces the concept of mixed use development, a term not currently found in the Village's Zoning Code. The plan envisions large development parcels, such as the Purple Hotel site, as including multiple land uses within its development. Individual buildings or smaller development parcels should also have

vertical multiple uses, namely retail on the ground floor and residential or office uses on upper floors.

In addition to permitting housing as part of certain mixed use developments, the plan also calls for the introduction of housing as an infill land use between the designated business hubs. Allowing housing as an in-fill land use will help demarcate the edges of the recommended business hubs and eliminate the current condition of a long, continuous strip of business properties. Moreover, development of in-fill housing between business hubs would visually extend the Village's residential neighborhoods into the corridor, thereby accentuating the residential persona of the community. In such in-fill areas, housing as a ground floor use should be permitted. Where housing is permitted, it is envisioned that residential uses in the corridor would be comprised of either multi-level townhouses or condominium buildings.

Recommendation: Modify the current land use code to allow for mixed use development and in-fill housing; and, to prohibit expansion of drive-thru operations and auto-oriented uses.

• Designate Business District Hubs

Related to the above changes being recommended, the Village should incorporate changes into its current zoning code to allow for development of the following proposed business hubs.

Lincoln-Touhy Business Hub

This business hub, centered on the intersection of Lincoln and Touhy Avenues, would include the Purple Hotel, Bank of Lincolnwood and Lincolnwood Produce sites, as well as nearby adjoining properties. This business hub is envisioned as the downtown for the community. Building height in this area is recommended to be limited to 5 floors, exceeding the 3 floor limitation recommended for the remainder of the corridor. Ground floor retail including restaurants, some with alfresco dining, would be a predominant ground floor use, with upper floors of residential and/or office space. Uses which encourage the public to gather, as well as strong or unique retail anchors, a boutique hotel or family entertainment venues, should be incorporated into specific parcel development plans. In addition, sculptures, fountains and plazas with outdoor seating should be designed into parcel redevelopments in this hub. It is envisioned that this business hub would have the highest development intensity in the corridor and potentially would require either underground parking, parking decks or a combination of both. Any parking structure should be well designed with public safety in mind and located so as to be unobtrusive to the primary use activities. For building sides located along Touhy Avenue, we suggest an exception to the recommended corridor build-to line of 5 feet. For such building sides, we recommend a build-to line of 15 feet or greater from the public right of way to account for high traffic volume on Touhy Avenue and pedestrian safety.

Lincoln-Pratt-Crawford Business Hub

This business hub is near the geographic center of the Village and a crossroads for Village residents. Functionally, this business area is expected to serve the more

immediate area and provide shopping convenience for residents. This area is recommended to include mixed use buildings having ground level retail, including such uses as book stores, bistros, drug stores, coffee shops, restaurants, dry cleaners and the like, as well as small outdoor gathering spaces. Two floors of residential units above the ground floor are also suggested for this area. Off street parking should be located toward the rear of properties.

Devon-Lincoln Business Hub

This business hub is an entryway into the community and a major gateway from Chicago. Uses in this business hub are recommended to be primarily retail uses on the ground level, with off street parking located toward the rear. Any development of 2nd or 3rd floors should be for office uses. Good building design, site planning and quality architecture, important throughout the corridor, is paramount in this gateway area for establishing the ambiance of the corridor. New developments in this hub should include strong anchors to provide identity and a sense of place.

Recommendation: Modify the Village Zoning Code to designate and regulate uses and encourage development in newly established business district hubs.

• Create Form-Based Development Regulations

The form and physical shape of private development is just as important as the type of uses which are permitted. Much of the existing development found in the corridor today is strip center in character, meaning that buildings are set back on the property and off street parking is provided directly in front of the building, toward the street. This form of development results in a scenery of endless parking lots, cars and freestanding signs for the passerby. Such an environment challenges any sense of place, and contributes to an auto dominated rather than pedestrian oriented environment. This plan recommends a very different approach and far different form for new development along the corridor.

Prior to modern conveniences, urban form generally took the shape of buildings built close to the public right-of-way, enclosing the street with buildings along it. Shop windows were large to let in light and also to display their products to the passerby.

Our plan embraces the essence of this earlier development pattern and seeks to update this earlier form for the 21st century. Our plan embraces the relatively new planning principles of Neo Traditional Development and New Urbanism, which seek to balance the needs of auto use with people and which assert that the form of development is as important as the permitted use of the property.

We recommend that, in addition to land use changes, applicable sections of the zoning code be created to contain form-based provisions applicable to new development along Lincoln Avenue. The following form-based provisions are recommended for the corridor (additional recommendations are contained in the addendum to this report).

Build-to Line: instead of a traditional setback line, where buildings are required to be setback from the front property line by a minimum distance, we recommend institution of a “Build-to line” that would require new buildings to be built-to. We suggest that this

build-to line should be no greater than 5 feet from the public right of way line. This recommendation would result in new buildings located very near the public sidewalk. For residential-only buildings this build-to line should be no greater than 10 feet from the right of way line, with a 4 foot wrought iron style fence, located 5 feet from the right of way.

Building Orientation: Because Lincoln Avenue travels through the Village at an angle, the orientation and facades of buildings in the corridor are critical and should be parallel to the street. Only in exceptional situations should the orientation of buildings and facades be at an angle to the Lincoln Avenue right-of-way line.

Building Height: Except for the Lincoln/Touhy Avenue Hub, the maximum height in the corridor should be 3 floors or 38 feet. In the Lincoln/Touhy downtown hub, a maximum height of 5 floors or 65 feet is recommended. Any building which is permitted to exceed 3 stories in height is recommended to have a building façade setback of 10 feet for all floors above 3 stories.

Building Corner Towers: Buildings located at corners, particularly at major intersections, should incorporate a turret or tower feature in harmony with the building's architecture. This tower feature component of the building may exceed the maximum height indicated, so as to give the building a distinctive form and focal point.

Façade Windows: Building facades fronting onto Lincoln Avenue, or onto major adjoining public streets or which otherwise are highly visible from the public right-of-way, should incorporate windows in their facades, particularly at the street level. Walls facing the public right of way which lack windows should be prohibited.

Recommendation: *Create form based development regulations as outlined here and elsewhere in this report.*

• **Create Building Design Guidelines**

Our report and the work of our consultant have suggested certain building design guidelines for new buildings in the corridor. The Task Force believes further work and additional design details are appropriate to help guide developers and architects and to illustrate design elements acceptable to the Village. It is envisioned that this work would result in a separate user friendly, heavily illustrated architectural design guideline handbook. To undertake this work, the Task Force recommends the formation of an Ad Hoc Committee of Village design professionals to help prepare this document.

Recommendation: *Create an Ad Hoc Village Design Committee to develop a Building Design Guideline Handbook.*

• **Regulate Off Street Parking Locations**

For new developments, off street parking should be prohibited from the front of new buildings along Lincoln Avenue. Such parking should be located toward the rear of the property. Related to this, the Village's recently adopted requirements ensuring buffering and screening of business properties with adjoining residential properties should be continued and required in all new developments.

The location of off street parking areas and the recommendation to prohibit them from the front of new buildings along Lincoln Avenue is a critical component of this plan and this new standard should only be varied in the most unusual circumstances. For some large scale developments, parking decks may be in order and these should be placed either behind the buildings they serve or with a level of activity, such as retail, located on the ground floor of the parking structure.

This plan does not recommend relaxation of the Village's off street parking standards at this time. These standards should be reviewed to determine their current adequacy for various uses and to investigate the feasibility of shared parking arrangements among uses.

Where new uses will occupy existing buildings, existing front yard parking areas should be screened from view with landscaping or a short screen wall along the public sidewalk. Recently, the Village adopted new code standards requiring such screening treatments for off street parking lots. Strict adherence to these standards is strongly recommended with variations granted only in the most unusual cases.

For the public right-of-way, this plan recommends the creation of a designated parking lane. Although the Task Force does not recommend that off street parking requirements be specifically relaxed, variations for certain desired proposed uses should be considered depending upon the degree of available nearby on-street parking. In the reuse of existing buildings, since there is a strong interplay between providing parking lot screening and providing off street parking spaces, where on-street parking exists and is generally available in the designated Lincoln Avenue parking lane, required parking lot screening should take precedence over the required number of off street parking stalls.

Recommendation: Prohibit new off-street parking lots in front of buildings along Lincoln Avenue. Continue to require landscape screening of existing parking areas.

• Improve Business Signage

The Village updated its Sign Code in the late 1990's to incorporate new community sign standards. This code encourages (but does not require) the use of monument signs instead of freestanding pole signs. With the greater emphasis on creating a pedestrian scale along Lincoln Avenue, consideration should be given to lowering the maximum height of monument signs, possibly to 8 feet, and either creating stronger incentives to use monument signs or reducing the maximum height of freestanding signs. Another option would be to allow only monument signs and not freestanding signs along Lincoln Avenue.

Recommendation: Explore stricter requirements for signs that would improve the pedestrian scale of the corridor.

Plan Implementation

To implement this plan, Village government must be focused at all levels and in all departments on achieving the enunciated goals and desired results of this plan. Each department and recommending body has a role in moving the Village forward toward achieving this plan. Below are the key elements of this plan and recommended implementation actions.

Streetscape Plan Development:

We recommend that funds be budgeted in the Village's Capital Improvement Plan for preparation of a streetscape plan for Lincoln Avenue. High priority consideration should be given for plan preparation in the next fiscal year, since this plan will help the Village seek grant funds for necessary streetscape improvements and provide reference for improvements which should take place as part of private development. In the actual development of this plan, where appropriate, input from various recommending bodies, should be sought.

Land Use and Form-Based Code Changes:

The land use changes and form based provisions recommended for Lincoln Avenue require changes to the Village's zoning code. Various options are available to achieve this result including overlay districts or separate zoning districts. We recommend that the consultant engaged to update the Village's Zoning Code also be charged with incorporating into this update, the recommendations of this report. Since this work may require further detailing and a variety of determinations, in concert with the Plan Commission, Task Force members have agreed to provide assistance in this work.

Village Departmental Actions

To achieve this plan, implementation will require the ongoing and concerted work of the entire Village organization. Coordinated through the Village Administrator's Office, departments must continue their current work such as median landscaping. These departments also must incorporate new activities called for in this report, such as improving crosswalks, into routine or scheduled work or maintenance plans. More significant improvements, such as those identified in the recommended streetscape plan, will require a strong working relationship and coordination between Village staff and IDOT for implementation.

Village Recommending Bodies

Citizen led recommending bodies to the Village Board have a very large role toward achieving and implementing this plan. Below are key Village Board recommending bodies having certain oversight, duties or expertise which can be utilized and employed to achieve the plan and vision for Lincoln Avenue.

Traffic Commission

Consider and provide input on the creation of the designated parking lane; the posted speed limit and other measures to reduce travel speeds in the corridor. It is possible that parking management in the corridor will become desirous in the future and parking restrictions and regulations for the parking lane may become appropriate. Further, the

Traffic Commission should study and provide input and recommendations regarding appropriate side street vacations, after first reviewing this report and clearly understanding its goals.

Plan Commission/Zoning Board of Appeals

Consider and include in the updated revised zoning code, the recommended use changes, the designation of new business district hubs and the addition of new form-based development regulations. Study and recommend changes to off-street parking requirements in this update. In the role as Zoning Board of Appeals, consider case-by-case variations to these new standards, only where unusual circumstances warrant.

Sculpture and Beautification Committees

Provide input and suggestions in the development of the recommended streetscape plan as well as suggestions on the selection of the specific styles of street furniture, street lights and monuments for the public space focal points.

Sign Appearance Review Board

Consider and review the existing Sign Code and consider recommending changes to the sign code that would enhance the pedestrian scale and environment of the corridor called for in this report.

Economic Development Commission

Study and provide recommendations which could assist in funding of corridor improvements including the long range component of new public open space, as well as measures which could further encourage new private reinvestment and corporate donations in the corridor.

Ad Hoc Building Design Guideline Committee

Study and create an illustrated architectural design guideline booklet which would communicate the building design elements and expectations of the community.

APPENDIX

LINCOLN AVENUE CORRIDOR URBAN DESIGN PLAN AND GUIDELINES

Lincoln Avenue Corridor Study
Lincolnwood, Illinois

**CORRIDOR URBAN DESIGN PLAN
& URBAN DESIGN GUIDELINES**



Prepared by Land Vision, Inc.
for
Lincoln Avenue Task Force
Final Report
November 16, 2005

**Lincoln Avenue Corridor Study
Corridor Urban Design Plan
& Urban Design Guidelines**

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Lincoln Avenue Corridor Study

Corridor Urban Design Plan & Urban Design Guidelines

Lincolnwood, Illinois

I. INTRODUCTION

The Village of Lincolnwood (the Village) and its Lincoln Avenue task Force (LATF) identified the Lincoln Avenue corridor as an opportunity for redevelopment. As the main street through the community, this strip could be transformed into a vibrant community centerpiece with a real sense of place. Land Vision, Incorporated (LVI) was retained in August 2004 to conduct the Lincoln Avenue Corridor Study that would assist the LATF in preparing an urban design plan and urban design guidelines to transform the corridor. Specifically, the LATF expected the Lincoln Avenue Corridor Study to focus on the following:

1. Identify activity centers and infill development opportunities in the corridor
2. Identify means of creating a unified image of the corridor
3. Apply principles of New Urbanism to identify appropriate building setbacks, heights and densities for new development
4. Identify areas for attractive gathering places
5. Recommend land uses and urban design guidelines to help achieve the corridor vision that can become the basis for updating the existing Village Comprehensive Plan and Zoning Ordinance
6. Provide development recommendations and guidelines for the hotel site at Lincoln and Touhy Avenues

To help accomplish these objectives, LVI invited input from Village citizens through an Image Preference Survey (IPS). Ninety-five citizens provided input, and the result of the survey showed strong preference for:

1. More traditional looking buildings than buildings with modern architecture
2. Two-to-four story mixed-use buildings with well-articulated architecture
3. Buildings with inviting, pedestrian-friendly, well-landscaped and safe sidewalks in front
4. Private developments with outdoor gathering places, such as sidewalk cafes, and pedestrian enclaves
5. Buildings located closer to residential areas with more complementary architecture
6. Unobtrusive signs, and
7. Parking areas that are well landscaped.

With the goals and objectives set by the Village and the design preferences identified by the Village citizens, LVI first prepared a Corridor Redevelopment Strategy, and overall Corridor Plan, and finally, a detailed set of Urban Design Guidelines that would govern the corridor redevelopment and enhance appearance of the existing properties.

LVI found that both the Village goals and residents' preferences coincided with the tenets of New Urbanism. New Urbanism is a modern planning movement that seeks to re-create a sense of place and foster walkable, human-scaled communities. It advocates for central downtowns, quality public spaces, diverse streetscapes, and location of stores, services, parks and schools within easy walking distance of homes.

LVI recommends that new buildings – regardless of their use or type – be placed at the front of the property, close to the street, with all parking located behind buildings. This is

principal tenet of New Urbanism; disallowing parking in front of buildings improves the street for pedestrians and enhances streetscape appearances.

Urban design guidelines will further direct the “form” of buildings: how they look, how they relate to the street and to one another, and how they, along with the streetscape, form a cohesive and well-designed urban environment. Such urban design guidelines, called for by New Urbanism, can be outlined in Village land use and zoning codes in what are called form-based regulations.

LVI strongly recommends that, as a follow-up to the adoption of the Corridor Plan and Urban Design Guidelines, the Village adopt a form-based code for the corridor. This will ensure that the vision for the Lincoln Avenue corridor is implemented consistently into the future.

II. CORRIDOR REDEVELOPMENT STRATEGY

As many other strip commercial corridors in the Chicago region, Lincoln Avenue has evolved into a corridor consisting primarily of retail and commercial uses. The current Village regulations permit banks to locate in the corridor more easily than retail uses such that the corridor now has a preponderance of banks. Most of the properties fronting onto Lincoln Avenue are shallow, and many of the existing retail uses and businesses in the corridor are at best nominal. In conjunction with a lack of proper maintenance on some of the corridor properties and a lack of adequate parking on other, the LATF determined that the existing form of development in the corridor is outmoded and fails to exploit the corridor’s full potential.

Given these concerns, LATF identified specific areas of the corridor that could be considered as future hubs of activity with new uses replacing the old and consisting of either mixed-uses or single uses. As opposed to a continuous set of uses spread uniformly along the corridor, LATF determined that creating one main hub and other smaller hubs in the corridor would encourage the development of a potential downtown in Lincolnwood that could be used for creating key gathering places to attract the Village residents and give the village a new identity. LATF also indicated that the site with the “Purple” hotel at the northwest corner of Lincoln Avenue and Touhy Avenue should be given consideration for creating the “downtown” hub in the Village.

In response, LVI recommended a Corridor Redevelopment Strategy that built on the key ideas of the LATF. As shown in Figure A, some of the major recommendations LVI made include:

1. To create a downtown hub in the Village, consider redevelopment of the “Purple” hotel site, the Bank of Lincolnwood site, and the site for Lincolnwood Produce. While some of these recommendations – especially the Bank of Lincolnwood site -- may be implemented in the long-run (over a period of next ten years and beyond), LVI recommended that in order to create a critical mass for the new developments to function as a downtown, the Village would have to allow the redevelopment of more than just the “Purple” hotel site. The range of mixed uses for these sites will include residential, retail and office. The “Purple” hotel site, in addition, may also include a boutique hotel, should the market conditions favor such use to be feasible.

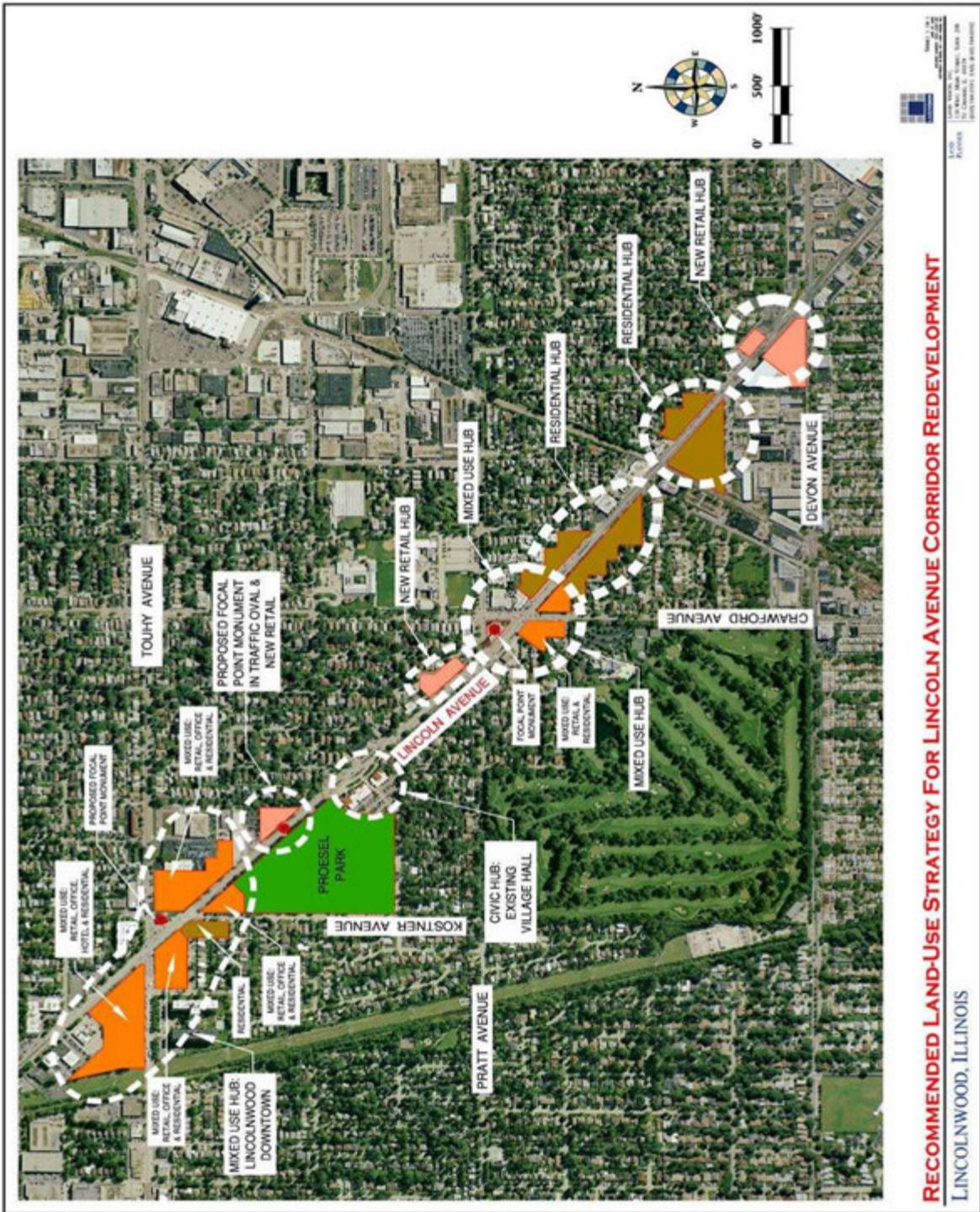


Figure A: Recommended Corridor Redevelopment Strategy

2. LVI also recommended redeveloping the existing Tuffy site with a plaza and a focal-point monument in order to enhance the visual appearance of the major point of entry in to the Village from north. It would also serve as a key marker for the new downtown Lincolnwood
3. Directly in front of Proesel Park, along the park's frontage with Lincoln Avenue, LVI recommended installing another focal-point monument in the middle of the road with new pavements channeling around the monument in an oval form. From a transportation standpoint, this was recommended to calm the traffic along Lincoln Avenue, and as an urban design strategy this would create another key identifier that in conjunction with the monument at the Tuffy site would be the start of forming a cohesive image of the corridor. (See later discussion on Urban Design Guidelines for how these monuments should be treated architecturally).
4. At the site of existing Mobil gas station, located at the triangular site bounded by Lincoln Avenue, Crawford Avenue and Pratt Avenue, LVI recommended the installation of another focal monument, as part of a new mixed-use hub at this location. Surrounded by three major roads, the existing gas station creates adverse traffic conditions in the corridor that would be best ameliorated by its replacement with a public plaza (with the monument). From an urban design standpoint, since the Lincolnwood Public Library is located on Pratt Avenue, the triangular plaza in its front will provide an appropriate foreground for the civic building.
5. For the properties around the new triangular plaza along Lincoln Avenue, LVI recommended retail and residential uses, such that along with the existing public library, this area will become another mixed-use hub in the corridor. While the existing Grossinger Toyota (soon to become Grossinger Volvo) will remain an auto-oriented use at this hub, the rest of the uses will emphasize pedestrian access along with specific outdoor gathering places as part of the redevelopment projects.
6. For the next hub at the intersection of Devon Street and Lincoln Avenue, with the "Bunny Hutch" site providing a key redevelopment opportunity, LATF recommended that the new uses should primarily be commercial. This hub will include the site east of Lincoln Avenue that will be redeveloped into a new commercial use. This commercial hub at the southern end of the corridor will need to design consistency between individual buildings to create a strong sense of entry into the corridor.
7. For the intermediate redevelopment opportunities in the corridor, LVI recommended the primary use to be medium density residential. The key sites proposed for such residential redevelopment include the industrial area southwest of Lincoln Avenue and the railroad and the area east of Lincoln Avenue.
8. In conjunction with many of the above redevelopment projects, LVI recommended closing a number of street segments as they intersect with Lincoln Avenue. These are highlighted on the Corridor Plan for Lincoln Avenue that LVI developed after discussing with the LATF the specific types of medium-density residential and mixed-used projects that would be most appropriate for the corridor.

III. LINCOLN AVENUE CORRIDOR URBAN DESIGN PLAN

As the Corridor Redevelopment Strategy was finalized in discussion with the LATF, LVI prepared a detailed Corridor Urban Design Plan (Corridor Plan) to show how the individual properties or groups of properties may be redeveloped to consistent with the overall strategy. This Corridor Plan is depicted in Figure B (with larger versions of this plan available in poster form).

As Figure B shows, LVI has shown detailed building footprints and parking areas or parking garages that would be necessary to serve a redevelopment project. Many of the recommended projects will require consolidation of individual parcels and street closings. It will be critical for the Village to allow redevelopment of projects with all required parcels consolidated and not permit their individual, piecemeal development. Another major thrust of the recommendations is to “line” the front of the properties with buildings, .i.e. place all new buildings at a set maximum distance from the property line (see Urban Design Guidelines for more discussion). The Village should require all redevelopment projects to have such “liner” buildings with parking for the projects concealed behind the buildings.

While LVI prepared the individual site plans for redevelopment to demonstrate that the recommended projects will be feasible following the principles of New Urbanism, LVI and LATF expressly state that none of the redevelopment projects have to follow the footprints of buildings exactly as drawn by LVI. The Corridor Plan is CONCEPTUAL in nature and it is the intent of LATF to allow developers and builders who will undertake redevelopment in the corridor, a full measure of flexibility provided their projects follow the urban design guidelines. As and when the Village adopts a new form-based code that is based on the urban design guidelines recommended by LVI (see below), the project redevelopers will need to conform to the new ordinance regulations of the Village.

IV. URBAN DESIGN GUIDELINES

As stated by the LATF, the future Lincoln Avenue Corridor must have an overall image that is not haphazard but more uniform and cohesive. A mile-long corridor with an array of uses developed over many years has an image today that is disparate, outmoded (in visual terms as well, in addition to economic) and dysfunctional. The challenge of achieving the goal of cohesive redevelopment out of such conditions can be met by adopting the urban design guidelines recommended by LVI.

In order to meet the full scope of a new urban design for the corridor that also includes the citizens’ image preferences, LVI recommended urban design guidelines that cover not only the domain of the public street, but also the domain of private properties and how the buildings in the corridor relate to the street and to one another.

The public domain guidelines apply to properties that will not be redeveloped for the foreseeable future. These guidelines include the “interface” between the public and private domains, covering the area of private property directly adjacent to the right-of-way. The private domain guidelines apply to a property or a group of properties that is set for redevelopment. These guidelines will cover the portions of redevelopment project that include sidewalks, street furniture, planters and landscaping.

For the corridor as a whole, LVI also recommended that there be on-street parking – whether associated with existing uses or redevelopment projects. While the corridor already has on-street parking, this parking will need to be integrated into a new sidewalk system with “bump-outs” from the existing sidewalk line.

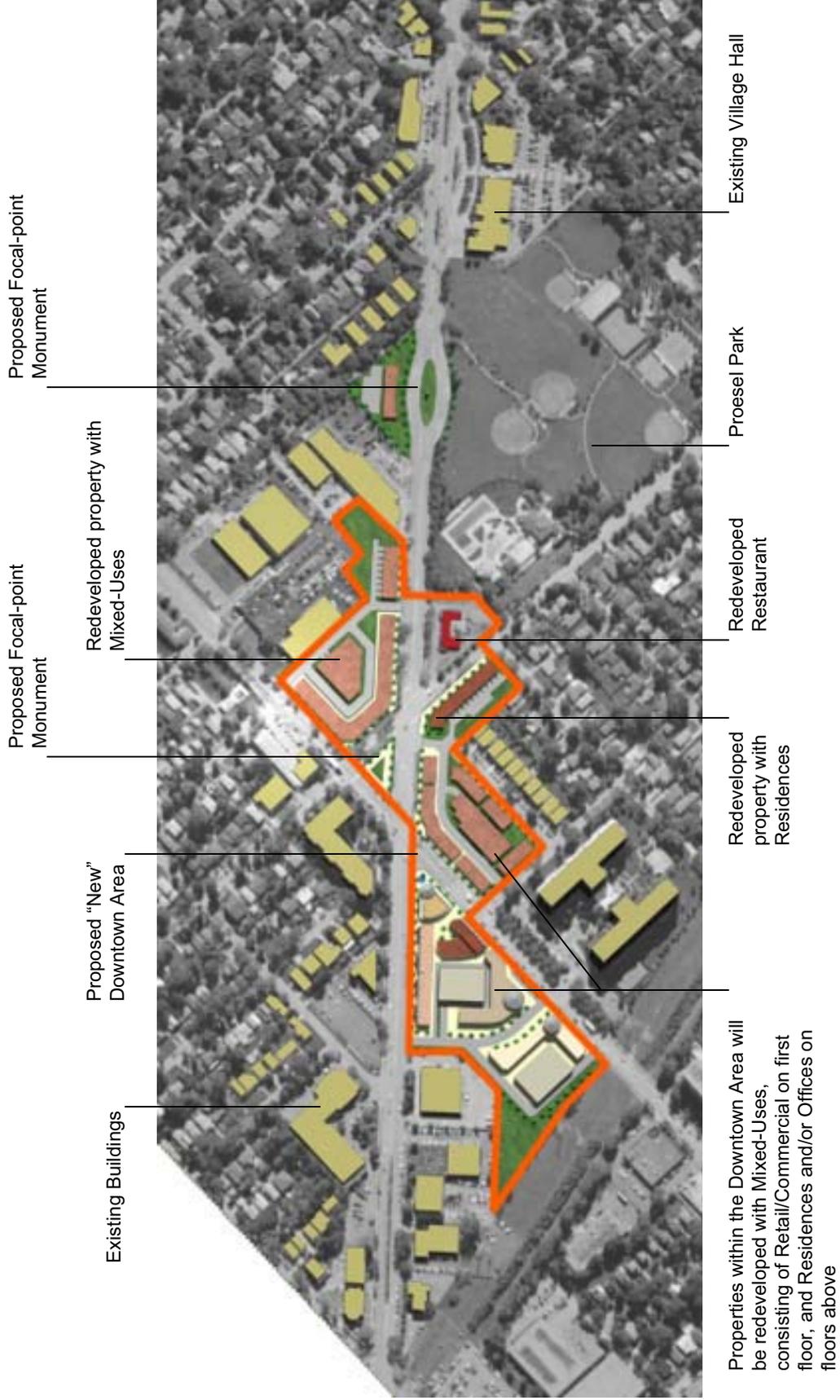


Figure B1: Illustrative Corridor Plan: North
 (Note: All site plans in this exhibit are shown for illustration only. Developers seeking to redevelop properties will follow the urban design guidelines)

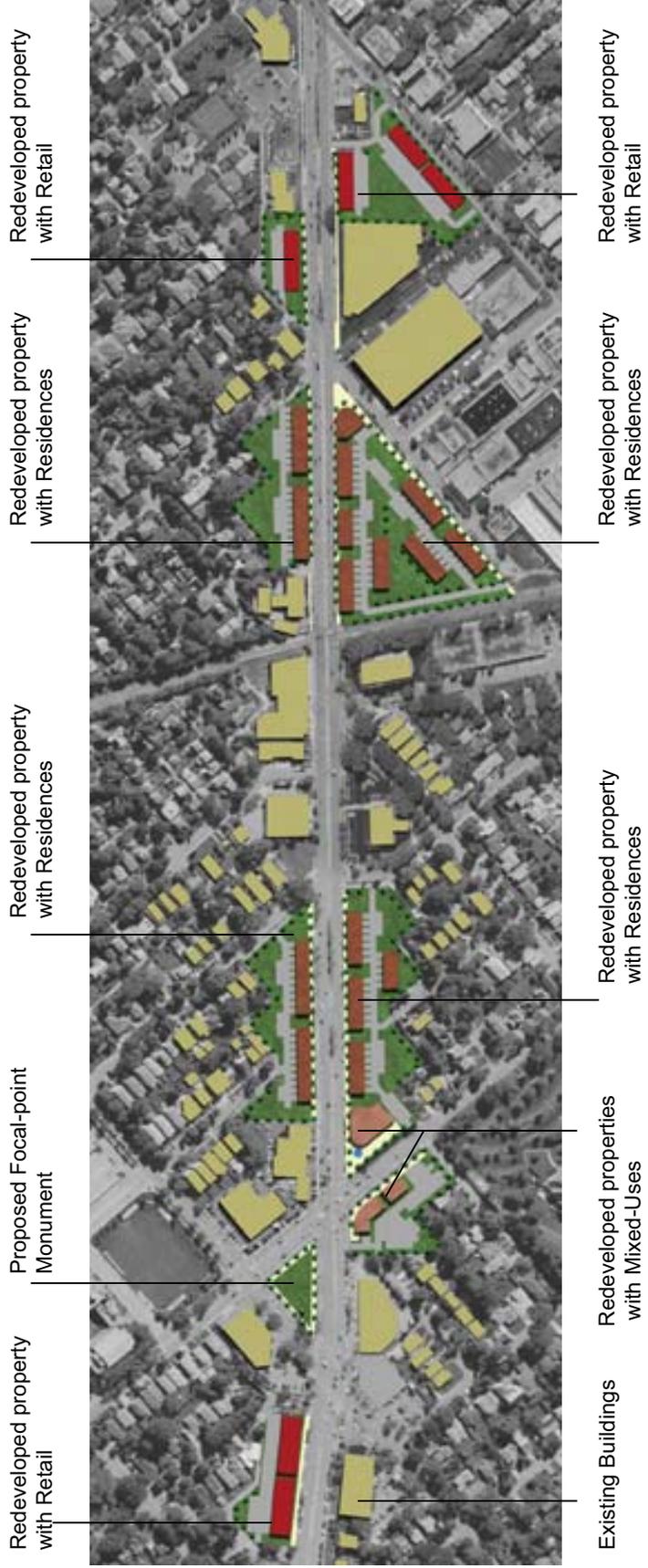


Figure B2: Illustrative Corridor Plan: South
 (Note: All site plans in this exhibit are shown for illustration only. Developers seeking to redevelop properties will follow the urban design guidelines)

The Village will need to prioritize the implementation of the urban design guidelines for the public domain based on the severity of existing conditions and availability of funding. If redevelopment is anticipated, then the urban design guidelines for the private domain (or the form-based code regulations) will be implemented at the time when new project is constructed. Regardless of the timing of when public or private improvements are implemented, in the long run, following the urban design guidelines (or form-based code regulations) will help the corridor achieve the desired cohesive image.

1. GUIDELINES FOR PUBLIC DOMAIN

Covering the area within the R.O.W., including these streetscape elements: Streets; Medians; Street curbs; Parkways; Sidewalks; Street signs; Private signs; Street lights; Median lights and landscaping

Taking into account the above streetscape elements, LVI identified several existing conditions that are typical of different sections of the corridor. For these corridor sections, LVI recommends a number of design guidelines that are illustrated in plan views and cross-sections below. LVI also recommends certain general guidelines that can be applied to any section in the corridor. The Village staff will need to determine the best fit between the existing conditions and the recommended guidelines to select the most appropriate streetscape improvements necessary for implementation in a given section. The recommended guidelines are not meant to supersede the existing, more restrictive zoning regulations that meet the design intent of the guidelines. What follows first is a description of the general urban design guidelines and then the more section-specific guidelines. The guidelines are described in no specific order of preference.

A. General Guidelines

1. **Parkway Trees:** All parkways within the Lincoln Avenue corridor with available space for landscaping should have trees planted at a minimum center-to-center distance of 25 feet (or, as per the existing Village ordinance)
2. **Median Landscaping:** Within the medians along the Lincoln Avenue Corridor, there should be adequate landscaping consisting of shrubs and other materials that are well-maintained
3. **Landscape Materials:** Selection of landscape materials to be used in the parkways, medians or sidewalks should be governed by their hardiness to road salt and with the goal of achieving consistency between one median landscaping to the next
4. **Sidewalk Materials:** Sidewalks in the corridor should have two different materials to better delineate the area used for pedestrians – especially when new sidewalks are built in conjunction with the redevelopment of properties. There should be consistency in the use of sidewalk materials from one property to another
5. **Pedestrian Buffers:** All pedestrian portions of sidewalks should be buffered from the street by landscape planters and/or row of trees
6. **Street Lights:** Selection of new light standards for the corridor should be based on how well they relate to the pedestrians without overwhelming them by their size, height or level of illumination. Examples from other communities such as Elmhurst where IDOT agreed to install special light fixtures in conjunction with a major roadway widening project, suggest that it should be possible for the Village to work with IDOT to have special light fixtures installed in the corridor. The light fixtures with special pedestrian oriented lighting should be located on both sides of the street and not in the median.

7. **Street Signs:** Street signs in the corridor should adopt a distinct new design that makes them stand out as compared to the regular street signs in the rest of the Village to create a special image of the corridor
8. **Street Furniture:** Street furniture, such as benches, information kiosks, telephone kiosks, waste receptacles and bicycle stands, should be provided along the municipal park where it meets the Lincoln Avenue corridor, mixed-use hubs and other areas of the corridor as later deemed appropriate. The design of street furniture in one area should match the street furniture design in another area to further reinforce the visual cohesiveness of the corridor
9. **Business Signs:** For free-standing signs advertising businesses located on the same property, their location, size, height and design should have a level of consistency that lends cohesiveness to the appearance of the corridor
10. **Focal-point Monuments:** The Corridor Plan recommends three focal-point monuments in the corridor: the first at the current Tuffy site (at Touhy and Lincoln Avenues); the second within the proposed bi-furcated roadway pavements of a "traffic oval" in front of Proesel Park, and the third at the current Mobil gas station site (at Crawford and Lincoln Avenue). The design of these monuments should be consistent in order to create a unified set of visual landmarks in the corridor. Of the three monuments the most significant in height and architectural treatment should be the tower in front of the Proesel Park. Because of its close proximity to the Village Hall that's built in Prairie Style architecture, the Village may consider requiring the design of this monument to be in Prairie style as well. In addition to visually tie these major identifiers together, the other two monuments may also be constructed in Prairie style architecture.

B. Section-specific Guidelines

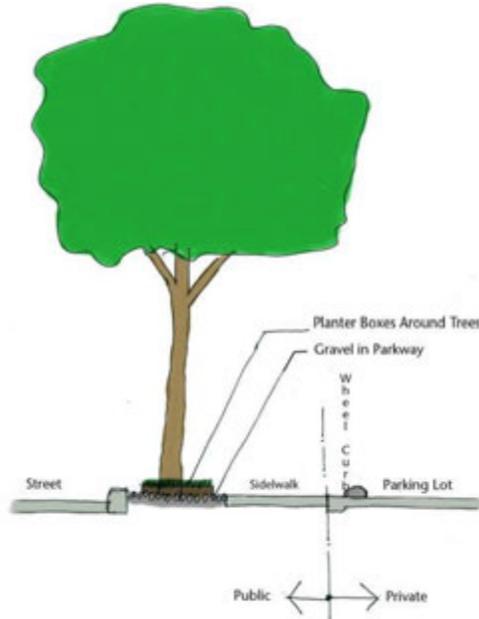
The use of these guidelines will vary from one section of the corridor to another. They are presented as illustrations that show the existing conditions needing improvement and the recommended guidelines to achieve better urban design. The guidelines also cover the interface where the private property abuts the right of way. This interface affects the visual perception of the corridor as much as the quality of the streetscape elements within the right-of-way. Until the time the buildings on a property remain the same – even if the uses in the buildings change – focusing on this interface between the public and private domains will be important. When properties redevelop, new buildings in the corridor will be located close to the street along with new sidewalks in front of the new buildings. This will create a new set of conditions that will be governed by the guidelines for Private Domain. Until change occurs on the private properties, the following design guidelines show how to treat the public domain and the edge of the private properties.

Section-specific Guidelines for Public Domain

NOTE: *These guidelines apply only to properties with existing buildings. For new or redevelopment projects, see Urban Design Guidelines for Private Domain*

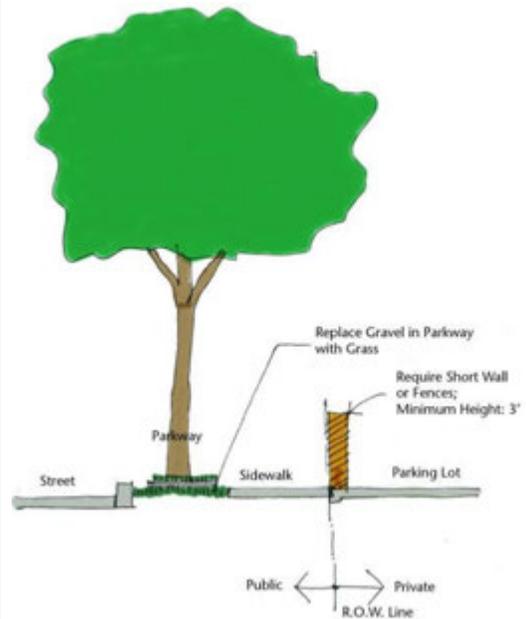
- Existing parkway has materials other than grass, and only wheel-stops exist between the sidewalk and adjoining parking lot

Figure 1: EXISTING CONDITIONS



- Appearance of gravel in the parkway is visually unappealing and inconsistent with the rest of the properties in the corridor with grass in parkway
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists. This makes the corridor more unattractive

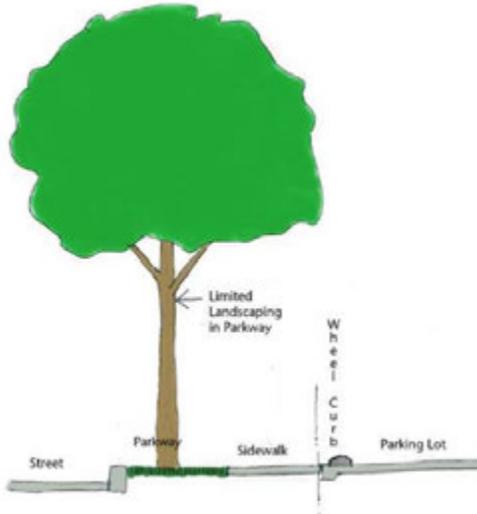
Figure 1A: RECOMMENDED GUIDELINES



- Replace gravel with grass in the parkway
- Require a short wall of no less than 3' to be installed at the property line to conceal the parked cars. Materials and color of wall should primarily match the materials and color of other walls along the corridor

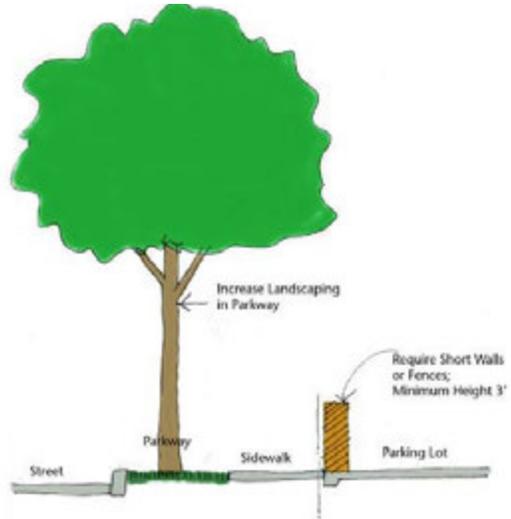
- Existing parkway has limited number of trees and like the first condition has no significant barrier between the sidewalk and the adjoining parking lot

Figure 2: EXISTING CONDITIONS



- Limited number of trees in the parkway fails to define the corridor edge effectively
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists makes the corridor more unattractive

Figure 2A: RECOMMENDED GUIDELINES



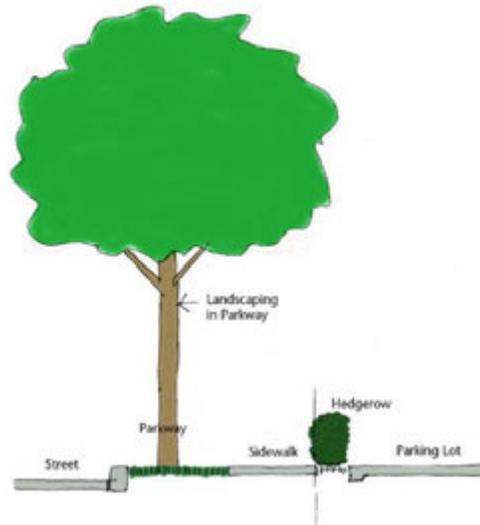
- Increase the number of trees in the parkway to be no more than 25' apart
- As in Recommendation 1, require short walls at the edge of the private property

Alternate to Short Walls (as shown in Recommendations '1' and '2')

Figure 3: EXISTING CONDITIONS

- Limited number of trees in the parkway fails to define the corridor edge effectively
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists. This makes the corridor more unattractive

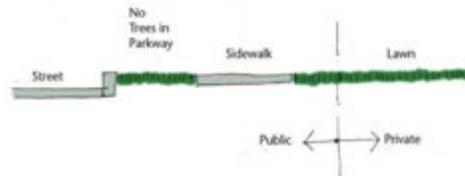
Figure 3A: RECOMMENDED GUIDELINES



- Increase the number of trees in the parkway to be no more than 25' apart
- Instead of short walls, require hedgerows at the edge of the private property in a way. This will not affect the overall parking supplied at the property. Hedgerows should be a minimum 3' tall

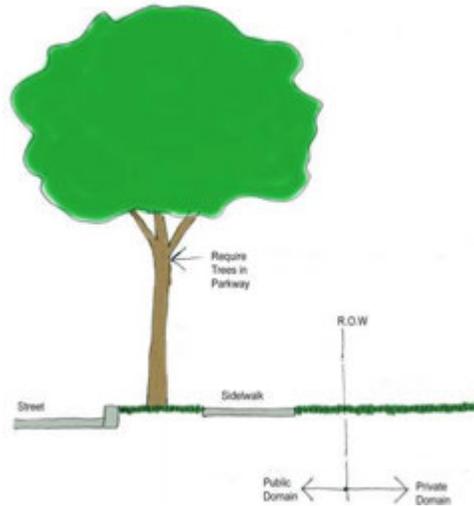
4. Existing parkway has no (or few) trees and the adjoining property has no parking lot

Figure 4: EXISTING CONDITIONS



1. Without the trees, even when the adjoining property has lawn in its front yard and no parking lot, the street lacks a defining edge.

Figure 4A: RECOMMENDED GUIDELINES



1. Require trees to be planted within the R.O.W. where there are no trees, OR when there are few trees,
2. Require trees in the parkway no less than 25' apart

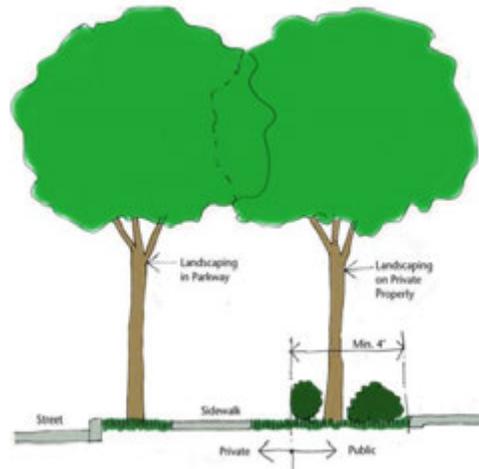
5. Ideal Condition for Landscaping on Private Property

Figure 5: Existing Conditions

When any of the following conditions exist on a property, i.e. if:

1. There are materials within the parkway that are not grass
2. There are limited number of trees in the parkway
3. There are no trees in the parkway at all, and when there are no physical elements separating the adjoining parking lots from the sidewalk

Figure 5A: Recommended Guidelines



1. Require parkway trees
2. On private property, require a landscape buffer no less than 4 feet wide for trees and shrubs. The double row of trees – one in the parkway, the other on private property – should be staggered.

(The Village of Lincolnwood already has examples of this type of streetscape along Devon Avenue near the intersection with Crawford Avenue).

2. GUIDELINES FOR PRIVATE DOMAIN

Covering the private property, including: Placement of buildings; Height of Buildings; Relation of building to street/sidewalk; Relation of building to buildings along one side of the street; Relation of buildings to buildings across the street; Treatment of building entryways; Relation of entryways of one building to the next; Treatment of building bulk; Treatment of special gathering places outside the building

The design guidelines for private domain are described in general terms and not on section-specific or site-specific basis. They are differentiated by categories of use and are based on the design preferences of the Village citizens and LATF members. The three categories of private domain guidelines are:

- A. All Buildings
- B. Residential Buildings
- C. Commercial and Mixed-use Buildings

A. Design Guidelines for All Buildings

1. ***Build-to Line for Placement of Commercial and Mixed-Use Buildings, and Residential Buildings**** : The new commercial and mixed-use buildings in the Lincoln Avenue corridor should be located close to the front of property, at 5 feet from the right-of-way line, with parking concealed from view from Lincoln Avenue and located behind buildings. Residential buildings that do not provide fences in the front yard should also be located at 5 feet from the ROW line. (See Figure 6)

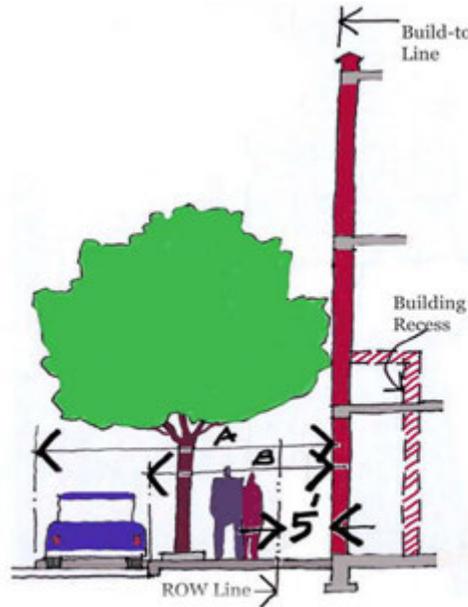


Figure 6: Illustrative Guidelines for Redevelopment of Properties

Maximum Distance from R.O.W to "Build-to" Line:	5 feet
'A': Maximum Distance from Exterior Wall to Street Curb, Including Parking Bay:	23-24 feet
'B': Maximum Distance from Exterior Wall to Street Curb, Excluding Parking Bay:	15 feet

* Applies to only Residential Buildings without fences in the front yard. For residential buildings with fences in the front yard, see guidelines for residential buildings

2. **Building Relationship to Street:** The relationship of new buildings (all commercial and mixed-use and some residential*) to the street and other street amenities is illustrated in Figure 7. The current right-of-way of Lincoln Avenue is 100 feet and the road pavement is 80 feet. The guidelines call for creating “bump-outs” from the existing sidewalk line of up to 8 feet or 9 feet to enclose parking bays within the sidewalk system. Recommended trees in the corridor will be installed at 25’ center-to-center. Buildings will be located at the “Build-to” line 5 feet from the Lincoln Avenue right-of-way. Touhy Avenue has the same right-of-way and pavement widths as Lincoln Avenue. The proposed guidelines will also apply for redevelopment of properties along Touhy Avenue within the recommended mixed-use/downtown hub.

(*Only for residential buildings without fences in the front yard)

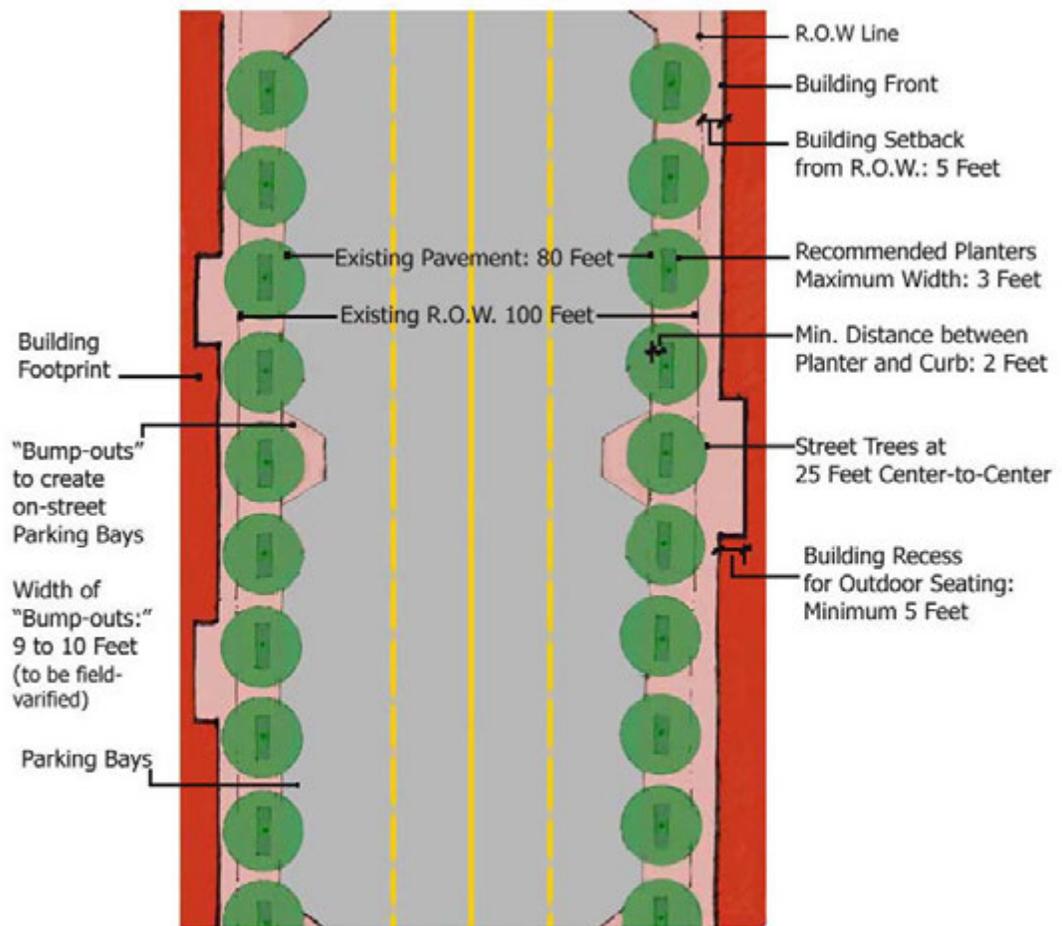


Figure 7: Lincoln Avenue Corridor Redevelopment Guidelines
 (Note: These guidelines also apply to Touhy Avenue Redevelopment, in proximity to Lincoln Avenue)

3. **Building Heights:** No building in Lincoln Avenue Corridor should have more than three floors, except in the recommended mixed-use/downtown hub where the buildings may be five stories tall. As shown in Figure 8, portions of building higher than three stories, should be recessed back a minimum of 10 feet.

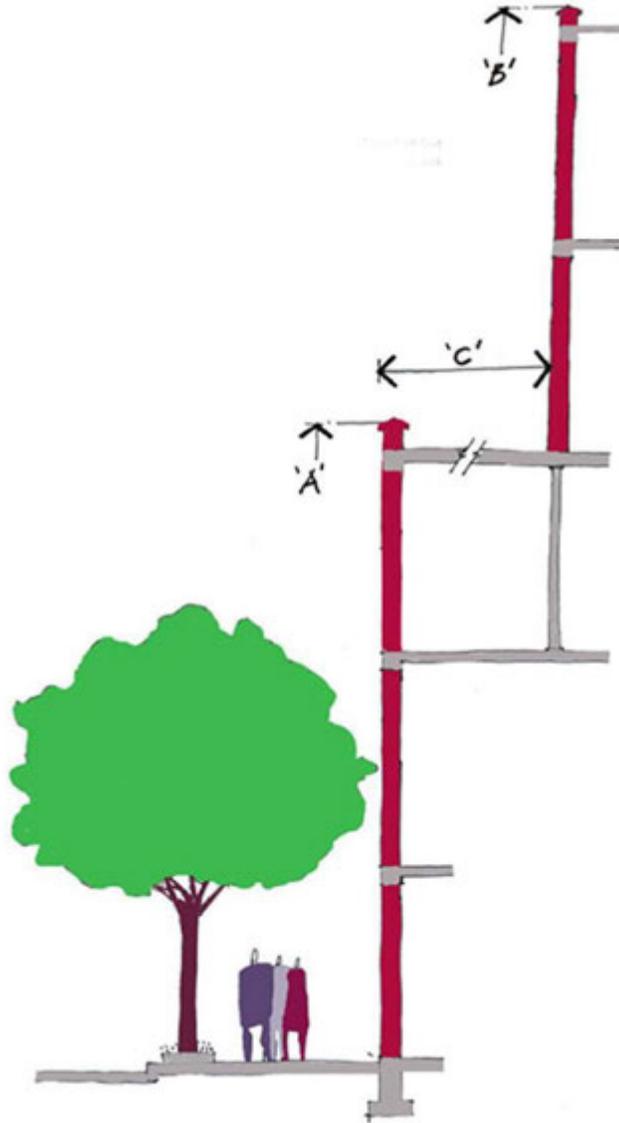


Figure 8: Illustrative Guidelines for Buildings Higher than Three Floors

- | | |
|--|------------------|
| 'A': Maximum Allowable Height Immediately Adjacent to Sidewalks: | 3 Floors/38 feet |
| (Measured to the top of eave from the top of the sidewalk) | |
| 'B': Maximum Allowable Height for Buildings: | 5 Floors/65 feet |
| (Measured to the top of the eave from the top of the sidewalk) | |
| 'C': Minimum Setback for Floors Above Three Floors: | 10 feet |

4. **Building Orientation and Elevation:** New buildings in the corridor should be located parallel to the street frontage. The mass of buildings should be broken up in a rhythmic fashion – well articulated -- so as not to create plain building facades. Balconies and other such elements may be used to create variety and interest and yet remain integral to the overall design of the building (See Image 1)



Image 1: An example of building placed parallel to Lincoln Avenue, with a well-articulated building elevation

5. **Angled Building Facades to Streets:** On an exceptional basis, provided there is a strong reason to do so, the Village may approve buildings with angled façade toward Lincoln Avenue. Image 1 is an example of angled buildings that may be considered acceptable on exceptional basis.



Image 2: An example of angled buildings facing a major street that may be acceptable

6. **Building Corner Towers:** Buildings located at a major intersection should have a tower feature at the corner that may emphasize key entrance to the building. These

corner towers should be in harmony with the building's architecture and will be allowed to exceed the maximum permitted building height so as to create the distinct appearance. (See Image 3 for illustration)



Image 3: An example of "Corner Tower" integral to the overall design of building

7. **On-street Parking:** For new buildings facing Lincoln Avenue there should be on-street parking as part of a sidewalk system that includes "bump-outs" from the existing sidewalk line. All required parking for a use should be provided behind the buildings. (See Figure 7 for parking "bump-outs.")
8. **Building to Building Relation Across or Along Lincoln Avenue:** Buildings located next to each other along the corridor, and buildings located across the street, should have a level of design consistency through either the use of similar materials and colors or distinct architectural elements such as cornices, dormers, balconies, brackets and pendants. It is not necessary for buildings to have the same architectural style to achieve design consistency, although it can help.
9. **Impacts on Adjoining Residential Uses:** Whenever the rear parking impacts the uses on adjoining properties, specific landscape buffers, fences or short walls, or a combination thereof, should be installed by the new building owners
10. **"Dead" Walls Along Streets:** While attractive landscaping can enhance the quality of streetscape, buildings with "dead" or blank walls along the sidewalks should be avoided (or minimized). Residential or other buildings with units above a terrace and parking below that create blank walls along the street should be avoided. (See Images 4 & 5 for comparison.)



Image 4: Example of attractive landscaping that enhances streetscape



Image 5 shows the same streetscape (as in Image 4) with buildings that create blank walls along a sidewalk. This is an example of building relation to street that should be discouraged.

B. Additional Design Guidelines for Residential Buildings

- 1. **Placement of Residential Buildings with Fences in Front Yard:** Residential buildings with fences in front yard should not be placed more than 10 feet from the right-of-way line. In such cases, the fence should be placed at 5 feet from the right-of-way line, with a hedgerow behind the fence toward the building side. The fences in the front yards should be wrought iron that are no more than 4' high. (See Image 6)



Image 6: An acceptable example of fence along residential buildings facing Lincoln Avenue

- 2. **Pedestrian Access to Residential Buildings:** Residential buildings should have direct access to units from the front street side, with strong entries to individual units emphasized by distinct architectural elements such as porticos. (See Image 7)



Image 7: Residential building along street with strong statement for entries to units

3. ***Vehicular Access to Residential Buildings from Lincoln Avenue:*** No residential building should be allowed to have garages facing Lincoln Avenue or driveways with direct vehicular access from Lincoln Avenue.
4. ***Garages in Residential Buildings:*** For residential buildings, the fronts of units at the rear of the property should not face the garages of the units in the building in front. (See Image 8 that is an example of what should not be allowed.)



Image 8 shows residential units facing garages. This should not be allowed in the corridor.

5. ***Pocket Parks in Residential Projects:*** For new residential projects on properties with shallow depth, pocket parks between buildings should be provided to create visual interest and on-site recreational opportunities. (See Image 9)



Image 9: Pocket parks for properties with shallow depth

C. Additional Design Guidelines for Commercial and Mixed-Use Buildings

1. **Outdoor Seating:** For commercial buildings that allow outdoor seating for their patrons, the portion of buildings that houses cafes, restaurants and other such uses should be set back 5' from the normal build-to line. (See Image 10)



Image 10: Additional building set back from build-to line to allow outdoor eating and other activities

2. **Minimum Height for Building Setbacks for Outdoor Seating:** For the outdoor seating areas created by the additional setback from the build-to line, the minimum height for “recesses” should be 10 feet. (See Figures 6 & 7 for building “recesses”)



MEMORANDUM

TO: Chairman Eisterhold
Members of the Plan Commission

FROM: Aaron N. Cook, AICP
Development Manager

DATE: October 18, 2013

SUBJECT: **Comprehensive Plan Amendment – Review of Lincoln Avenue Task Force Report**

Note: Below is the summary provided at the September 11, 2103 Plan Commission meeting. This matter was continued in order to invite members of the Lincoln Avenue Task Force to a Plan Commission meeting to participate in a discussion/review of the LATF Report. Several members of the Task Force are anticipated to be present at the October 23rd hearing.

At the Committee of the Whole (COTW) meeting of July 16, 2013, the Committee discussed multi-family housing in the Lincoln Avenue corridor. The Zoning Code regulations for housing along Lincoln Avenue have their origins in the recommendation and vision adopted as part of the Lincoln Avenue Task Force (LATF) Report. Certain other recommendations of the LATF Report such as land use, design requirements, parking requirements, etc. have been incorporated into the Zoning Code. The LATF Report was adopted and made part of the Comprehensive Plan in May 2006.

At the July 16th COTW meeting, discussion included a possible prohibition on residential developments along Lincoln Avenue which is not consistent with the Zoning Code or the Comprehensive Plan. Trustee Cope indicated his concern over residential developments along Lincoln Avenue include excessive building height, increased traffic generation, and decreased property values. Rather than focus on the housing recommendations only, several trustees recommended a full review of the LATF Report. At the conclusion of the July 16th discussion, the COTW directed the Plan Commission to hold a public hearing to fully review the Lincoln Avenue Task Force Report. As part of this referral, the Village Board requests consideration by the Plan Commission to place a moratorium on residential development along the Lincoln Avenue corridor while the LATF Report is under review.

At the September 11th Plan Commission meeting the members should be prepared to 1) recommend to the Village Board on adopting a moratorium on residential development along the Lincoln Avenue corridor and 2) discuss the LATF Report beginning with the recommendations relative to residential development but also the report as a whole. Upon completion of the review of the LATF Report, if recommendations are made to change the findings of the Report, it is likely that the text of the Zoning Code will need to be amended to ensure consistency between the Comprehensive Plan and the Zoning Code.

Attachments:

1. July 16, 2013 Committee of the Whole Minutes
2. Ordinance No. 2006-2710: An Ordinance Amendment the Village of Lincolnwood's Comprehensive Plan to Incorporate the Report and Recommendations of the Lincoln Avenue Task Force

indicated that NCG does not object to expanding the TIF District. Mr. Elrod informed the Village Board that his staff would prepare a Resolution for consideration at the August 20 Village Board meeting which would confirm that NCG is paying for the study and authorizing staff to proceed.

3. Discussion Concerning the Purple Hotel Site Restoration Standards

The item was presented by Mr. Cook who indicated that the Pre-Development agreement between NCG and the Village for the Purple Hotel expired on July 1, 2013. NCG is requesting reinstatement of the agreement plus additional items. Staff is recommending that NCG be required to submit for land-use relief no later than December 1, 2013, receive land-use approval no later than July 1, 2014, and be permitted to extend site restoration waivers for the adjoining properties until July 1, 2014. Mr. Firsell reported that all asbestos had been abated from the Purple Hotel and the demolition contractor is ready to begin demolishing the building. Mr. Stein indicated that the only items which are pending are the pre-development agreement and the Illinois Department of Transportation's approval. Discussion ensued. The Village Board directed that the subcommittee of Trustees Elster and Patel convene a meeting to discuss the terms of the new pre-development agreement

4. Discussion Concerning the Design Standards for Non-Residential Structures

The item was presented by Mr. Cook. On April 16, 2013, the Village Board considered a recommendation by the Plan Commission concerning proposed changes to the Village's design standards for non-residential buildings. These design standards are commonly referred to as the Village's masonry requirements. In considering this matter at the April 16, 2013 meeting, the Village Board referred this item to the Committee on Ordinances, Rules & Buildings (CORB) for further deliberation. The Plan Commission recommendation on this matter included the following:

1. Eliminating "Masonry Only" requirement from the Lincoln Avenue Overlay District
2. Establish that the exterior walls of non-residential buildings be constructed with a minimum of 75% "high quality materials". This includes a requirement that from the ground level to the top of the first floor windows must be constructed entirely of "high quality materials"
3. Establish a list of permitted "high quality materials" which includes:
 - 1) Brick with a minimum thickness of 2.25 inches
 - 2) Natural stone
 - 3) Sandstone
 - 4) Other native stone
 - 5) Pre-cast decorative stone
 - 6) Glass
4. Add definitions for Brick, Decorative Concrete Block, Masonry, and Stone; and
5. Add *Exterior Insulation Finishing Systems ("EIFS")* and *metal and vinyl lap board siding* as specifically prohibited materials.

CORB considered this matter at its June 11, 2013 meeting and now seeks to have the COTW consider its recommendations on this matter. Discussion ensued. The Village Board referred CORB's recommendation to the Plan Commission for their consideration.

5. Discussion Concerning Multi-Family Housing Zoning in the Lincoln Avenue Corridor

The item was introduced by Mr. Wiberg who indicated that Trustee Cope requested the discussion of multi-family on Lincoln Avenue be discussed at the Committee of the Whole. Mr. Cook indicated that zoning for multi-family housing was the result of the Lincoln Avenue Task Force Plan and provided further background on the matter. Trustee Cope indicated his opposition to allowing the condo development on 6535 North Lincoln Avenue with concerns relating to height, traffic, and affecting other area property values. He proposed prohibiting such developments from Lincoln Avenue in the future and for the Village to place a moratorium in the immediate term. Trustee Patel suggested that the Plan Commission review the Lincoln Avenue Task Force Plan in its entirety. Mr. Cook recommended that the Comprehensive Plan be reviewed first. The Village Board concurred. The matter of reviewing the Plan and placing a moratorium on multi-family housing on Lincoln Avenue was referred to the Plan Commission

Adjournment

VILLAGE OF LINCOLNWOOD

ORDINANCE NO. 2006 2710

**AN ORDINANCE AMENDING THE VILLAGE OF LINCOLNWOOD'S COMPREHENSIVE
PLAN TO INCORPORATE THE REPORT AND RECOMMENDATIONS OF THE LINCOLN
AVENUE TASK FORCE**

ADOPTED BY THE
PRESIDENT AND BOARD OF TRUSTEES
OF THE VILLAGE OF LINCOLNWOOD
THIS 18TH DAY OF MAY, 2006

ORDINANCE NO. 2006-2710

AN ORDINANCE AMENDING THE VILLAGE OF LINCOLNWOOD'S COMPREHENSIVE PLAN TO INCORPORATE THE REPORT AND RECOMMENDATIONS OF THE LINCOLN AVENUE TASK FORCE

WHEREAS, the Village of Lincolnwood (the "Village") is a home rule municipality in accordance with Article VII Section 6(a) of the Constitution of the State of Illinois of 1970;

WHEREAS, the Village has the authority to adopt ordinances and to promulgate rules and regulations that pertain to its government and affairs;

WHEREAS, on November 1, 2001, the Village Board adopted the Village of Lincolnwood Comprehensive Plan (the "Comprehensive Plan");

WHEREAS, the Comprehensive Plan includes, but is not limited to, a policy guide for future Village Board decisions concerning land use within the Village;

WHEREAS, subsequent to the adoption of the Comprehensive Plan, the Village created the Lincoln Avenue Task Force (the "Task Force") to propose a plan for retail redevelopment of properties along the Lincoln Avenue Corridor (the "Corridor") that would be attractive and image enhancing, providing residents with a safe, convenient and pedestrian-friendly environment;

WHEREAS, in November, 2005, the Task Force completed its report and recommendations, with exhibits (the "Report") for enhancement and redevelopment of the Corridor;

WHEREAS, on April 19, 2006, pursuant to a timely, legal notice of public hearing published in the *Lincolnwood Review*, the Task Force presented the Report to the Plan Commission for its review, consideration and recommendation to the Village Board regarding incorporating the Report into the Comprehensive Plan;

WHEREAS, at the April 19, 2006, public hearing all interested persons were allowed the opportunity to present their views regarding the Report, pro and con, to the Plan Commission;

WHEREAS, subsequent to the April 19, 2006, public hearing, the Plan Commission made a recommendation to the Village Board to incorporate the Report into the Comprehensive Plan;

WHEREAS, the Report is attached hereto and incorporated herein by reference as Exhibit A;

WHEREAS, the Village Board reviewed the Report and recommendation of the Plan Commission and determined that it is in the public interest and will promote the public health, safety and welfare of the Village and its residents if the Report is incorporated into the Comprehensive Plan.

NOW, THEREFORE, BE IT ORDAINED by the President and the Trustees of the Village Board as follows:

Section 1. The facts and statements contained in the preamble to this Ordinance are found to be true and correct and are hereby adopted as part of this Ordinance.

Section 2. The Report attached hereto as Exhibit A is hereby incorporated into and made a part of the Comprehensive Plan as Appendix G.

Section 3. In the event there are any inconsistencies between the Comprehensive Plan and the Report, the Report shall control.

Section 4. In all other respects, the Comprehensive Plan shall remain the same.

Section 5. If any section, paragraph or provision of this Ordinance shall be held to be invalid or unenforceable for any reason, the invalidity or unenforceability of such section, paragraph or provision shall not affect any of the remaining provisions of this Ordinance.

Section 6. This Ordinance shall be in full force and effect from and after its passage and approval according to law.

PASSED this 18th day of May, 2006.

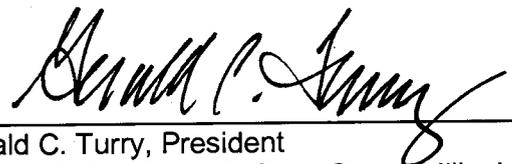
AYES: Trustees Froman, Heidtke, Lebovits, Elster, Sprogis-Marohn and Leftakes

NAYS: None

ABSENT: None

ABSTENTION: None

APPROVED by me this 18th day of May, 2006.



Gerald C. Turry, President
Village of Lincolnwood, Cook County, Illinois

ATTESTED and FILED in my office the
18th day of May, 2006.


Beryl Herman, Village Clerk
Village of Lincolnwood, Cook County, Illinois

EXHIBIT A

Copy of the Lincoln Avenue Corridor Report and Recommendations
of the Lincoln Avenue Task Force

(See Attached)

Lincoln Avenue Corridor



Report and Recommendations of the Lincoln Avenue Task Force

**Village of Lincolnwood
November 2005**

Lincoln Avenue Task Force Members

James V. Persino, Chairman

Member and Chair, Lincolnwood Economic Development Commission

Mary Couzin

Member, Lincolnwood Plan Commission and past Village Trustee

Maureen Ehrenberg

Member, Lincolnwood Economic Development Commission

Lawrence Elster

Village Trustee

Kenneth Klint

Member, Lincolnwood Plan Commission

Renee Silberman

Resident at -large

Renee Sprogis-Marohn

Village Trustee

Staff Assistance

Timothy M. Clarke, AICP
Community Development Director

Consultation Provided

Walter J. Magdziarz, AICP
Mahender Vasandani
Land Vision, Inc.

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Executive Summary

We present to you a bold and dynamic plan for Lincoln Avenue. When fully implemented, we believe this plan will result in an attractive corridor and desirable main street for our community. This report is intended to set out the necessary public actions and to guide private reinvestment and redevelopment in order to achieve the dynamic vision we have for the Corridor.

In formulating our recommendations, the Lincoln Avenue Task Force studied the redevelopment and revitalization efforts of a number of communities, including Park Ridge, Deerfield, Mount Prospect and Glenview. While our recommendations embody a bold and dynamic vision for the future of Lincoln Avenue, one which incorporates cutting-edge planning principles, these measures we recommend have been tested and proven successful elsewhere.

The central focus guiding our work has been to foster redevelopment of properties along the corridor which are both attractive and image enhancing, creating retail districts which provide residents with a safe, convenient, attractive and pedestrian friendly environment.

First, our plan calls for a dramatically different land use pattern for the corridor and we recommend significant land use changes to Village code. Specifically, we recommend creating three business districts along the Lincoln Avenue Corridor: one at Devon Avenue, another at Pratt and Crawford Avenues, and a third at Touhy Avenue. Pedestrian oriented retail is called for as the primary ground-floor use in these areas. We envision businesses such as bookstores, galleries, bistros, restaurants, coffee and ice cream shops, boutiques and other specialty retail stores enlivening these business nodes. Our plan also calls for mixed-use development in these business nodes, with upper floors of residential or office uses, to foster redevelopment and to encourage a lively, pedestrian-friendly atmosphere.

At Touhy Avenue, our plan envisions creation of a downtown center for our community, having ground floor retail, community gathering spaces and such anchor uses as a boutique hotel, sit-down restaurants, or entertainment venues. On upper floors, as well as between the three business nodes, our plan calls for introduction of residential development in the corridor.

Second, our plan calls for enhancements to the public right-of-way to improve public safety, create ambiance, and foster a strong sense of place. We recommend designating a parking lane on Lincoln Avenue; improving pedestrian crosswalks; and continuing efforts to restore parkways and to complete the median landscaping project. Most important, we call for a specific streetscape plan to be developed, which would include new streetlights and identifying street furniture and other urban design amenities and elements for the corridor. A bold but longer range component of our plan calls for creating new open spaces and focal points, such as towers or monuments in the corridor at key locations.

Third, we believe that the shape and form of new development in the corridor is critical. Therefore, our plan embraces neo-traditional planning and new urbanism concepts for new development along Lincoln Avenue. In this regard, we call for a set of new development regulations which will regulate the shape and the form of new buildings and private development in the corridor. These “form-based” recommendations for instance call for new buildings along Lincoln Avenue to be parallel to and positioned close to the street, with new off street parking located toward the rear. We recommend additional form based regulations in our report and to assist developers in meeting our vision, we also call for the creation of an illustrated architectural design handbook to guide new building development in the corridor.

Our vision for Lincoln Avenue is exciting and our work has set out many recommendations, both broad and specific. Execution will require a number of actions in order for our vision to become a reality. With Village government dedicated to the vision we present, we believe the Lincoln Avenue Corridor can truly become a main street for our community.

Report and Recommendations for a New Lincoln Avenue Corridor

Introduction

Background of Task Force

The Lincoln Avenue Task Force is an outgrowth of discussions first held in 2002 by the Village's Economic Development Commission. These discussions quickly expanded to include the Village's Plan Commission and a joint meeting of these bodies was held in the fall of 2002 to discuss Lincoln Avenue and its future. The result was a recommendation that a task force be established to consider the challenges, assets and possibilities for the Lincoln Avenue Corridor. President Peter Moy, with consent of the Village Board, subsequently established the Lincoln Avenue Task Force to undertake this work.

The seven-member appointed Task Force is comprised of two members from each of the following bodies: the Village Board, the Plan Commission, and the Economic Development Commission, with one citizen-at-large member completing the roster. Primary staff assistance to the Task Force has been provided by the Community Development Director, with technical and field support provided by key members of the Building Department. The work of the Task Force has been further assisted with an engagement of Land Vision Inc., an urban planning and design firm.

In performing its charge, the Task Force examined many elements. These ranged from conducting a windshield survey of current conditions along Lincoln Avenue; studying the redevelopment efforts of other communities such as Glenview, Deerfield, Park Ridge and Mount Prospect; seeking community input through an Image Preference Survey and conducting and soliciting additional public input in a Town Meeting setting.

The work of the Task Force has resulted in a bold and dynamic plan which will transform the Lincoln Avenue Corridor into a valuable asset for the community.

History of Corridor

Little is written about the origins of Lincoln Avenue. It is known that Lincoln Avenue began as one of the early roads linking Chicago northward to the rural farm areas of Illinois and beyond. Some of the very first businesses in the Village opened along this road, some reportedly to be out of the reach of city restrictions. As Chicago's population expanded outward and traffic increased, so did use of Lincoln Avenue and over time it became a major artery into and out of the City.

Although records are limited, most initial development along Lincoln Avenue in the Village appears to have occurred between the end of World War II and the mid 1960's. With the opening of the Edens Expressway in the 1950's however, Lincoln Avenue's role as a major thoroughfare declined. Just south of the Village, a string of 1950's-1960's era motels still can be found, testament to Lincoln Avenue's history as a major artery before the advent of the Edens.

Existing Development Pattern

The existing development pattern along Lincoln Avenue can be characterized as an auto dependent form of commercial strip. Much of the initial development of this strip occurred at a time when the automobile was beginning to exert a dominant force on land use in America. Unlike areas of Chicago, Evanston, or rail towns like Park Ridge, Lincolnwood and Lincoln Avenue developed without street cars, rail stations, mass transit or the influence of great numbers of pedestrians. With the lack of any significant mass transit, specific business hubs, or a semblance of a central gathering location, the corridor became a long, nearly continuous stretch of commercial development, dependent mainly on access by automobile.

This development pattern occurred prior to the now common phenomenon of multi-car families. Traffic and parking demands in the corridor at the time of its initial development were much less than they are today. Developments during this period often had only a limited number of off street parking spaces, generally arranged along the front of the building and sometimes evolving into a continuous curb cut onto Lincoln Avenue.

Although the Lincoln Avenue Corridor is nearly a continuous strip of businesses from Devon Avenue to Jarvis Avenue, the character of the street changes significantly, albeit gradually, from south to north. At its south end, near the border with Chicago, land uses along Lincoln Avenue tend to be more varied, including auto repair shops, older strip retail centers with continuous curb cuts, and a storage facility. Developments along this stretch of Lincoln Avenue tend to be older, more functionally obsolete in nature and contain more marginal and less intensive uses than areas north. The middle section of Lincoln Avenue, roughly from the rail crossing near Hamlin Avenue to Touhy Avenue, contains a mix of uses, including more current and intense development. This area has multi-story office and bank buildings, automobile dealerships, restaurants, renovated strip centers and community institutional uses such as Village Hall, the Library and Proesel Park. North of Touhy Avenue, to the Village's boundary with Skokie, Lincoln Avenue is primarily comprised of office buildings.

The Task Force found that, except for the office corridor north of Touhy Avenue, the existing development pattern on Lincoln Avenue was aging, many uses were marginal, and the corridor was not functioning as a vibrant main street of the community.

Challenges and Assets of Corridor

Lincoln Avenue presents a number of challenges and assets to its revitalization. First, Lincoln Avenue runs through the Village on a diagonal to the overall grid system of

streets. This results in a number of odd or irregularly shaped parcels at various corners which can inhibit typical development. Moreover, many of the parcels fronting along Lincoln have rather short depths, limiting to a degree their usefulness and redevelopment potential. This is compounded by the close proximity of single family homes to the corridor, some of which utilize shared alleyways with Lincoln Avenue properties and others which have no alley whatsoever.

Much of the existing development along the street was developed without adequate barriers or buffering measures with residential areas. This lack of buffering combined with short parcel depths and irregular parcel configurations can and often has produced land use conflicts. Recently however, the Village has strengthened its zoning code to require landscape buffering of business properties from residential areas. Where space is too constrained for landscape buffers, barrier walls are now required for new development. The implementation of these new standards for buffering should be continued and should help reduce land use conflicts within the corridor.

Lincoln Avenue itself is a relatively wide right-of-way corridor, averaging between 90 – 100 feet in width throughout its length in the Village. Existing conditions in the right-of-way, coupled with its physical width, currently inhibit safe pedestrian crossings and help aid in the dominance of the auto in the corridor. The conditions and width of this right-of-way, currently an impediment to a pedestrian friendly environment, can however be smartly designed to create a unique sense of place and a friendly pedestrian environment. With the creation of a designated parking lane for instance and a thoughtful streetscape design, this wide right-of-way can be utilized and tapped as an asset in the redevelopment of the corridor.

Another untapped asset of the corridor is the immediate market. Lincolnwood is an affluent community and with Lincoln Avenue running through its center, an attractive local market exists. Nonetheless, many marginal uses currently exist in the corridor. With changes to the Village's zoning code to encourage, for instance, restaurants and higher end retailers to locate in the corridor, this local market can be tapped to both serve the community and assist in the corridor's redevelopment.

A further asset which the Village can play upon is the recent and ongoing redevelopment and revitalization efforts occurring immediately south of the Village, along the corridor in the City of Chicago. Recently, a number of existing developments, including the Lincoln Village Shopping Center as well as the conversion of a Kmart store into a Home Depot, have resulted in significant revitalization. Moreover, the City of Chicago has recently acquired a few of the older motel properties, converting these to parks or other uses. And south of Peterson Avenue, new housing in the corridor has recently been constructed.

Vision for the New **Lincoln Avenue Corridor**

We envision the Lincoln Avenue corridor as a vibrant Main Street through our Village. We see it not only as the centerpiece and spine of the community; it is also the entryway into the many residential neighborhoods of the Village. Major Village institutions, such as Village government, the public library, community center and local schools, are located along the street or very nearby. Lincoln Avenue is central to life in the Village.

We envision that as Lincoln Avenue enters the Village from Chicago, the character of the street changes dramatically, being readily apparent to the passerby that they have left behind the big city and have entered a high quality suburb. A landscaped median, new street lights, attractive signs, public sculpture and pedestrian friendly features including easy to use crosswalks and a marked parking lane, are evident and add to the sense that Lincoln Avenue is the Main Street of an upscale community.

Instead of a long continuous commercial strip as presently exists, hubs of activity have been created along Lincoln Avenue, primarily located at major intersections. These activity hubs include ground level retail in high quality, architecturally significant buildings with adequate customer parking, tasteful business signs and attractive landscaping. Comprised of restaurants, ice cream and coffee shops and boutique retail stores, these areas serve the immediate neighboring residential areas and the community as a whole and may include second or third story residential units.

Between these hubs of retail dotting Lincoln Avenue, new housing has been added to already existing bank buildings and office complexes, to replace tired strip centers, auto garages, storage facilities or other obsolete buildings and uses. We envision this in-fill housing as no higher than three stories, with appropriate green space and comprised of attractive townhouse, 3-flat, or small condominium buildings and developments. We see this new low density in-fill housing as providing on-the-one-hand, an attractive entryway into the adjoining single family residential areas, and on-the-other hand, a buffering and transitional use from the business areas and Lincoln Avenue itself to the nearby single family neighborhoods.

While Lincoln Avenue is still a major thoroughfare through the community, through much work, it has been transformed to be more pedestrian friendly and less auto dominated. Instead of a rapid auto path akin to an expressway and a barrier separating Lincolnwood's neighborhoods, the new Lincoln Avenue links neighborhoods together, bringing the Village together at its major institutions and becoming the main street of our community.

Recommendations

The Task Force's recommendations below are divided into two major sections. The first section, Public Streets and Spaces, recommends actions and improvements in the public right-of-way which can help to create a pedestrian-friendly environment and foster a real sense of place. In this section, improved green space, wider sidewalks, new street lights, various urban design elements, as well as slower traffic speeds and eye-catching focal points are recommended to enhance the streetscape and invite residents and shoppers to the corridor. These are improvements which can be undertaken in the public right of way, by the Village in concert with the Illinois Department of Transportation (IDOT).

The second major section of our recommendations, Development Regulations and Guidelines, relates to controls on private property and new development in the corridor. The appearance, location and mix of businesses and residences along the corridor shape perceptions and use. To achieve our vision, this section recommends changes in the Village's existing land use regulations, creation of new form-based regulations and creation of design guidelines which will not only encourage private reinvestment, but will aid in shaping it. Together and in tandem, we believe these set of recommendations will help the Village create a new sense of place and achieve the vision we have for the Corridor.

I. Public Streets and Spaces

Creating an attractive, vibrant and pedestrian-oriented environment in the public right-of-way is an essential element of the Lincoln Avenue Corridor Plan. This plan reinforces many of the Village's ongoing and recently begun initiatives, such as median landscaping and parkway restoration. It also recommends a number of new initiatives, such as creating a designated parking lane and the creation of a coordinated streetscape, including installation of new street lights and street furniture. Long term, the plan calls for creating three public plazas and/or visual focal points along the corridor.

While the plan emphasizes pedestrian orientation and creating pedestrian spaces, it recognizes that Lincoln Avenue is and will remain a major and important thoroughfare in the community. By undertaking attractive pedestrian enhancements, along with the other recommendations of this plan, Lincoln Avenue can become the main street for the Village. Since it is a state controlled street, coordination with the Illinois Department of Transportation (IDOT) is essential and will be required to undertake and implement these recommendations to the right-of-way.

The following are specific recommendations regarding public streets and spaces.

- **Complete Median Landscaping**

In the late 1990's, the Village initiated landscape improvements for the medians of Lincoln Avenue. This multi-year project is continuing. Median landscaping enhances the attractiveness of the corridor and creates positive perceptions of the corridor and community.

Recommendation: Continue and complete median landscaping throughout the corridor.

- **Continue Parkway Restoration**

In 2002 the Village created standards for parkways which include the placement of trees every 25 feet in the parkway and a pervious surface of sod or vegetative ground cover. Until these standards were instituted, parkways in the Village's business areas, including Lincoln Avenue, were essentially allowed to be treated by the adjoining owner as they so desired, providing no cohesion in the corridor and producing a visually cluttered, haphazard, disjointed and unkempt appearance. Moreover, in some sections, parkway areas have been essentially confiscated and turned into long continuous driveway curb cuts that directly access off-street private parking spaces, creating large stretches of impervious surfaces devoid of street trees. This situation conflicts with pedestrian use of public sidewalks and severely affects pedestrian safety. The restoration of parkways and public sidewalks, consistent with the recently established parkway standards, is an essential element toward improving the corridor. Parkway restoration should be diligently pursued, undertaken and implemented whenever possible.

Recommendation: Diligently pursue parkway restoration whenever possible along Lincoln Avenue, including tree planting and sidewalk restoration.

- **Improve Pedestrian Crosswalks**

Currently, many of the marked crosswalks across Lincoln Avenue do not provide the pedestrian with the shortest possible crossing distance for safety. Pedestrians are in the path of traffic for longer periods of time than necessary. To enhance the pedestrian environment and safety, conscious efforts should be undertaken to create highly visible marked crosswalks utilizing the shortest distance possible. The creation of intersection bump outs (described below) will further aid in reducing crosswalk distances, improving public safety and encouraging pedestrian use.

Recommendation: Enhance crosswalk markings and designations, including signage.

- **Create a Designated Parking Lane**

Currently, parking is generally permitted along the entire length of the corridor, although it is not well marked as such. Only in a few concentrated areas, such as in the vicinity of Air Room, is street parking now commonplace. Creation of designated parking lanes on both sides of Lincoln Avenue would help slow traffic and support active retail uses.

The width of the Lincoln Avenue street corridor currently allows for creation of this parking lane. Initially, such a parking lane could be achieved relatively inexpensively through painted striping and signage. A more long term, lasting and beneficial

improvement is to create curbed “bump outs” at intersections. Such bump-out areas essentially bring the sidewalk/pedestrian area of the public right-of-way out into the parking lane, approximately 8-9 feet toward the traffic lanes. Bump outs increase public safety and enhance the pedestrian environment. Specifically, bump outs at intersections: 1) protect and better define the beginning and end of the parking lane between intersections; and, 2) physically shorten the distance for pedestrians crossing Lincoln Avenue. In some areas, additional mid-block bump outs might be desirable to visually break up long parking lane stretches. Moreover, certain urban design features could be added to these new pedestrian spaces. Items such as decorative brick pavers, lighting, benches, bicycle racks, additional landscaping, newspaper vending machines, or even public art could be incorporated into these spaces.

Recommendation: Create a designated, marked parking lane with pedestrian bump outs.

• Reduce Traffic Speed

High speed traffic is not conducive to either a pedestrian environment, or to residential or retail uses. Physical changes such as instituting a parking lane with bump outs, in and of itself, will likely reduce speed to some extent, both because driving lane widths will be perceived to be narrower and because of parking movement maneuvers slowing the speed of traffic.

Although no formal traffic study was commissioned as part of this work, traffic observations in the corridor suggest that much of existing Lincoln Avenue traffic, particularly during rush hour, is essentially “cut-through” traffic generated by Chicago residents seeking a quicker way to the Edens Expressway. This is suggested by two left turn lanes on Lincoln for westbound Touhy Avenue and two left turn lanes on Lincoln in Chicago for eastbound Peterson Avenue. Cut through traffic, particularly related to rush hour periods and home-to-work trips, typically consists of drivers intent on minimizing time. Slowing down Lincoln Avenue traffic in Lincolnwood would result in lengthened drive times for such drivers, deterring some traffic and making alternative routes more attractive.

By slowing down traffic, pedestrian safety is enhanced and greater visual observation by motorists is achieved. Businesses in the corridor will be more visible to the motorist and businesses will be better able to capitalize on impulse shopping and the customer base produced by the traffic volume. Therefore, slower traffic on Lincoln Avenue can help to contribute to the Village’s sales tax revenue.

The current posted traffic speed on Lincoln Avenue is 35 mph, although traffic appears to typically travel at speeds well in excess of the posted limit. Touhy Avenue west of Lincoln is posted at 30 mph, and a reduction in the posted speed limit on Lincoln Avenue to 30 mph should be seriously considered.

Recommendation: Take action to reduce the speed of traffic in the corridor, including physical street design changes and a reduction in the posted speed limit.

- **Reduce and Minimize Parkway Curb Cuts**

Throughout the corridor, particularly as new developments are reviewed, driveway curb cuts in the Parkway should be kept at a minimum and apron width limited to only that needed for vehicle ingress and egress. Where possible and particularly for residential development called for in this plan, driveway curb cuts should be located not on Lincoln Avenue itself, but on adjoining side streets. This would further improve pedestrian safety as well as the aesthetic appeal of the streetscape.

Recommendation: Reduce and minimize Parkway curb cuts along Lincoln and relocate as needed.

- **Create a Unified Corridor Streetscape Plan**

A specific streetscape plan should be created and implemented, specifying and delineating urban design elements that create an attractive, pedestrian scale and sense of place within the corridor. As envisioned, this streetscape plan would specify the style and identify the location of urban design and pedestrian friendly elements such as lighting, benches, bicycle racks, trash bins, and kiosks. This work could explore such other items as special signage, public art and sculptures, hanging flower containers and street banners. Use of brick pavers in sidewalks and crosswalks and specific designs for the recommended parking lane bump outs are other suggested elements that could be explored in development of this streetscape plan.

One critical component of the streetscape plan would be new streetlights for the entire corridor, to replace existing streetlights which are rapidly aging and appear to have exceeded their useful life. Our review suggests that new streetlights should not be installed in the medians as they are now, but along the sides of the street. However, this could be further investigated as the specific streetscape plan is developed. Creation of a specific streetscape plan would not only result in the identification of specific styles and designs for street furniture, sidewalks and light standards, but also would locate them in the corridor. The advantage to developing such a plan is twofold: it will help guide specific improvements in the public right-of-way at specific locations and it would allow the Village to seek Illinois Transportation Enhancement Program (ITEP) grants to undertake such improvements. We believe this is a critical component which should be budgeted within the next two fiscal years.

Recommendation: Create and implement a unified streetscape plan for the corridor.

- **Consider Vacating Portions of Intersecting Streets**

Lincoln Avenue cuts diagonally through Lincolnwood, resulting in many unusual lot configurations along the corridor. The angle of Lincoln Avenue, set against the Village's basic grid pattern of streets, also has produced a number of intersections in the corridor which vary from the typical 90 degree street intersection angle, some involving more than one intersecting street. Because such intersections are not standard or typical, they pose public safety concerns, particularly to those not familiar with the area. This situation also results in long stretches of pavement and active traffic for the pedestrian to confront.

Our plan suggests the vacation of portions of some of these intersecting side streets to eliminate such unusual intersections. Such street vacations also have the potential benefits of reducing traffic into adjoining residential neighborhoods as well as enlarging and improving the shape of development parcels in the corridor. Any vacation of a specific street, however, must be thoroughly examined as to its potential effects. This plan has not undertaken such an investigation and therefore is only suggestive in this regard.

Recommendation: Side street vacations should be explored and examined at appropriate locations.

• Create Public Space Focal Points

A more long term but bold component of this plan, is to consider creating new public space focal points at three locations in the corridor: 1) Lincoln/Touhy/Kostner; 2) Pratt/Lincoln/Crawford; and 3) Lincoln Avenue at Proesel Park.

The first two locations are triangular shaped sites surrounded on all sides by public streets. Converting these sites into public spaces would result in open vistas at these important intersections, producing natural focal points. Along with redevelopment of adjoining properties, these new public spaces would help create a unique sense of place. Based largely on results of an Image Preference Survey of residents, our plan suggests that in these spaces, tower-like monuments be constructed of similar design, potentially utilizing prairie style elements, to help bring continuity to the corridor. This is however only an initial suggestion. As the community moves forward with implementing this element of the plan, other focal point ideas could emerge or be incorporated into such designs. These spaces for instance could commemorate significant local or national events or persons, celebrate the multicultural diversity of the community, or become spaces for the remembrance of the holocaust or of war dead.

A third recommended focal point along the Lincoln Avenue corridor is at Proesel Park. Currently the park, a major asset of the community extends a relatively long distance along Lincoln Avenue. Our observation is that the current design of the park provides neither a welcoming presence nor much activity along the Lincoln Avenue corridor. Our plan suggests highlighting this community asset by creating a focal point on Lincoln Avenue at Proesel Park, similar to those proposed on the triangle parcels. Specifically, this suggestion is to widen and expand the current Lincoln Avenue median at this location and install a similar tower-like icon, visually unifying the three sites. Creating such an expanded median area would require moving and arcing the Lincoln Avenue traffic lanes somewhat, producing an oval shape. This would be another feature which would slow Lincoln Avenue traffic. This proposal obviously would require IDOT review and approval and likely would require some land acquisition.

Recommendation: Consider creating three public focal points at key corridor locations.

II. Development Regulations and Guidelines

In addition to improving the corridor through the appearance and use of public streets and spaces, the Task Force is recommending major changes to the Village's land use codes; the creation of new "form-based" regulations for new development; and, creation of design guidelines for new buildings. Below are the Task Force recommendations as they relate to private properties and their development in the Corridor.

• Modify Land Use Codes to Encourage New Uses

Land use in the corridor is a critical component to the plan. Except for that portion of Lincoln Avenue north of the Purple Hotel site, the plan recommends significant changes to the Village's current land use policies and zoning code. These changes, over time, will enhance and encourage a pedestrian environment for the corridor. Currently, the corridor is one long, mostly undistinguished strip of business properties. The plan proposes creation of three specific business activity hubs at key intersections, with the intersection of Lincoln and Touhy Avenues identified for higher density and activity, in an effort toward creating a downtown business district for the Village (described further below).

Currently, most of Lincoln Avenue is zoned B1 Restricted Business District. This zone permits office and bank facility developments. Retail is currently allowed only through special use designation. To achieve the vision for Lincoln Avenue, a significant change in land use, over time, is warranted. This plan calls for major changes in the zoning code regarding permitted and special uses. Under this plan, retail uses which cater to both Lincolnwood residents and the larger community should be encouraged to locate in designated business hubs. In such hubs, retail uses should either be exclusive uses on the ground level or predominant uses on the ground level to help achieve a pedestrian and retail oriented nexus. Book and drug stores, food specialty shops, coffee shops, sit-down restaurants, ice cream shops, dry cleaners, furniture, clothing and boutique stores should be encouraged and permitted to operate in such hub districts, subject only to complying with the community's development standards, buffering requirements with residential properties and the Village's off street parking requirements. Because office or bank uses generally do not contribute to cross-shopping, walk-in traffic, or a pedestrian environment, such uses in designated business hubs should be kept to a minimum or prohibited altogether on the ground level.

Similarly, specifically auto-oriented or auto-related businesses should be either prohibited altogether or kept to a minimum throughout the corridor. Automobile service and repair shops, oil change centers, gasoline stations, auto part stores and car washes do not contribute to either a pedestrian or shopping environment and should either be prohibited altogether, or highly limited. Where such uses may be authorized because of significant contributions to the tax base, such as automobile dealerships, careful attention to outdoor vehicle display and its effect on the streetscape is in order. Similarly, future drive-thru operations should be highly regulated, limited or prohibited altogether.

The plan presented for Lincoln Avenue embraces the concept of mixed use development, a term not currently found in the Village's Zoning Code. The plan envisions large development parcels, such as the Purple Hotel site, as including multiple land uses within its development. Individual buildings or smaller development parcels should also have

vertical multiple uses, namely retail on the ground floor and residential or office uses on upper floors.

In addition to permitting housing as part of certain mixed use developments, the plan also calls for the introduction of housing as an infill land use between the designated business hubs. Allowing housing as an in-fill land use will help demarcate the edges of the recommended business hubs and eliminate the current condition of a long, continuous strip of business properties. Moreover, development of in-fill housing between business hubs would visually extend the Village's residential neighborhoods into the corridor, thereby accentuating the residential persona of the community. In such in-fill areas, housing as a ground floor use should be permitted. Where housing is permitted, it is envisioned that residential uses in the corridor would be comprised of either multi-level townhouses or condominium buildings.

Recommendation: Modify the current land use code to allow for mixed use development and in-fill housing; and, to prohibit expansion of drive-thru operations and auto-oriented uses.

• Designate Business District Hubs

Related to the above changes being recommended, the Village should incorporate changes into its current zoning code to allow for development of the following proposed business hubs.

Lincoln-Touhy Business Hub

This business hub, centered on the intersection of Lincoln and Touhy Avenues, would include the Purple Hotel, Bank of Lincolnwood and Lincolnwood Produce sites, as well as nearby adjoining properties. This business hub is envisioned as the downtown for the community. Building height in this area is recommended to be limited to 5 floors, exceeding the 3 floor limitation recommended for the remainder of the corridor. Ground floor retail including restaurants, some with alfresco dining, would be a predominant ground floor use, with upper floors of residential and/or office space. Uses which encourage the public to gather, as well as strong or unique retail anchors, a boutique hotel or family entertainment venues, should be incorporated into specific parcel development plans. In addition, sculptures, fountains and plazas with outdoor seating should be designed into parcel redevelopments in this hub. It is envisioned that this business hub would have the highest development intensity in the corridor and potentially would require either underground parking, parking decks or a combination of both. Any parking structure should be well designed with public safety in mind and located so as to be unobtrusive to the primary use activities. For building sides located along Touhy Avenue, we suggest an exception to the recommended corridor build-to line of 5 feet. For such building sides, we recommend a build-to line of 15 feet or greater from the public right of way to account for high traffic volume on Touhy Avenue and pedestrian safety.

Lincoln-Pratt-Crawford Business Hub

This business hub is near the geographic center of the Village and a crossroads for Village residents. Functionally, this business area is expected to serve the more

immediate area and provide shopping convenience for residents. This area is recommended to include mixed use buildings having ground level retail, including such uses as book stores, bistros, drug stores, coffee shops, restaurants, dry cleaners and the like, as well as small outdoor gathering spaces. Two floors of residential units above the ground floor are also suggested for this area. Off street parking should be located toward the rear of properties.

Devon-Lincoln Business Hub

This business hub is an entryway into the community and a major gateway from Chicago. Uses in this business hub are recommended to be primarily retail uses on the ground level, with off street parking located toward the rear. Any development of 2nd or 3rd floors should be for office uses. Good building design, site planning and quality architecture, important throughout the corridor, is paramount in this gateway area for establishing the ambiance of the corridor. New developments in this hub should include strong anchors to provide identity and a sense of place.

Recommendation: Modify the Village Zoning Code to designate and regulate uses and encourage development in newly established business district hubs.

• Create Form-Based Development Regulations

The form and physical shape of private development is just as important as the type of uses which are permitted. Much of the existing development found in the corridor today is strip center in character, meaning that buildings are set back on the property and off street parking is provided directly in front of the building, toward the street. This form of development results in a scenery of endless parking lots, cars and freestanding signs for the passerby. Such an environment challenges any sense of place, and contributes to an auto dominated rather than pedestrian oriented environment. This plan recommends a very different approach and far different form for new development along the corridor.

Prior to modern conveniences, urban form generally took the shape of buildings built close to the public right-of-way, enclosing the street with buildings along it. Shop windows were large to let in light and also to display their products to the passerby.

Our plan embraces the essence of this earlier development pattern and seeks to update this earlier form for the 21st century. Our plan embraces the relatively new planning principles of Neo Traditional Development and New Urbanism, which seek to balance the needs of auto use with people and which assert that the form of development is as important as the permitted use of the property.

We recommend that, in addition to land use changes, applicable sections of the zoning code be created to contain form-based provisions applicable to new development along Lincoln Avenue. The following form-based provisions are recommended for the corridor (additional recommendations are contained in the addendum to this report).

Build-to Line: instead of a traditional setback line, where buildings are required to be setback from the front property line by a minimum distance, we recommend institution of a “Build-to line” that would require new buildings to be built-to. We suggest that this

build-to line should be no greater than 5 feet from the public right of way line. This recommendation would result in new buildings located very near the public sidewalk. For residential-only buildings this build-to line should be no greater than 10 feet from the right of way line, with a 4 foot wrought iron style fence, located 5 feet from the right of way.

Building Orientation: Because Lincoln Avenue travels through the Village at an angle, the orientation and facades of buildings in the corridor are critical and should be parallel to the street. Only in exceptional situations should the orientation of buildings and facades be at an angle to the Lincoln Avenue right-of-way line.

Building Height: Except for the Lincoln/Touhy Avenue Hub, the maximum height in the corridor should be 3 floors or 38 feet. In the Lincoln/Touhy downtown hub, a maximum height of 5 floors or 65 feet is recommended. Any building which is permitted to exceed 3 stories in height is recommended to have a building façade setback of 10 feet for all floors above 3 stories.

Building Corner Towers: Buildings located at corners, particularly at major intersections, should incorporate a turret or tower feature in harmony with the building's architecture. This tower feature component of the building may exceed the maximum height indicated, so as to give the building a distinctive form and focal point.

Façade Windows: Building facades fronting onto Lincoln Avenue, or onto major adjoining public streets or which otherwise are highly visible from the public right-of-way, should incorporate windows in their facades, particularly at the street level. Walls facing the public right of way which lack windows should be prohibited.

Recommendation: *Create form based development regulations as outlined here and elsewhere in this report.*

• **Create Building Design Guidelines**

Our report and the work of our consultant have suggested certain building design guidelines for new buildings in the corridor. The Task Force believes further work and additional design details are appropriate to help guide developers and architects and to illustrate design elements acceptable to the Village. It is envisioned that this work would result in a separate user friendly, heavily illustrated architectural design guideline handbook. To undertake this work, the Task Force recommends the formation of an Ad Hoc Committee of Village design professionals to help prepare this document.

Recommendation: *Create an Ad Hoc Village Design Committee to develop a Building Design Guideline Handbook.*

• **Regulate Off Street Parking Locations**

For new developments, off street parking should be prohibited from the front of new buildings along Lincoln Avenue. Such parking should be located toward the rear of the property. Related to this, the Village's recently adopted requirements ensuring buffering and screening of business properties with adjoining residential properties should be continued and required in all new developments.

The location of off street parking areas and the recommendation to prohibit them from the front of new buildings along Lincoln Avenue is a critical component of this plan and this new standard should only be varied in the most unusual circumstances. For some large scale developments, parking decks may be in order and these should be placed either behind the buildings they serve or with a level of activity, such as retail, located on the ground floor of the parking structure.

This plan does not recommend relaxation of the Village's off street parking standards at this time. These standards should be reviewed to determine their current adequacy for various uses and to investigate the feasibility of shared parking arrangements among uses.

Where new uses will occupy existing buildings, existing front yard parking areas should be screened from view with landscaping or a short screen wall along the public sidewalk. Recently, the Village adopted new code standards requiring such screening treatments for off street parking lots. Strict adherence to these standards is strongly recommended with variations granted only in the most unusual cases.

For the public right-of-way, this plan recommends the creation of a designated parking lane. Although the Task Force does not recommend that off street parking requirements be specifically relaxed, variations for certain desired proposed uses should be considered depending upon the degree of available nearby on-street parking. In the reuse of existing buildings, since there is a strong interplay between providing parking lot screening and providing off street parking spaces, where on-street parking exists and is generally available in the designated Lincoln Avenue parking lane, required parking lot screening should take precedence over the required number of off street parking stalls.

Recommendation: Prohibit new off-street parking lots in front of buildings along Lincoln Avenue. Continue to require landscape screening of existing parking areas.

• Improve Business Signage

The Village updated its Sign Code in the late 1990's to incorporate new community sign standards. This code encourages (but does not require) the use of monument signs instead of freestanding pole signs. With the greater emphasis on creating a pedestrian scale along Lincoln Avenue, consideration should be given to lowering the maximum height of monument signs, possibly to 8 feet, and either creating stronger incentives to use monument signs or reducing the maximum height of freestanding signs. Another option would be to allow only monument signs and not freestanding signs along Lincoln Avenue.

Recommendation: Explore stricter requirements for signs that would improve the pedestrian scale of the corridor.

Plan Implementation

To implement this plan, Village government must be focused at all levels and in all departments on achieving the enunciated goals and desired results of this plan. Each department and recommending body has a role in moving the Village forward toward achieving this plan. Below are the key elements of this plan and recommended implementation actions.

Streetscape Plan Development:

We recommend that funds be budgeted in the Village's Capital Improvement Plan for preparation of a streetscape plan for Lincoln Avenue. High priority consideration should be given for plan preparation in the next fiscal year, since this plan will help the Village seek grant funds for necessary streetscape improvements and provide reference for improvements which should take place as part of private development. In the actual development of this plan, where appropriate, input from various recommending bodies, should be sought.

Land Use and Form-Based Code Changes:

The land use changes and form based provisions recommended for Lincoln Avenue require changes to the Village's zoning code. Various options are available to achieve this result including overlay districts or separate zoning districts. We recommend that the consultant engaged to update the Village's Zoning Code also be charged with incorporating into this update, the recommendations of this report. Since this work may require further detailing and a variety of determinations, in concert with the Plan Commission, Task Force members have agreed to provide assistance in this work.

Village Departmental Actions

To achieve this plan, implementation will require the ongoing and concerted work of the entire Village organization. Coordinated through the Village Administrator's Office, departments must continue their current work such as median landscaping. These departments also must incorporate new activities called for in this report, such as improving crosswalks, into routine or scheduled work or maintenance plans. More significant improvements, such as those identified in the recommended streetscape plan, will require a strong working relationship and coordination between Village staff and IDOT for implementation.

Village Recommending Bodies

Citizen led recommending bodies to the Village Board have a very large role toward achieving and implementing this plan. Below are key Village Board recommending bodies having certain oversight, duties or expertise which can be utilized and employed to achieve the plan and vision for Lincoln Avenue.

Traffic Commission

Consider and provide input on the creation of the designated parking lane; the posted speed limit and other measures to reduce travel speeds in the corridor. It is possible that parking management in the corridor will become desirous in the future and parking restrictions and regulations for the parking lane may become appropriate. Further, the

Traffic Commission should study and provide input and recommendations regarding appropriate side street vacations, after first reviewing this report and clearly understanding its goals.

Plan Commission/Zoning Board of Appeals

Consider and include in the updated revised zoning code, the recommended use changes, the designation of new business district hubs and the addition of new form-based development regulations. Study and recommend changes to off-street parking requirements in this update. In the role as Zoning Board of Appeals, consider case-by-case variations to these new standards, only where unusual circumstances warrant.

Sculpture and Beautification Committees

Provide input and suggestions in the development of the recommended streetscape plan as well as suggestions on the selection of the specific styles of street furniture, street lights and monuments for the public space focal points.

Sign Appearance Review Board

Consider and review the existing Sign Code and consider recommending changes to the sign code that would enhance the pedestrian scale and environment of the corridor called for in this report.

Economic Development Commission

Study and provide recommendations which could assist in funding of corridor improvements including the long range component of new public open space, as well as measures which could further encourage new private reinvestment and corporate donations in the corridor.

Ad Hoc Building Design Guideline Committee

Study and create an illustrated architectural design guideline booklet which would communicate the building design elements and expectations of the community.

APPENDIX

LINCOLN AVENUE CORRIDOR URBAN DESIGN PLAN AND GUIDELINES

Lincoln Avenue Corridor Study
Lincolnwood, Illinois

**CORRIDOR URBAN DESIGN PLAN
& URBAN DESIGN GUIDELINES**



Prepared by Land Vision, Inc.
for
Lincoln Avenue Task Force
Final Report
November 16, 2005

**Lincoln Avenue Corridor Study
Corridor Urban Design Plan
& Urban Design Guidelines**

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Lincoln Avenue Corridor Study

Corridor Urban Design Plan & Urban Design Guidelines

Lincolnwood, Illinois

I. INTRODUCTION

The Village of Lincolnwood (the Village) and its Lincoln Avenue task Force (LATF) identified the Lincoln Avenue corridor as an opportunity for redevelopment. As the main street through the community, this strip could be transformed into a vibrant community centerpiece with a real sense of place. Land Vision, Incorporated (LVI) was retained in August 2004 to conduct the Lincoln Avenue Corridor Study that would assist the LATF in preparing an urban design plan and urban design guidelines to transform the corridor. Specifically, the LATF expected the Lincoln Avenue Corridor Study to focus on the following:

1. Identify activity centers and infill development opportunities in the corridor
2. Identify means of creating a unified image of the corridor
3. Apply principles of New Urbanism to identify appropriate building setbacks, heights and densities for new development
4. Identify areas for attractive gathering places
5. Recommend land uses and urban design guidelines to help achieve the corridor vision that can become the basis for updating the existing Village Comprehensive Plan and Zoning Ordinance
6. Provide development recommendations and guidelines for the hotel site at Lincoln and Touhy Avenues

To help accomplish these objectives, LVI invited input from Village citizens through an Image Preference Survey (IPS). Ninety-five citizens provided input, and the result of the survey showed strong preference for:

1. More traditional looking buildings than buildings with modern architecture
2. Two-to-four story mixed-use buildings with well-articulated architecture
3. Buildings with inviting, pedestrian-friendly, well-landscaped and safe sidewalks in front
4. Private developments with outdoor gathering places, such as sidewalk cafes, and pedestrian enclaves
5. Buildings located closer to residential areas with more complementary architecture
6. Unobtrusive signs, and
7. Parking areas that are well landscaped.

With the goals and objectives set by the Village and the design preferences identified by the Village citizens, LVI first prepared a Corridor Redevelopment Strategy, and overall Corridor Plan, and finally, a detailed set of Urban Design Guidelines that would govern the corridor redevelopment and enhance appearance of the existing properties.

LVI found that both the Village goals and residents' preferences coincided with the tenets of New Urbanism. New Urbanism is a modern planning movement that seeks to re-create a sense of place and foster walkable, human-scaled communities. It advocates for central downtowns, quality public spaces, diverse streetscapes, and location of stores, services, parks and schools within easy walking distance of homes.

LVI recommends that new buildings – regardless of their use or type – be placed at the front of the property, close to the street, with all parking located behind buildings. This is

principal tenet of New Urbanism; disallowing parking in front of buildings improves the street for pedestrians and enhances streetscape appearances.

Urban design guidelines will further direct the “form” of buildings: how they look, how they relate to the street and to one another, and how they, along with the streetscape, form a cohesive and well-designed urban environment. Such urban design guidelines, called for by New Urbanism, can be outlined in Village land use and zoning codes in what are called form-based regulations.

LVI strongly recommends that, as a follow-up to the adoption of the Corridor Plan and Urban Design Guidelines, the Village adopt a form-based code for the corridor. This will ensure that the vision for the Lincoln Avenue corridor is implemented consistently into the future.

II. CORRIDOR REDEVELOPMENT STRATEGY

As many other strip commercial corridors in the Chicago region, Lincoln Avenue has evolved into a corridor consisting primarily of retail and commercial uses. The current Village regulations permit banks to locate in the corridor more easily than retail uses such that the corridor now has a preponderance of banks. Most of the properties fronting onto Lincoln Avenue are shallow, and many of the existing retail uses and businesses in the corridor are at best nominal. In conjunction with a lack of proper maintenance on some of the corridor properties and a lack of adequate parking on other, the LATF determined that the existing form of development in the corridor is outmoded and fails to exploit the corridor’s full potential.

Given these concerns, LATF identified specific areas of the corridor that could be considered as future hubs of activity with new uses replacing the old and consisting of either mixed-uses or single uses. As opposed to a continuous set of uses spread uniformly along the corridor, LATF determined that creating one main hub and other smaller hubs in the corridor would encourage the development of a potential downtown in Lincolnwood that could be used for creating key gathering places to attract the Village residents and give the village a new identity. LATF also indicated that the site with the “Purple” hotel at the northwest corner of Lincoln Avenue and Touhy Avenue should be given consideration for creating the “downtown” hub in the Village.

In response, LVI recommended a Corridor Redevelopment Strategy that built on the key ideas of the LATF. As shown in Figure A, some of the major recommendations LVI made include:

1. To create a downtown hub in the Village, consider redevelopment of the “Purple” hotel site, the Bank of Lincolnwood site, and the site for Lincolnwood Produce. While some of these recommendations – especially the Bank of Lincolnwood site -- may be implemented in the long-run (over a period of next ten years and beyond), LVI recommended that in order to create a critical mass for the new developments to function as a downtown, the Village would have to allow the redevelopment of more than just the “Purple” hotel site. The range of mixed uses for these sites will include residential, retail and office. The “Purple” hotel site, in addition, may also include a boutique hotel, should the market conditions favor such use to be feasible.

2. LVI also recommended redeveloping the existing Tuffy site with a plaza and a focal-point monument in order to enhance the visual appearance of the major point of entry in to the Village from north. It would also serve as a key marker for the new downtown Lincolnwood
3. Directly in front of Proesel Park, along the park's frontage with Lincoln Avenue, LVI recommended installing another focal-point monument in the middle of the road with new pavements channeling around the monument in an oval form. From a transportation standpoint, this was recommended to calm the traffic along Lincoln Avenue, and as an urban design strategy this would create another key identifier that in conjunction with the monument at the Tuffy site would be the start of forming a cohesive image of the corridor. (See later discussion on Urban Design Guidelines for how these monuments should be treated architecturally).
4. At the site of existing Mobil gas station, located at the triangular site bounded by Lincoln Avenue, Crawford Avenue and Pratt Avenue, LVI recommended the installation of another focal monument, as part of a new mixed-use hub at this location. Surrounded by three major roads, the existing gas station creates adverse traffic conditions in the corridor that would be best ameliorated by its replacement with a public plaza (with the monument). From an urban design standpoint, since the Lincolnwood Public Library is located on Pratt Avenue, the triangular plaza in its front will provide an appropriate foreground for the civic building.
5. For the properties around the new triangular plaza along Lincoln Avenue, LVI recommended retail and residential uses, such that along with the existing public library, this area will become another mixed-use hub in the corridor. While the existing Grossinger Toyota (soon to become Grossinger Volvo) will remain an auto-oriented use at this hub, the rest of the uses will emphasize pedestrian access along with specific outdoor gathering places as part of the redevelopment projects.
6. For the next hub at the intersection of Devon Street and Lincoln Avenue, with the "Bunny Hutch" site providing a key redevelopment opportunity, LATF recommended that the new uses should primarily be commercial. This hub will include the site east of Lincoln Avenue that will be redeveloped into a new commercial use. This commercial hub at the southern end of the corridor will need to design consistency between individual buildings to create a strong sense of entry into the corridor.
7. For the intermediate redevelopment opportunities in the corridor, LVI recommended the primary use to be medium density residential. The key sites proposed for such residential redevelopment include the industrial area southwest of Lincoln Avenue and the railroad and the area east of Lincoln Avenue.
8. In conjunction with many of the above redevelopment projects, LVI recommended closing a number of street segments as they intersect with Lincoln Avenue. These are highlighted on the Corridor Plan for Lincoln Avenue that LVI developed after discussing with the LATF the specific types of medium-density residential and mixed-used projects that would be most appropriate for the corridor.

III. LINCOLN AVENUE CORRIDOR URBAN DESIGN PLAN

As the Corridor Redevelopment Strategy was finalized in discussion with the LATF, LVI prepared a detailed Corridor Urban Design Plan (Corridor Plan) to show how the individual properties or groups of properties may be redeveloped to consistent with the overall strategy. This Corridor Plan is depicted in Figure B (with larger versions of this plan available in poster form).

As Figure B shows, LVI has shown detailed building footprints and parking areas or parking garages that would be necessary to serve a redevelopment project. Many of the recommended projects will require consolidation of individual parcels and street closings. It will be critical for the Village to allow redevelopment of projects with all required parcels consolidated and not permit their individual, piecemeal development. Another major thrust of the recommendations is to “line” the front of the properties with buildings, .i.e. place all new buildings at a set maximum distance from the property line (see Urban Design Guidelines for more discussion). The Village should require all redevelopment projects to have such “liner” buildings with parking for the projects concealed behind the buildings.

While LVI prepared the individual site plans for redevelopment to demonstrate that the recommended projects will be feasible following the principles of New Urbanism, LVI and LATF expressly state that none of the redevelopment projects have to follow the footprints of buildings exactly as drawn by LVI. The Corridor Plan is CONCEPTUAL in nature and it is the intent of LATF to allow developers and builders who will undertake redevelopment in the corridor, a full measure of flexibility provided their projects follow the urban design guidelines. As and when the Village adopts a new form-based code that is based on the urban design guidelines recommended by LVI (see below), the project redevelopers will need to conform to the new ordinance regulations of the Village.

IV. URBAN DESIGN GUIDELINES

As stated by the LATF, the future Lincoln Avenue Corridor must have an overall image that is not haphazard but more uniform and cohesive. A mile-long corridor with an array of uses developed over many years has an image today that is disparate, outmoded (in visual terms as well, in addition to economic) and dysfunctional. The challenge of achieving the goal of cohesive redevelopment out of such conditions can be met by adopting the urban design guidelines recommended by LVI.

In order to meet the full scope of a new urban design for the corridor that also includes the citizens’ image preferences, LVI recommended urban design guidelines that cover not only the domain of the public street, but also the domain of private properties and how the buildings in the corridor relate to the street and to one another.

The public domain guidelines apply to properties that will not be redeveloped for the foreseeable future. These guidelines include the “interface” between the public and private domains, covering the area of private property directly adjacent to the right-of-way. The private domain guidelines apply to a property or a group of properties that is set for redevelopment. These guidelines will cover the portions of redevelopment project that include sidewalks, street furniture, planters and landscaping.

For the corridor as a whole, LVI also recommended that there be on-street parking – whether associated with existing uses or redevelopment projects. While the corridor already has on-street parking, this parking will need to be integrated into a new sidewalk system with “bump-outs” from the existing sidewalk line.

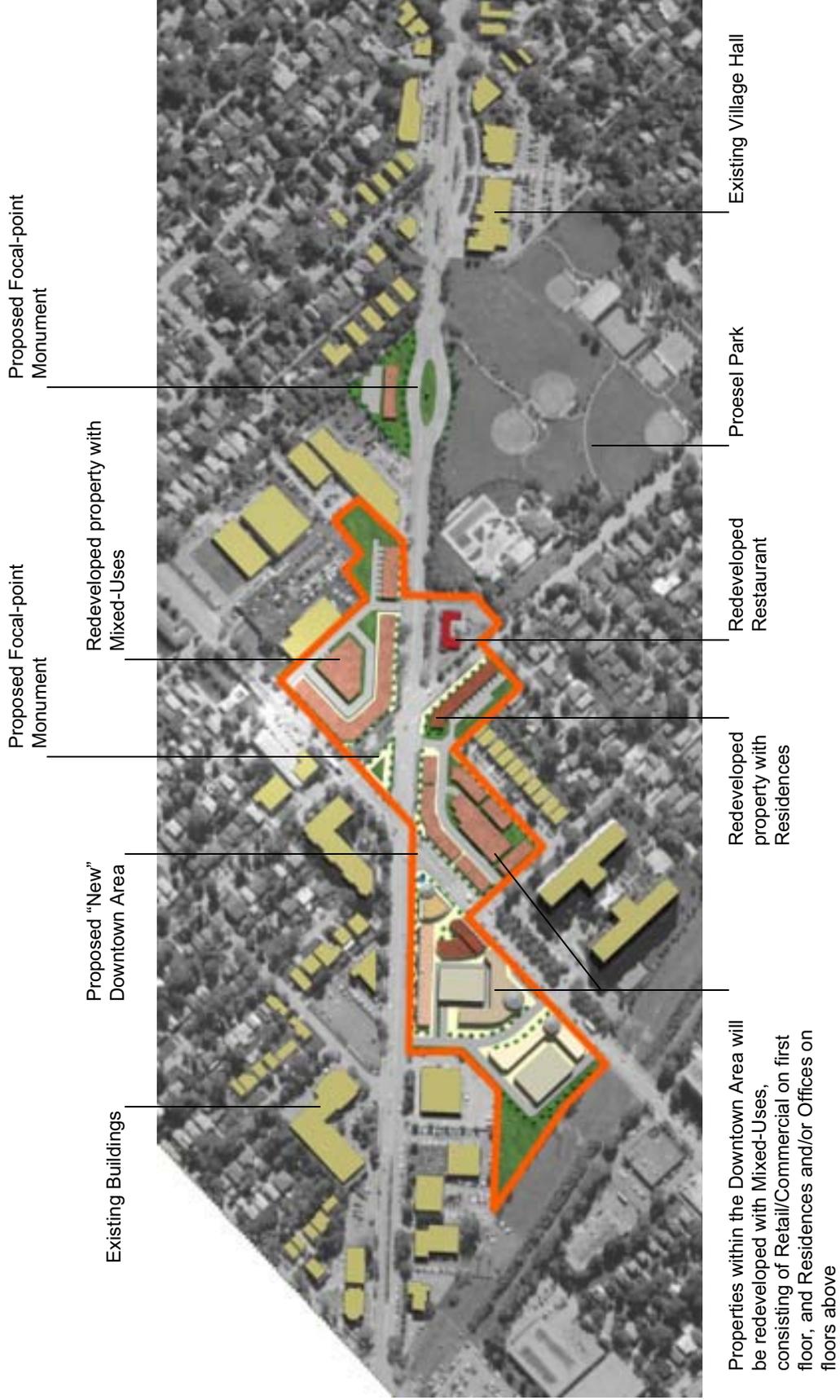


Figure B1: Illustrative Corridor Plan: North
 (Note: All site plans in this exhibit are shown for illustration only. Developers seeking to redevelop properties will follow the urban design guidelines)

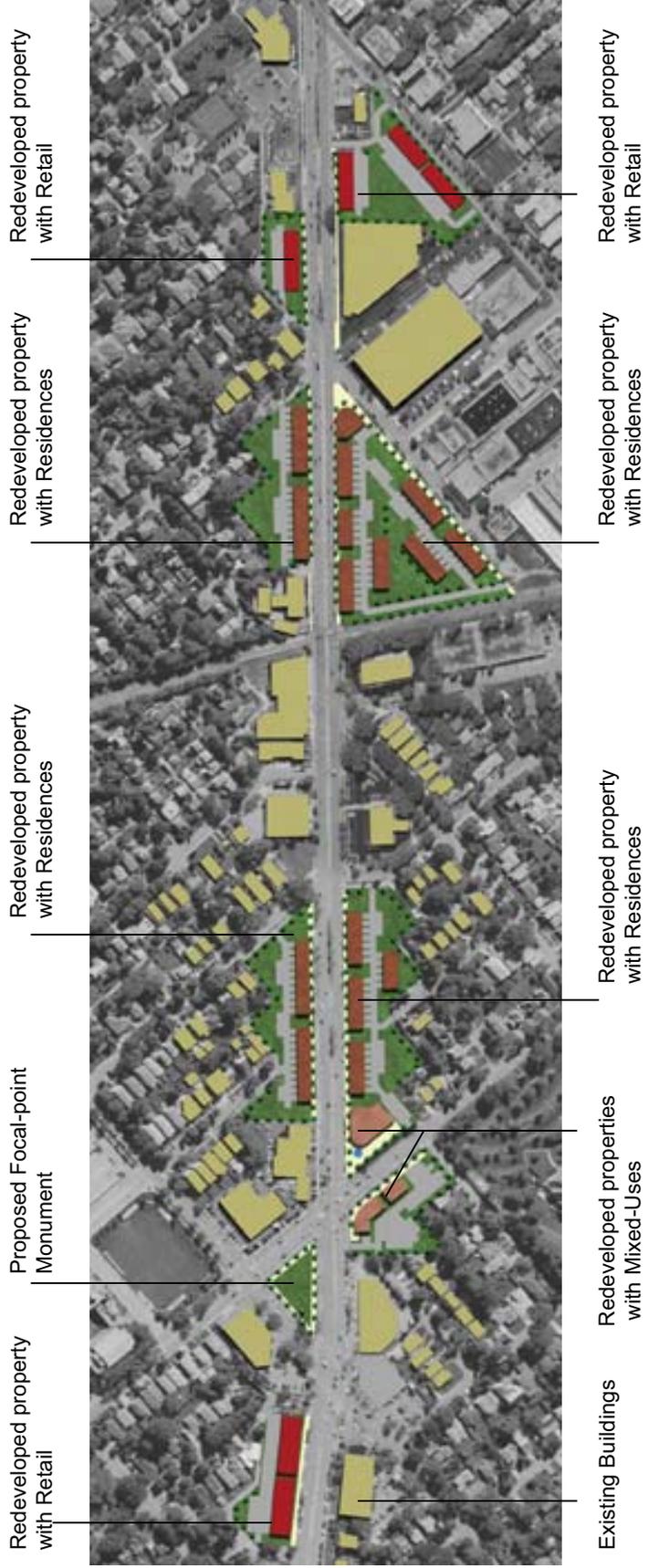


Figure B2: Illustrative Corridor Plan: South
 (Note: All site plans in this exhibit are shown for illustration only. Developers seeking to redevelop properties will follow the urban design guidelines)

The Village will need to prioritize the implementation of the urban design guidelines for the public domain based on the severity of existing conditions and availability of funding. If redevelopment is anticipated, then the urban design guidelines for the private domain (or the form-based code regulations) will be implemented at the time when new project is constructed. Regardless of the timing of when public or private improvements are implemented, in the long run, following the urban design guidelines (or form-based code regulations) will help the corridor achieve the desired cohesive image.

1. GUIDELINES FOR PUBLIC DOMAIN

Covering the area within the R.O.W., including these streetscape elements: Streets; Medians; Street curbs; Parkways; Sidewalks; Street signs; Private signs; Street lights; Median lights and landscaping

Taking into account the above streetscape elements, LVI identified several existing conditions that are typical of different sections of the corridor. For these corridor sections, LVI recommends a number of design guidelines that are illustrated in plan views and cross-sections below. LVI also recommends certain general guidelines that can be applied to any section in the corridor. The Village staff will need to determine the best fit between the existing conditions and the recommended guidelines to select the most appropriate streetscape improvements necessary for implementation in a given section. The recommended guidelines are not meant to supersede the existing, more restrictive zoning regulations that meet the design intent of the guidelines. What follows first is a description of the general urban design guidelines and then the more section-specific guidelines. The guidelines are described in no specific order of preference.

A. General Guidelines

1. **Parkway Trees:** All parkways within the Lincoln Avenue corridor with available space for landscaping should have trees planted at a minimum center-to-center distance of 25 feet (or, as per the existing Village ordinance)
2. **Median Landscaping:** Within the medians along the Lincoln Avenue Corridor, there should be adequate landscaping consisting of shrubs and other materials that are well-maintained
3. **Landscape Materials:** Selection of landscape materials to be used in the parkways, medians or sidewalks should be governed by their hardiness to road salt and with the goal of achieving consistency between one median landscaping to the next
4. **Sidewalk Materials:** Sidewalks in the corridor should have two different materials to better delineate the area used for pedestrians – especially when new sidewalks are built in conjunction with the redevelopment of properties. There should be consistency in the use of sidewalk materials from one property to another
5. **Pedestrian Buffers:** All pedestrian portions of sidewalks should be buffered from the street by landscape planters and/or row of trees
6. **Street Lights:** Selection of new light standards for the corridor should be based on how well they relate to the pedestrians without overwhelming them by their size, height or level of illumination. Examples from other communities such as Elmhurst where IDOT agreed to install special light fixtures in conjunction with a major roadway widening project, suggest that it should be possible for the Village to work with IDOT to have special light fixtures installed in the corridor. The light fixtures with special pedestrian oriented lighting should be located on both sides of the street and not in the median.

7. **Street Signs:** Street signs in the corridor should adopt a distinct new design that makes them stand out as compared to the regular street signs in the rest of the Village to create a special image of the corridor
8. **Street Furniture:** Street furniture, such as benches, information kiosks, telephone kiosks, waste receptacles and bicycle stands, should be provided along the municipal park where it meets the Lincoln Avenue corridor, mixed-use hubs and other areas of the corridor as later deemed appropriate. The design of street furniture in one area should match the street furniture design in another area to further reinforce the visual cohesiveness of the corridor
9. **Business Signs:** For free-standing signs advertising businesses located on the same property, their location, size, height and design should have a level of consistency that lends cohesiveness to the appearance of the corridor
10. **Focal-point Monuments:** The Corridor Plan recommends three focal-point monuments in the corridor: the first at the current Tuffy site (at Touhy and Lincoln Avenues); the second within the proposed bi-furcated roadway pavements of a "traffic oval" in front of Proesel Park, and the third at the current Mobil gas station site (at Crawford and Lincoln Avenue). The design of these monuments should be consistent in order to create a unified set of visual landmarks in the corridor. Of the three monuments the most significant in height and architectural treatment should be the tower in front of the Proesel Park. Because of its close proximity to the Village Hall that's built in Prairie Style architecture, the Village may consider requiring the design of this monument to be in Prairie style as well. In addition to visually tie these major identifiers together, the other two monuments may also be constructed in Prairie style architecture.

B. Section-specific Guidelines

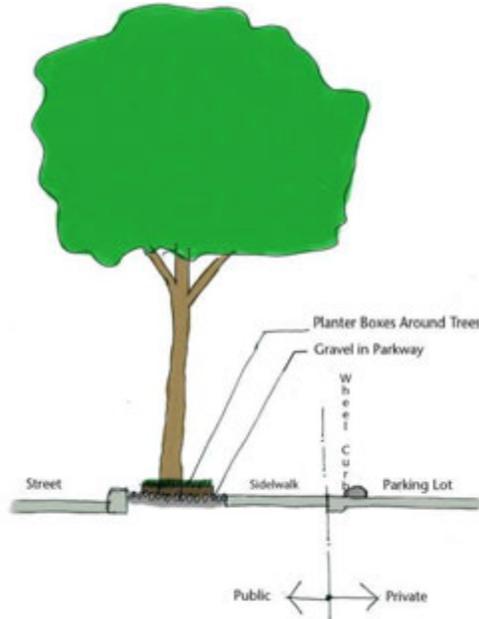
The use of these guidelines will vary from one section of the corridor to another. They are presented as illustrations that show the existing conditions needing improvement and the recommended guidelines to achieve better urban design. The guidelines also cover the interface where the private property abuts the right of way. This interface affects the visual perception of the corridor as much as the quality of the streetscape elements within the right-of-way. Until the time the buildings on a property remain the same – even if the uses in the buildings change – focusing on this interface between the public and private domains will be important. When properties redevelop, new buildings in the corridor will be located close to the street along with new sidewalks in front of the new buildings. This will create a new set of conditions that will be governed by the guidelines for Private Domain. Until change occurs on the private properties, the following design guidelines show how to treat the public domain and the edge of the private properties.

Section-specific Guidelines for Public Domain

NOTE: *These guidelines apply only to properties with existing buildings. For new or redevelopment projects, see Urban Design Guidelines for Private Domain*

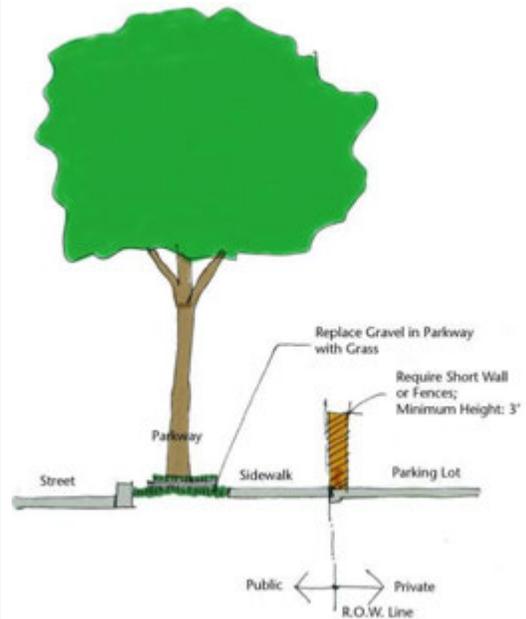
- Existing parkway has materials other than grass, and only wheel-stops exist between the sidewalk and adjoining parking lot

Figure 1: EXISTING CONDITIONS



- Appearance of gravel in the parkway is visually unappealing and inconsistent with the rest of the properties in the corridor with grass in parkway
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists. This makes the corridor more unattractive

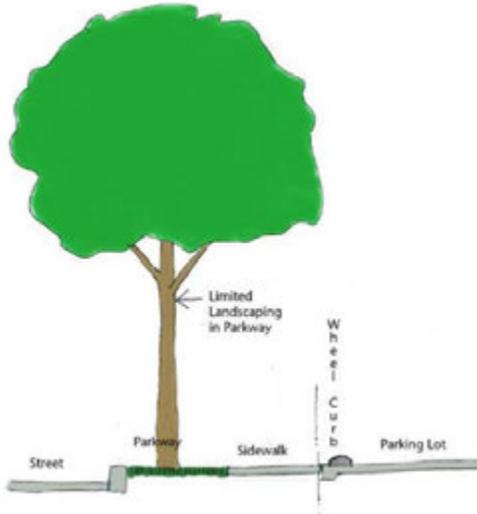
Figure 1A: RECOMMENDED GUIDELINES



- Replace gravel with grass in the parkway
- Require a short wall of no less than 3' to be installed at the property line to conceal the parked cars. Materials and color of wall should primarily match the materials and color of other walls along the corridor

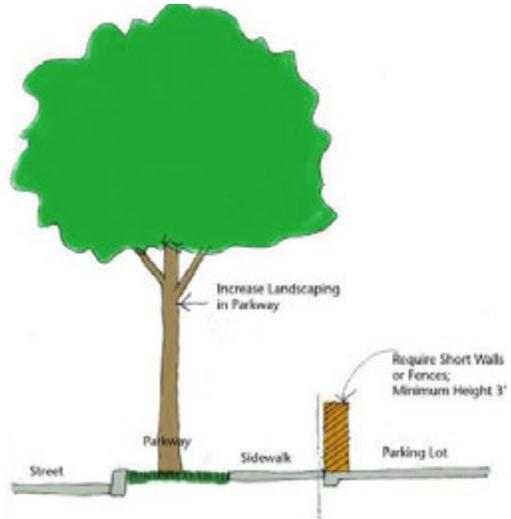
- Existing parkway has limited number of trees and like the first condition has no significant barrier between the sidewalk and the adjoining parking lot

Figure 2: EXISTING CONDITIONS



- Limited number of trees in the parkway fails to define the corridor edge effectively
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists makes the corridor more unattractive

Figure 2A: RECOMMENDED GUIDELINES



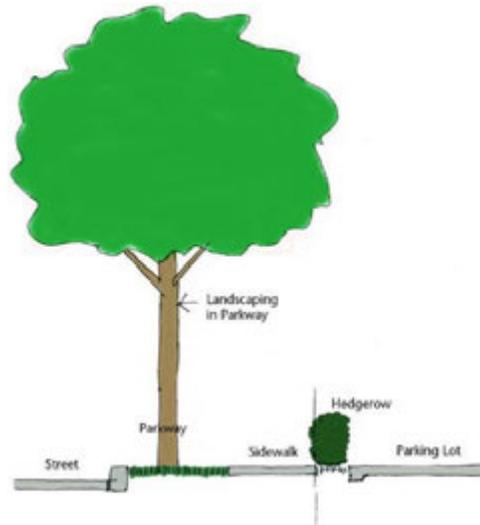
- Increase the number of trees in the parkway to be no more than 25' apart
- As in Recommendation 1, require short walls at the edge of the private property

Alternate to Short Walls (as shown in Recommendations '1' and '2')

Figure 3: EXISTING CONDITIONS

- Limited number of trees in the parkway fails to define the corridor edge effectively
- With no other barrier than wheel curbs, cars in a parking lot are exposed to direct view of the motorists. This makes the corridor more unattractive

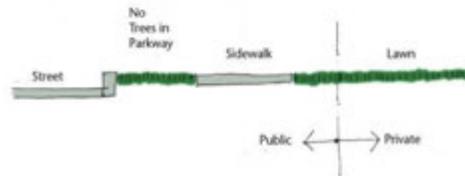
Figure 3A: RECOMMENDED GUIDELINES



- Increase the number of trees in the parkway to be no more than 25' apart
- Instead of short walls, require hedgerows at the edge of the private property in a way. This will not affect the overall parking supplied at the property. Hedgerows should be a minimum 3' tall

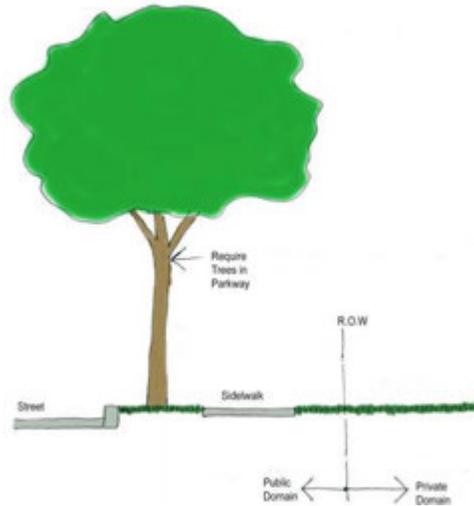
4. Existing parkway has no (or few) trees and the adjoining property has no parking lot

Figure 4: EXISTING CONDITIONS



1. Without the trees, even when the adjoining property has lawn in its front yard and no parking lot, the street lacks a defining edge.

Figure 4A: RECOMMENDED GUIDELINES



1. Require trees to be planted within the R.O.W. where there are no trees, OR when there are few trees,
2. Require trees in the parkway no less than 25' apart

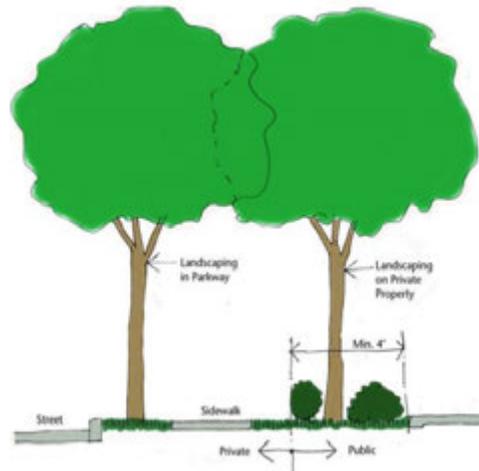
5. Ideal Condition for Landscaping on Private Property

Figure 5: Existing Conditions

When any of the following conditions exist on a property, i.e. if:

1. There are materials within the parkway that are not grass
2. There are limited number of trees in the parkway
3. There are no trees in the parkway at all, and when there are no physical elements separating the adjoining parking lots from the sidewalk

Figure 5A: Recommended Guidelines



1. Require parkway trees
2. On private property, require a landscape buffer no less than 4 feet wide for trees and shrubs. The double row of trees – one in the parkway, the other on private property – should be staggered.

(The Village of Lincolnwood already has examples of this type of streetscape along Devon Avenue near the intersection with Crawford Avenue).

2. GUIDELINES FOR PRIVATE DOMAIN

Covering the private property, including: Placement of buildings; Height of Buildings; Relation of building to street/sidewalk; Relation of building to buildings along one side of the street; Relation of buildings to buildings across the street; Treatment of building entryways; Relation of entryways of one building to the next; Treatment of building bulk; Treatment of special gathering places outside the building

The design guidelines for private domain are described in general terms and not on section-specific or site-specific basis. They are differentiated by categories of use and are based on the design preferences of the Village citizens and LATF members. The three categories of private domain guidelines are:

- A. All Buildings
- B. Residential Buildings
- C. Commercial and Mixed-use Buildings

A. Design Guidelines for All Buildings

1. ***Build-to Line for Placement of Commercial and Mixed-Use Buildings, and Residential Buildings**** : The new commercial and mixed-use buildings in the Lincoln Avenue corridor should be located close to the front of property, at 5 feet from the right-of-way line, with parking concealed from view from Lincoln Avenue and located behind buildings. Residential buildings that do not provide fences in the front yard should also be located at 5 feet from the ROW line. (See Figure 6)

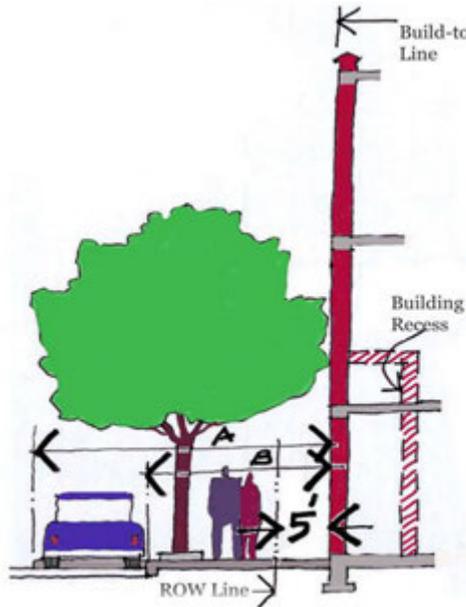


Figure 6: Illustrative Guidelines for Redevelopment of Properties

Maximum Distance from R.O.W to "Build-to" Line:	5 feet
'A': Maximum Distance from Exterior Wall to Street Curb, Including Parking Bay:	23-24 feet
'B': Maximum Distance from Exterior Wall to Street Curb, Excluding Parking Bay:	15 feet

* Applies to only Residential Buildings without fences in the front yard. For residential buildings with fences in the front yard, see guidelines for residential buildings

2. **Building Relationship to Street:** The relationship of new buildings (all commercial and mixed-use and some residential*) to the street and other street amenities is illustrated in Figure 7. The current right-of-way of Lincoln Avenue is 100 feet and the road pavement is 80 feet. The guidelines call for creating “bump-outs” from the existing sidewalk line of up to 8 feet or 9 feet to enclose parking bays within the sidewalk system. Recommended trees in the corridor will be installed at 25’ center-to-center. Buildings will be located at the “Build-to” line 5 feet from the Lincoln Avenue right-of-way. Touhy Avenue has the same right-of-way and pavement widths as Lincoln Avenue. The proposed guidelines will also apply for redevelopment of properties along Touhy Avenue within the recommended mixed-use/downtown hub.

(*Only for residential buildings without fences in the front yard)

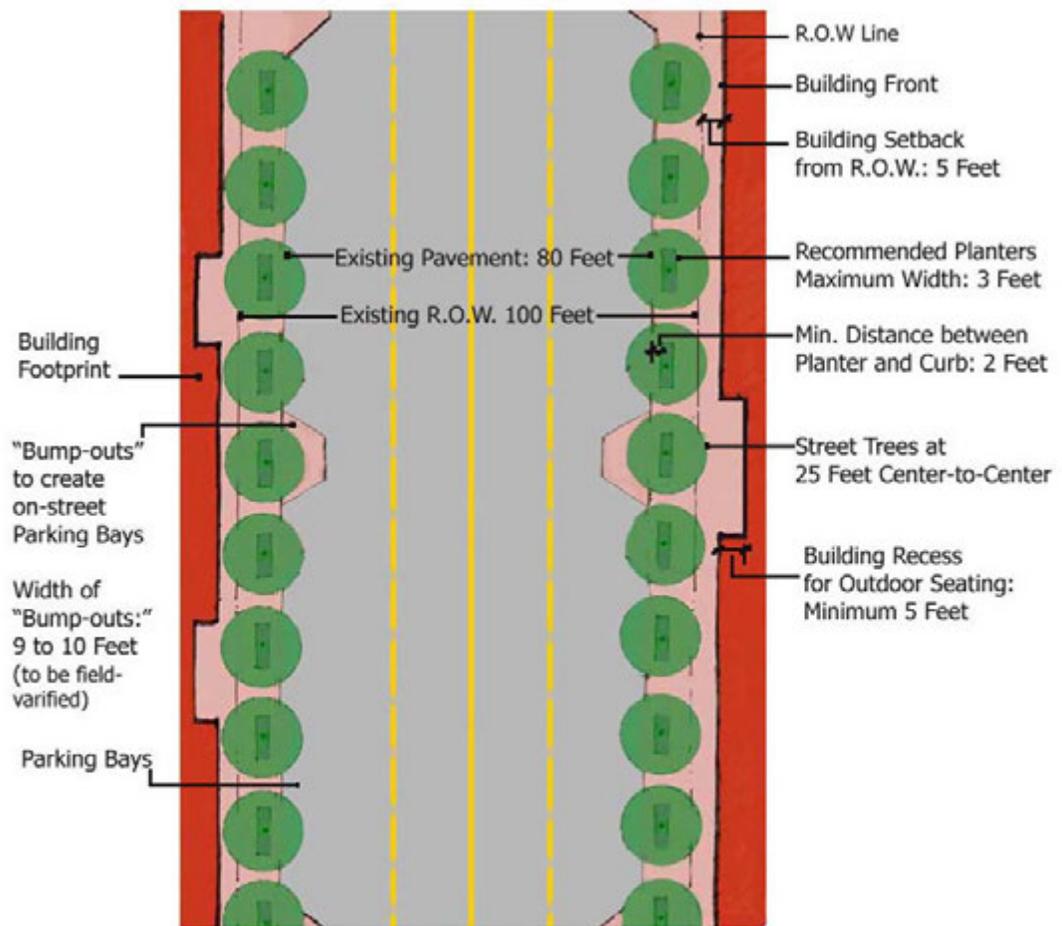


Figure 7: Lincoln Avenue Corridor Redevelopment Guidelines
 (Note: These guidelines also apply to Touhy Avenue Redevelopment, in proximity to Lincoln Avenue)

3. **Building Heights:** No building in Lincoln Avenue Corridor should have more than three floors, except in the recommended mixed-use/downtown hub where the buildings may be five stories tall. As shown in Figure 8, portions of building higher than three stories, should be recessed back a minimum of 10 feet.

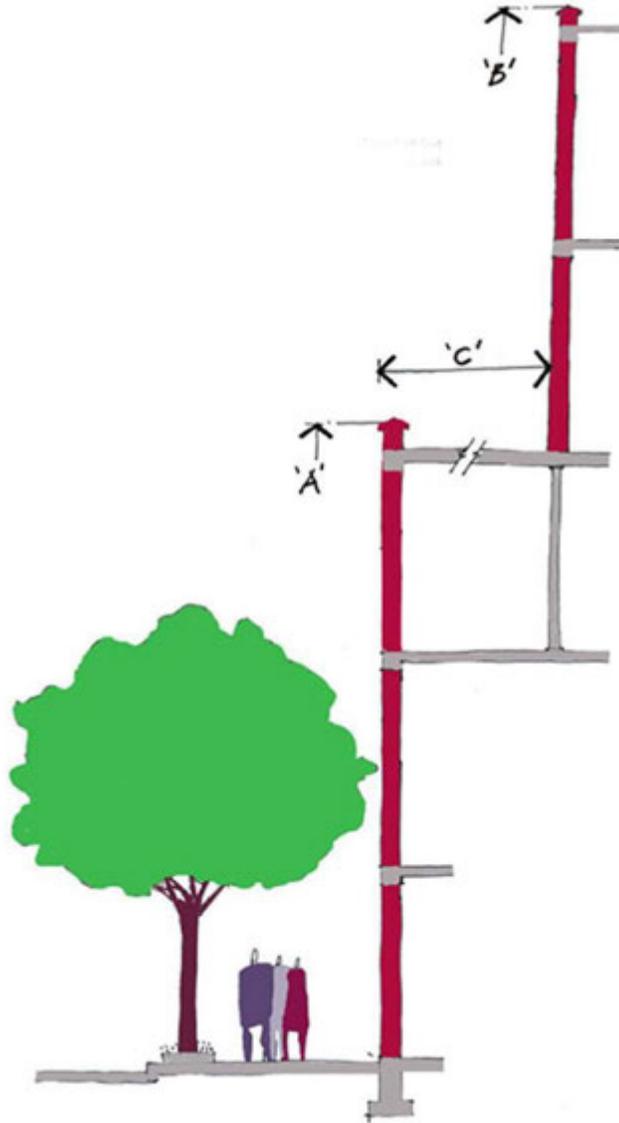


Figure 8: Illustrative Guidelines for Buildings Higher than Three Floors

- | | |
|--|------------------|
| 'A': Maximum Allowable Height Immediately Adjacent to Sidewalks: | 3 Floors/38 feet |
| (Measured to the top of eave from the top of the sidewalk) | |
| 'B': Maximum Allowable Height for Buildings: | 5 Floors/65 feet |
| (Measured to the top of the eave from the top of the sidewalk) | |
| 'C': Minimum Setback for Floors Above Three Floors: | 10 feet |

4. **Building Orientation and Elevation:** New buildings in the corridor should be located parallel to the street frontage. The mass of buildings should be broken up in a rhythmic fashion – well articulated -- so as not to create plain building facades. Balconies and other such elements may be used to create variety and interest and yet remain integral to the overall design of the building (See Image 1)



Image 1: An example of building placed parallel to Lincoln Avenue, with a well-articulated building elevation

5. **Angled Building Facades to Streets:** On an exceptional basis, provided there is a strong reason to do so, the Village may approve buildings with angled façade toward Lincoln Avenue. Image 1 is an example of angled buildings that may be considered acceptable on exceptional basis.



Image 2: An example of angled buildings facing a major street that may be acceptable

6. **Building Corner Towers:** Buildings located at a major intersection should have a tower feature at the corner that may emphasize key entrance to the building. These

corner towers should be in harmony with the building's architecture and will be allowed to exceed the maximum permitted building height so as to create the distinct appearance. (See Image 3 for illustration)



Image 3: An example of "Corner Tower" integral to the overall design of building

7. **On-street Parking:** For new buildings facing Lincoln Avenue there should be on-street parking as part of a sidewalk system that includes "bump-outs" from the existing sidewalk line. All required parking for a use should be provided behind the buildings. (See Figure 7 for parking "bump-outs.")
8. **Building to Building Relation Across or Along Lincoln Avenue:** Buildings located next to each other along the corridor, and buildings located across the street, should have a level of design consistency through either the use of similar materials and colors or distinct architectural elements such as cornices, dormers, balconies, brackets and pendants. It is not necessary for buildings to have the same architectural style to achieve design consistency, although it can help.
9. **Impacts on Adjoining Residential Uses:** Whenever the rear parking impacts the uses on adjoining properties, specific landscape buffers, fences or short walls, or a combination thereof, should be installed by the new building owners
10. **"Dead" Walls Along Streets:** While attractive landscaping can enhance the quality of streetscape, buildings with "dead" or blank walls along the sidewalks should be avoided (or minimized). Residential or other buildings with units above a terrace and parking below that create blank walls along the street should be avoided. (See Images 4 & 5 for comparison.)



Image 4: Example of attractive landscaping that enhances streetscape



Image 5 shows the same streetscape (as in Image 4) with buildings that create blank walls along a sidewalk. This is an example of building relation to street that should be discouraged.

B. Additional Design Guidelines for Residential Buildings

- 1. **Placement of Residential Buildings with Fences in Front Yard:** Residential buildings with fences in front yard should not be placed more than 10 feet from the right-of-way line. In such cases, the fence should be placed at 5 feet from the right-of-way line, with a hedgerow behind the fence toward the building side. The fences in the front yards should be wrought iron that are no more than 4' high. (See Image 6)



Image 6: An acceptable example of fence along residential buildings facing Lincoln Avenue

- 2. **Pedestrian Access to Residential Buildings:** Residential buildings should have direct access to units from the front street side, with strong entries to individual units emphasized by distinct architectural elements such as porticos. (See Image 7)



Image 7: Residential building along street with strong statement for entries to units

3. ***Vehicular Access to Residential Buildings from Lincoln Avenue:*** No residential building should be allowed to have garages facing Lincoln Avenue or driveways with direct vehicular access from Lincoln Avenue.
4. ***Garages in Residential Buildings:*** For residential buildings, the fronts of units at the rear of the property should not face the garages of the units in the building in front. (See Image 8 that is an example of what should not be allowed.)



Image 8 shows residential units facing garages. This should not be allowed in the corridor.

5. ***Pocket Parks in Residential Projects:*** For new residential projects on properties with shallow depth, pocket parks between buildings should be provided to create visual interest and on-site recreational opportunities. (See Image 9)



Image 9: Pocket parks for properties with shallow depth

C. Additional Design Guidelines for Commercial and Mixed-Use Buildings

1. **Outdoor Seating:** For commercial buildings that allow outdoor seating for their patrons, the portion of buildings that houses cafes, restaurants and other such uses should be set back 5' from the normal build-to line. (See Image 10)



Image 10: Additional building set back from build-to line to allow outdoor eating and other activities

2. **Minimum Height for Building Setbacks for Outdoor Seating:** For the outdoor seating areas created by the additional setback from the build-to line, the minimum height for “recesses” should be 10 feet. (See Figures 6 & 7 for building “recesses”)

Request For Board Action

REFERRED TO BOARD: October 7, 2014

AGENDA ITEM NO: 6

ORIGINATING DEPARTMENT: Public Works

SUBJECT: Consideration of an Ordinance Waiving the Competitive Bidding Process and Accepting a Proposal from Christopher B. Burke Engineering, Ltd. for the Provision of Civil Engineering Services to Develop a Streetscape Concept Plan for the Devon-Lincoln Tax Increment Financing District in the Not-to-Exceed Amount of \$47,500

SUMMARY AND BACKGROUND OF SUBJECT MATTER:

On June 24, 2014 the Devon-Lincoln TIF District ("TIF District") was established. During the process of establishing the TIF District in September of 2013 the Village and the City of Chicago engaged the Urban Land Institute (ULI) through the Chicago Metropolitan Agency for Planning (CMAP) to provide recommendations for redevelopment of the six-block span from Lincoln Avenue to McCormick Boulevard on Devon Avenue within the TIF District. As part of this report several recommendations were made regarding revitalization of the area. Specifically, the report recommends implementing streetscape enhancements. On October 28, 2013 the report was presented and subsequently endorsed by the Village Board.

Upon establishment of the TIF District staff convened to discuss implementing the streetscape recommendation. Staff requested that the Village's Engineering Firm Christopher B. Burke Engineering, Ltd. (CBBEL) develop concept plans for the Devon-Lincoln TIF District. Attached for your consideration is a proposal from CBBEL to develop concept plans. The scope of work includes the following:

- Develop typical cross sections for four primary alternatives for the Devon Avenue Corridor:
 - Parkway enhancements on the north side only
 - Parkway enhancements on the north and south side
 - Road diet (lane reduction or rechannelization) for bike lane and landscaped medians
 - Road diet for bike lane and widened parkways
- Conduct site visits
- Topographic survey
- Stakeholder outreach
- Develop 4 streetscape concepts
- Crosswalk enhancements
- Cost estimate and schedule
- Present concepts to the Village Board

Upon completion of the concept plans they will be presented to the Village Board for review. Upon acceptance of the final concept plan CBBEL will draft a proposal to develop construction plans for the project.

The Village began utilizing CBBEL as the Village Engineer in June of 2014. To date, the Village has found that CBBEL has performed at a high level and met deadlines in a timely manner. In addition, CBBEL has excellent references regarding streetscape design. Staff recommends waiving the competitive bidding process and utilizing CBBEL to develop civil engineering streetscape concept plans for Devon Avenue.

At the September 16, 2014 Village Board meeting the Village Board tabled this item and requested and requested that staff return at a Committee of the Whole to further review the alternatives discussed within the proposal at the October 7, 2014 Committee of the Whole. The Village Board requested additional information regarding alternative 3 and 4. Specifically, for alternative 3 whether a road diet would be ideal for Devon Avenue and for alternative 4 whether the City of Chicago would be interested in or plans to implement similar improvements.

FINANCIAL IMPACT:

The not-to-exceed amount for this proposal is \$47,500. Funds were not budgeted in FY 2014/2015 for this project due to the fact that the Devon-Lincoln Tax Increment Financing (TIF) District was not established at that time; however \$2 million in funds were allocated for engineering and capital improvements for the Devon-Lincoln TIF within the Appropriation Ordinance which was approved after the TIF was established.

DOCUMENTS ATTACHED:

1. Proposed Ordinance
2. Christopher B. Burke Engineering, Ltd. Proposal

RECOMMENDED MOTION:

Move to approve an Ordinance waiving competitive bidding and accepting a proposal from Christopher B. Burke Engineering, Ltd. of Rosemont, IL for the provision of civil engineering services to develop streetscape concept plans for the Devon-Lincoln Tax Increment Financing District.

VILLAGE OF LINCOLNWOOD

ORDINANCE NO. 2014-_____

**AN ORDINANCE WAIVING COMPETITIVE BIDDING AND
AUTHORIZING AN AGREEMENT WITH
CHRISTOPHER B. BURKE ENGINEERING, LTD., OF ROSEMONT, ILLINOIS,
FOR THE PROVISION OF CIVIL ENGINEERING SERVICES
TO DEVELOP CONCEPT STREETScape PLANS FOR THE DEVON-LINCOLN TAX
INCREMENT FINANCING DISTRICT**

ADOPTED BY THE
PRESIDENT AND BOARD OF TRUSTEES
OF THE VILLAGE OF LINCOLNWOOD
THIS ____ DAY OF _____, 2014.

Published in pamphlet form
by the authority of the
President and Board of Trustees
of the Village of Lincolnwood,
Cook County, Illinois this
_____ day of _____, 2014

Village Clerk

**AN ORDINANCE WAIVING COMPETITIVE BIDDING AND
AUTHORIZING AN AGREEMENT WITH
CHRISTOPHER B. BURKE ENGINEERING, LTD., OF ROSEMONT, ILLINOIS,
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TO DEVELOP CONCEPT STREETScape PLANS FOR THE DEVON-LINCOLN TAX
INCREMENT FINANCING DISTRICT**

WHEREAS, the Village of Lincolnwood is a home rule municipal corporation in accordance with Article VII, Section 6(a) of the Constitution of the State of Illinois of 1970; and

WHEREAS, on June 24, 2014, pursuant to the Tax Increment Allocation Redevelopment Act, 65 ILCS 5/11-74.4-1 et seq., the Village President and Board of Trustees adopted Ordinance No. 2014-2110, designating the area generally including the properties generally located along the north side of Devon Avenue between McCormick Boulevard and Proesel Avenue, and the properties generally located in the adjacent areas to the north of such property along Devon Avenue, as the Devon-Lincoln Redevelopment Project Area ("**Redevelopment Project Area**"); and

WHEREAS, the Urban Land Institute has issued a report recommending that the Village design and implement streetscape improvements within that portion of the Redevelopment Project Area located along the north side of Devon Avenue between McCormick Boulevard and Lincoln Avenue (collectively, the "**Proposed Streetscape Improvements**"); and

WHEREAS, the Village has identified the need to develop civil engineering streetscape concept plans to evaluate the Proposed Streetscape Improvements ("**Concept Plans**"); and

WHEREAS, the Village desires to retain a civil engineering consultant to develop the Concept Plans; and

WHEREAS, Christopher B. Burke Engineering, Ltd., of Rosemont, Illinois ("**CBBEL**"), has submitted a proposal to the Village to develop the Concept Plans, in an amount not to exceed \$47,500 ("**Proposal**");

WHEREAS, the Village President and Board of Trustees have determined that CBBEL is uniquely qualified to develop the Concept Plans in an efficient and cost-effective manner, and that it is appropriate and in the best interests of the Village for CBBEL to perform the civil engineering work for the development of the Concept Plans; and

WHEREAS, in order to accept the Proposal, the President and Board of Trustees have determined that it is appropriate to waive competitive bidding for the acceptance of the Proposal and the development of the Concept Plans by CBBEL, pursuant to Section 8-8-13 of the Municipal Code of Lincolnwood, as amended ("**Village Code**"); and

WHEREAS, the President and Board of Trustees has determined that it will serve and be in the best interests of the Village to accept the Proposal submitted by CBBEL for the development of the Concept Plans;

NOW, THEREFORE, BE IT ORDAINED BY THE PRESIDENT AND BOARD OF TRUSTEES OF THE VILLAGE OF LINCOLNWOOD, COOK COUNTY, ILLINOIS, as follows:

SECTION 1. RECITALS. The facts and statements contained in the preamble to this Ordinance are found to be true and correct and are hereby adopted as part of this Ordinance.

SECTION 2. WAIVER OF COMPETITIVE BIDDING. The advertising and bidding requirements for the Proposal, and for the development of the Concept Plans by CBBEL, are hereby waived in accordance with Section 8-8-13 of the Village Code and the home rule powers of the Village.

SECTION 3. ACCEPTANCE OF PROPOSAL. The President and Board of Trustees hereby accept the Proposal submitted by CBBEL for the development of the Concept Plans.

SECTION 4. EXECUTION OF PROPOSAL. The Village President, Village Manager and Village Clerk are hereby authorized and directed to execute and attest, on behalf of the Village, the Proposal and all related documentation necessary for the development by CBBEL of the Concept Plans.

SECTION 5. SEVERABILITY. If any provision of this Ordinance or part thereof is held invalid by a court of competent jurisdiction, the remaining provisions of this Ordinance are to remain in full force and effect, and are to be interpreted, applied, and enforced so as to achieve, as near as may be, the purpose and intent of this Ordinance to the greatest extent permitted by applicable law.

SECTION 6. EFFECTIVE DATE. This Ordinance will be in full force and effect from and after its passage, by a vote of two-thirds of the Board of Trustees, and approval and publication in the manner provided by law.

[SIGNATURE PAGE FOLLOWS]

PASSED this ____ day of _____, 2014.

AYES: _____

NAYS: _____

ABSENT: _____

ABSTENTION: _____

APPROVED by me this ____ day of _____, 2014.

Gerald C. Turry, President
Village of Lincolnwood, Cook County, Illinois

ATTESTED and FILED in my office this

____ day of _____, 2014

Beryl Herman, Village Clerk
Village of Lincolnwood, Cook County, Illinois



CHRISTOPHER B. BURKE ENGINEERING, LTD.

9575 West Higgins Road Suite 600 Rosemont, Illinois 60018 TEL (847) 823-0500 FAX (847) 823-0520

August 22, 2014

REVISED September 25, 2014

Village of Lincolnwood
7001 N. Lawndale Avenue
Lincolnwood, IL 60712

Attention: Ms. Ashley R. Engelmann
Assistant to the Public Works Director

Subject: Proposal for Professional Engineering Services
Devon Avenue Streetscape – Concept Planning
Village of Lincolnwood

Dear Ashley:

As requested, Christopher B. Burke Engineering, Ltd. (CBBEL) is providing this proposal for professional engineering services related to the Village of Lincolnwood's Devon Avenue Streetscape Project. Included in this proposal are our Understanding of the Assignment, Scope of Services and Estimate of Fee.

UNDERSTANDING OF THE ASSIGNMENT

CBBEL understands that the Village of Lincolnwood would like to develop concepts for streetscape improvements for the portion of Devon Avenue between Lincoln Avenue and McCormick Boulevard. CBBEL also understands that the Village of Lincolnwood has Tax Increment Financing (TIF) dollars available to fund improvements for the portion within the Village limits however the Village is interested in developing a landscape median option and would like to look at the effects of a road diet. The development of these elements will affect both the north and south sides of the roadway and since the southern half of Devon Avenue is within the City of Chicago we will want to reach out to the Alderman of that ward and the Chicago Department of Transportation (CDOT) for their input on these features.

CBBEL envisions developing typical cross sections for 4 primary alternatives for this portion of Devon Avenue. Once an alternative is selected, a concept plan shall be prepared.

➤ Alternative 1 – Parkway Enhancements along north side only

This is the simplest scenario. It will look at only the north portion of Devon Avenue. This concept will show pedestrian pavement improvements, roadway / pedestrian lighting, ADA

route improvements, site furnishings and general enhancements to the aesthetics of these 6 blocks.

➤ Alternative 2 – Parkway Enhancements along north and south sides

This alternative will look at improving both the north and south parkways of Devon Avenue. This concept will show pedestrian pavement improvements, roadway / pedestrian lighting, ADA route improvements, site furnishings and general enhancements to the aesthetics of these 6 blocks.

➤ Alternative 3 – Road Diet for bike lane and landscaped medians

This alternative includes the parkway enhancement elements plus looks at the development of landscape median(s) in the center of Devon Avenue. The development of the median will require the removal of one of the through lanes in each direction which will necessitate a Road Diet for this portion of Devon Avenue. The current ADT for this portion of Devon is 14,300 which is at the upper end for IDOT allowable Road Diets. Since the dividing line between the Village of Lincolnwood and the City of Chicago is the center of Devon Avenue, cooperation and approval will be sought from both the adjoining Alderman's office and CDOT. This scenario does not disturb the south curb line or any of their existing lighting.

➤ Alternative 4 – Road Diet for bike lane and widened parkways

The last alternative will look at a 'Complete Streets' option where both the north and south sides of the corridor are enhanced. In this option it will be important to coordinate the proposed appearance and infrastructure improvements with the City of Chicago. The Alderman, CDOT and the various utility divisions for the City of Chicago will be involved in the planning process.

Once the board approves the concept design, CBBEL will provide a proposal for Phase 2 / Design Engineering for developing final plans, specifications and soliciting bids for construction.

SCOPE OF SERVICES

Task 1 – Site Visits (Initial Meeting and Walkthrough): CBBEL Staff will meet with representative of the Village to understand their goals and desires for the project area. Members of our project team will accompany VILLAGE personnel on a site visit of the project area (N. Lincoln Avenue – North McCormick Boulevard). The team will walk the entire project length including portions of the side streets and determine the exact project limits and identify any unique features which may affect the streetscape design. CBBEL will prepare meeting minutes and a photograph log for use in preparing the design. As the project design continues, Team members will return to the site to collect additional data as issues are identified.

Task 2 – Topographic Survey: CBBEL will perform a topographic survey within the Concept Project Limits (Lincoln to McCormick) to understand design and utility parameters. The survey will extend to the alleyway down side streets. The survey will be used as an accurate base for developing the various concepts and for final design engineering.

Task 3 – Stakeholder Outreach: When we begin to explore the alternatives, we will contact the local Alderman's office, CDOT, and the various City of Chicago Utility departments to identify potential conflicts, goals and desires for this portion of the project.

Task 4 – Streetscape Concept Typical Section: Using the information gathered in the above tasks, CBBEL will create a Concept Streetscape Typical Cross Section for the project area, illustrating alternative one.

Task 5 – Streetscape Concept Plan: Once the preferred alternate is selected by Village Staff, CBBEL will prepare a concept Streetscape plan of the preferred alternative. The plan will consist of a plan view and typical illustrations of the proposed appearance.

Task 6 – Crosswalk Enhancements: CBBEL will investigate and provide various crosswalk enhancement alternatives for the corridor.

Task 7 – Cost Estimate and Schedule: An estimate will be developed for alternative one along with an accompanying implementation timeline.

Task 8 – Staff Meetings and Village Board Presentation: CBBEL will attend 2 working staff meetings to discuss the project scope and solicit input from Village Staff and one meeting with the Economic Development Commission. The final product will be a presentation of the concept design to the Village Board.

Task 9 – Streetscape Concept Typical Sections for Alternatives 2-4: Using the information gathered in the above tasks, CBBEL will create Concept Streetscape Typical Cross Section for the project area, illustrating alternatives 2-4.

Task 10 – Cost Estimate and Schedule for Alternatives 2-4: An estimate will be developed for alternatives 2-4 along with an accompanying implementation timeline.

ESTIMATE OF FEE

TASK	FEE
Task 1 – Site Visit	\$ 1,000
Task 2 – Full Topographic Survey	\$ 16,500
Task 3 – Stakeholder Outreach	\$ 3,000
Task 4 – Streetscape Concept Typical Section	\$ 500
Task 5 – Streetscape Concept Plan	\$ 10,000
Task 6 – Crosswalk Enhancements	\$ 3,000

Task 7 – Cost Estimate and Schedule	\$ 1,125
Task 8 – Staff Meetings and Village Board Presentation	\$ 5,000
	<hr/> Direct Costs \$ 2,500
	<hr/> SUBTOTAL \$42,625

OPTIONAL TASKS

Task 9 – Streetscape Concept Typical Sections for Alternatives 2-4	\$ 1,500
Task 10 – Cost Estimate and Schedule for Alternatives 2-4	\$ 3,375
	<hr/> TOTAL \$47,500

We will bill you at the hourly rates specified in our previously accepted Master Agreement. Direct costs for blueprints, photocopying, mailing, overnight delivery, messenger services and report compilation are not included in the fee estimate. Please note that any requested meetings or additional services are not included in the preceding fee estimate and will be billed at the hourly rates.

Please sign and return one copy of this agreement as an indication of acceptance and notice to proceed. Please feel free to contact us anytime.

Sincerely,



Christopher B. Burke, PhD, PE, D.WRE, Dist.M.ASCE
President

THIS PROPOSAL ACCEPTED FOR THE VILLAGE OF LINCOLNWOOD:

BY: _____

TITLE: _____

DATE: _____